



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Allyson Foster Boyd Allyson Foster Boyd
2026.02.06 13:45:57-05'00'

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CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921
Allyson Foster Boyd, P.E. No. 119,736

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

| SHEET NAME | SHEET NO. |
|---|--------------|
| SIGNATURE SHEET | BRIDGE-SIGN1 |
| TITLE SHEET | 1 |
| INDEX AND STANDARD DRAWINGS | 1A |
| PROJECT COMMITMENTS | 1B |
| ESTIMATED ROADWAY QUANTITIES & UTILITY OWNERS | 2 |
| GENERAL NOTES, SPECIAL NOTES, AND ENVIRONMENTAL SPECIAL NOTES | 2C |
| TABULATED QUANTITIES | 2F – 2F2 |
| PERMANENT PAVEMENT MARKING PLAN BR. NO. 47-10040-19.30 | 3 |
| TRAFFIC CONTROL PLAN BR. NO. 47-10040-19.30 KEY MAP | 3A |
| TRAFFIC CONTROL PLAN BR. NO. 47-10040-19.30 PHASE 1 | 4, 4A – 4U |
| TRAFFIC CONTROL PLAN BR. NO. 47-10040-19.30 PHASE 2 | 5, 5A – 5S |

NOTE: THE ALPHABETICAL LETTERS "I", "O", AND "Q" ARE NOT USED IN THE NUMBERING OF SHEETS



THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Susanne Dawson Susanne Dawson
2026.02.06 13:35:18-05'00'

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CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921
Susanne Dawson, P.E. No. 111,826

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

| SHEET NAME | SHEET NO. |
|---|--------------|
| SIGNATURE SHEET | BRIDGE-SIGN1 |
| TITLE SHEET | 1 |
| LAYOUT OF BRIDGE TO BE REPAIRED | BR-133-304 |
| GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES | BR-133-305 |
| PHASE 1 CONSTRUCTION | BR-133-306 |
| PHASE 2 CONSTRUCTION | BR-133-307 |
| SUPERSTRUCTURE REPAIRS SPAN A | BR-133-308 |
| SUPERSTRUCTURE REPAIRS SPAN J | BR-133-309 |
| BENT 1 - DIAPHRAGM REPAIRS | BR-133-310 |
| BENT 3 - DIAPHRAGM REPAIRS | BR-133-311 |
| BENT 4 - DIAPHRAGM REPAIRS | BR-133-312 |
| BENT 5 - DIAPHRAGM REPAIRS | BR-133-313 |
| BENT 6 - DIAPHRAGM REPAIRS | BR-133-314 |
| BENT 8A AND 8B - DIAPHRAGM REPAIRS | BR-133-315 |
| BENT 9A, 9B, AND 9C - DIAPHRAGM REPAIRS | BR-133-316 |
| JOINT REPAIR DETAILS | BR-133-317 |
| CONCRETE REPAIR DETAILS | BR-133-318 |

| YEAR | PROJECT NO. | SHEET NO. |
|------|---------------|--------------|
| 2026 | 471040-M3-012 | BRIDGE-SIGN1 |
| | | |
| | | |

KNOX CO. I-40

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

INDEX OF SHEETS

SEE SHEET 1A FOR INDEX

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

KNOX COUNTY

I-40 BRIDGE OVER NORFOLK SOUTHERN
RAILROAD, WILLOW AVENUE, AND
JACKSON AVENUE, LM 19.30

PS&E

BRIDGE REPAIR

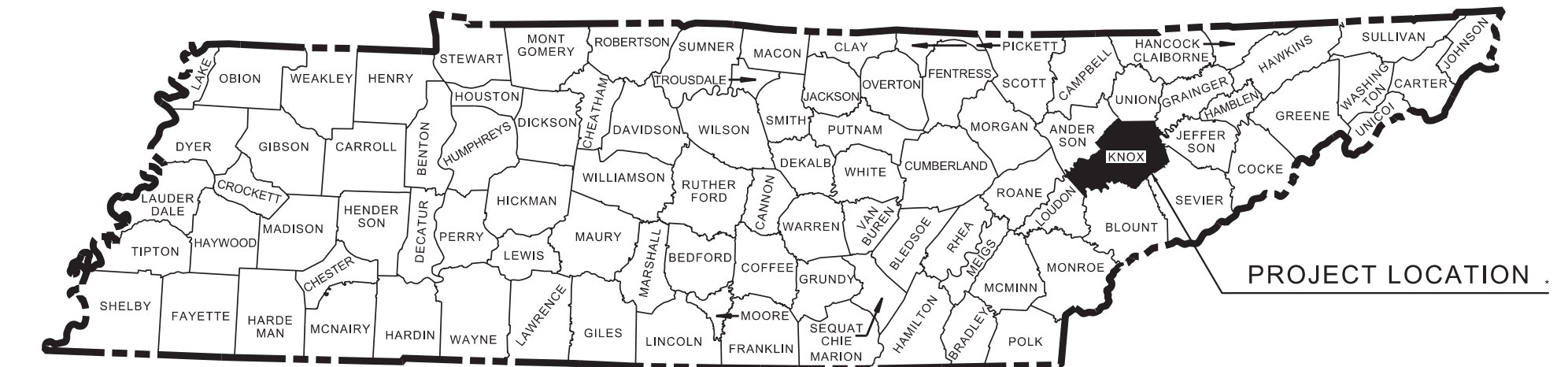
STATE HIGHWAY NO. N/A
F.A.H.S. NO. I-40

BRIDGE ID. NO. 47-I0040-19.30

| | | |
|--|--------------------------------------|-------------------------------------|
| DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86 | YES | <input checked="" type="radio"/> NO |
| WORK ZONE SIGNIFICANCE DETERMINATION | | |
| SIGNIFICANT | <input checked="" type="radio"/> YES | <input type="radio"/> NO |

| | | |
|--------------------|---------------|-----------|
| TENN. | YEAR | SHEET NO. |
| | 2026 | 1 |
| FED. AID PROJ. NO. | | |
| STATE PROJ. NO. | 471040-M3-012 | |
| FED. BRIDGE ID NO. | 47100400205 | |

KNOX CO.



SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : STEPHEN WILSON

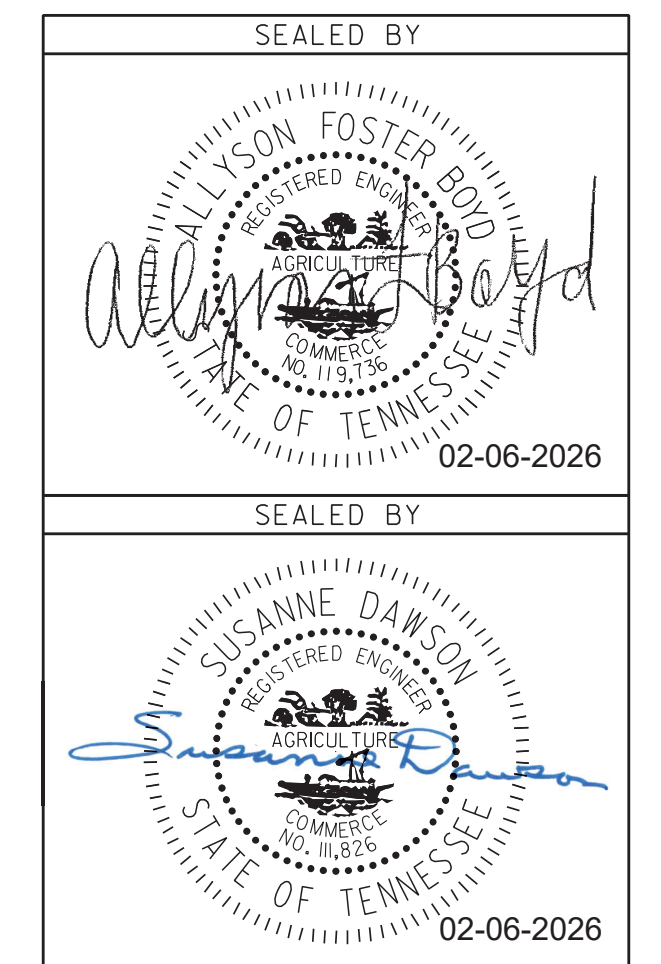
DESIGN FIRM : CDM SMITH

DESIGNER : SUSANNE DAWSON / ALLYSON FOSTER BOYD CHECKED BY FRANK G. BALE, JR. / KATHLEEN C. HOLMES

P.E. NO. N/A

PIN NO. 134994.00

TOTAL DISTURBED AREA = LESS THAN 0.95 ACRES



APPROVED: 
SHANE HESTER, CHIEF ENGINEER

DATE: _____

APPROVED: 
WILL REID, COMMISSIONER

INTERSTATE 40

TRAFFIC DATA

| | |
|------------|---------|
| ADT (2025) | 38,553 |
| ADT (2045) | 46,264 |
| DHV (2025) | 3,911 |
| D | 63 - 37 |
| T (ADT) | 5 % |
| T (DHV) | 5 % |
| V | 60 MPH |

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 1A |
| PS&E | 2026 | 471040-M3-012 | 1A |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

LIST OF BRIDGE DRAWINGS

| DRAWING | DWG. NO. |
|---|------------|
| LAYOUT OF BRIDGE TO BE REPAIRED | BR-133-304 |
| GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES | BR-133-305 |
| PHASE 1 CONSTRUCTION | BR-133-306 |
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| JOINT REPAIR DETAILS | BR-133-317 |
| CONCRETE REPAIR DETAILS | BR-133-318 |

LIST OF BRIDGE REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)
M-472-123 THRU M-472-125, M-472-142 THRU M-472-158,
M-472-160A, M-472-166, M-472-171 THRU M-472-173, M-472-186, M-472-187,
M-472-190, M-472-191, M-472-193, M-472-194, M-472-200, M-472-201A,
M-472-201B, M-472-201C AND STANDARD DWG. STD-1-5 (REV. 07-31-00)

STANDARD BRIDGE DRAWINGS

| DRAWING | REV. DATE | DESCRIPTION |
|---------|-----------|-----------------------------------|
| STD-8-5 | 10-01-25 | STANDARD PROTECTIVE FENCE DETAILS |

LIST OF SPECIAL PROVISIONS

| ID | REV. DATE | DESCRIPTION |
|--------|-----------|----------------------------|
| 712PTQ | 10-07-24 | TRAFFIC QUEUE PROTECTION |
| 716CPM | 03-30-22 | CONTRAST PAVEMENT MARKINGS |

ROADWAY INDEX

| SHEET NAME | SHEET NO. |
|---|--------------|
| SIGNATURE SHEET | BRIDGE-SIGN1 |
| TITLE SHEET | 1 |
| INDEX AND STANDARD DRAWINGS | 1A |
| PROJECT COMMITMENTS | 1B |
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STANDARD ROADWAY DRAWINGS

| DWG. | REV. | DESCRIPTION |
|------|------|-------------|
|------|------|-------------|

STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS

| | | |
|---------|----------|------------------------------------|
| RD-A-1 | 02-20-20 | STANDARD ABBREVIATIONS A THROUGH L |
| RD-A-2 | | STANDARD ABBREVIATIONS M THROUGH Z |
| RD-L-1 | 02-20-20 | STANDARD LEGEND |
| RD-L-1A | | STANDARD LEGEND |

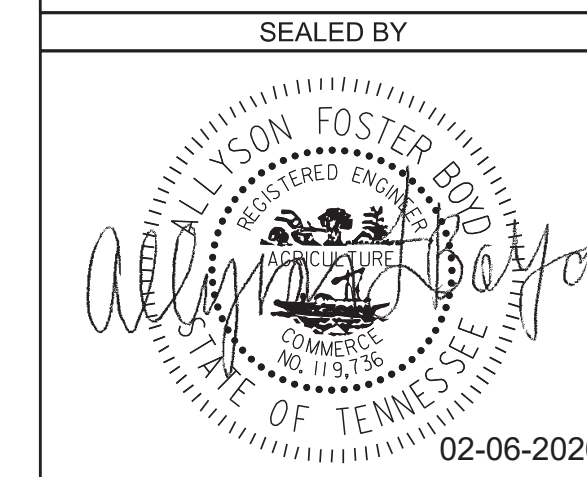
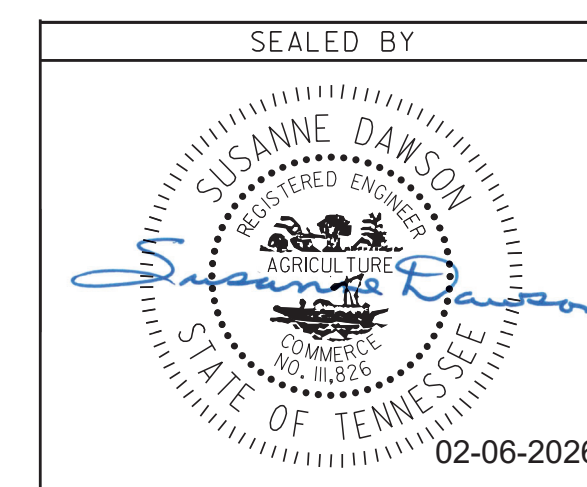
SAFETY DESIGN AND GUARDRAILS

| | | |
|--------|----------|---------------|
| S-CC-1 | 10-01-24 | CRASH CUSHION |
|--------|----------|---------------|

STANDARD TRAFFIC DESIGN DRAWINGS

DESIGN - TRAFFIC CONTROL

| | | |
|-----------|----------|---|
| T-M-5 | 01-24-25 | MARKING DETAIL FOR FREEWAYS |
| T-M-6 | 01-24-25 | MARKING DETAIL FOR EXPRESSWAY & FREEWAY INTERCHANGES |
| T-M-7 | 01-24-25 | GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES |
| T-M-8 | 01-24-25 | MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS |
| T-M-9A | 01-24-25 | PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS |
| T-WZ-10 | 03-26-25 | ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS |
| T-WZ-11 | 03-26-25 | ONE LANE CLOSURE DETAIL FOR DIVIDED HIGHWAYS |
| T-WZ-16 | 03-26-25 | LANE SHIFT FOR DIVIDED HIGHWAYS AND FREEWAYS |
| T-WZ-63 | 03-26-25 | WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP |
| T-WZ-FAB1 | 03-26-25 | FLASHING YELLOW ARROW BOARD |



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

INDEX
AND
STANDARD
DRAWINGS

| | | | |
|------|------|---------------|-----------|
| TYPE | YEAR | PROJECT NO. | SHEET NO. |
| L&G | 2025 | 471040-M3-012 | 1B |
| PS&E | 2026 | 471040-M3-012 | 1B |
| | | | |

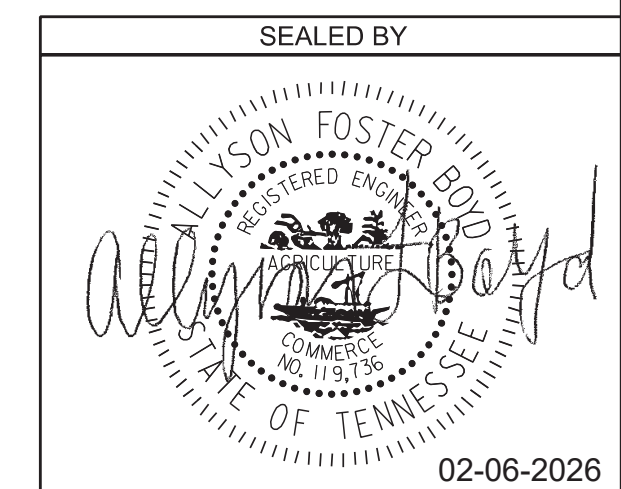
KNOX COUNTY I-40 (JAMES WHITE PKWY)

PROJECT COMMITMENTS

| COMMITMENT ID | SOURCE DIVISION | DESCRIPTION | STA. / LOCATION |
|---------------|--|--|---|
| EDHZ001 | ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS | An Asbestos Containing Material (ACM) survey was completed on Bridge No. 47100400205 James White Pkwy over NSRR, Willow Ave, and Jackson Ave LM 19.30 (47-10040-19.30). No asbestos was detected. Please see the report for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03). | I-40 Bridge over Norfolk Southern Railway, Willow Ave., and Jackson Ave. (BR. NO. 47-10040-19.30) |
| | | | |

1/29/2026 12:34:34 PM c:\pw_p11\peplowrta\d5113642\001B_WO13_Project_Commitments_FC.dgn

PIN NO.: 134994.00



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 2A |
| PS&E | 2026 | 471040-M3-012 | 2 |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

ESTIMATED ROADWAY QUANTITIES

- ⑧
- ③⑤
- ②
- ①③⑥
- ⑤
- ⑤
- ⑦
- ④⑤
- ④⑤
- ⑤
- ⑨
- ⑨
- ⑨
- ⑨
- ⑩

| ITEM NO. | DESCRIPTION | UNIT | QUANTITY |
|-----------|--|------|----------|
| 712-01 | TRAFFIC CONTROL | LS | 1 |
| 712-04.01 | FLEXIBLE DRUMS (CHANNELIZING) | EACH | 489 |
| 712-05.01 | WARNING LIGHTS (TYPE A) | EACH | 24 |
| 712-06 | SIGNS (CONSTRUCTION) | S.F. | 1,780 |
| 712-07.03 | TEMPORARY BARRICADE (TYPE III) | L.F. | 234 |
| 712-08.03 | ARROW BOARD (TYPE C) | EACH | 2 |
| 712-08.11 | QUEUE PROTECTION TRUCK | HOUR | 1,800 |
| 712-09.02 | REMOVABLE PAVEMENT MARKING (8" BARRIER LINE) | L.F. | 12,682 |
| 712-09.31 | REMOVABLE BLACK-OUT TAPE (8") | L.F. | 4,836 |
| 713-16.01 | CHANGEABLE MESSAGE SIGN UNIT | EACH | 6 |
| 716-01.23 | SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR) | EACH | 107 |
| 716-02.06 | PLASTIC PAVEMENT MARKING (TURN LANE ARROW) | EACH | 3 |
| 716-02.07 | PLASTIC PAVEMENT MARKING (24" BARRIER LINE) | L.F. | 93 |
| 716-03.01 | PLASTIC WORD PAVEMENT MARKING (ONLY) | EACH | 6 |
| 716-03.09 | PLASTIC WORD PAVEMENT MARKING (WEST) | EACH | 2 |
| 716-03.10 | PLASTIC WORD PAVEMENT MARKING (EAST) | EACH | 2 |
| 716-03.11 | PLASTIC WORD PAVEMENT MARKING (NORTH) | EACH | 2 |
| 716-04.01 | PLASTIC WORD PAVEMENT MARKING (STRAIGHT-TURN ARROW) | EACH | 4 |
| 716-04.07 | PLASTIC WORD PAVEMENT MARKING (EXIT ONLY ARROW) | EACH | 2 |
| 716-09.83 | CONTRAST PAVEMENT MARKING 6 INCH | L.F. | 4,979 |
| 716-09.84 | CONTRAST PAVEMENT MARKING 8 INCH | L.F. | 378 |
| 716-09.87 | CONTRAST PAVEMENT MARKING, WORDS, AND SYMBOLS | EACH | 25 |
| 716-09.89 | CONTRAST PAVEMENT MARKING 12 INCH | L.F. | 701 |
| 716-09.97 | CONTRAST PAVEMENT SHADOW MARKING 6 INCH | L.F. | 1,395 |
| 716-10.50 | PREFORMED PLASTIC PAVEMENT MARKING (INTERSTATE SHIELD) | EACH | 6 |
| 716-12.02 | ENHANCED FLATLINE THERMO PVMT MRKNG (6 IN LINE) | L.M. | 1.3 |
| 716-12.06 | ENHANCED FLAT LINE THERMO (8 IN BARRIER LINE) | L.F. | 378 |
| 716-12.09 | ENHANCED FLAT LINE THERMO (12 IN LINE) | L.F. | 701 |
| 717-01 | MOBILIZATION | LS | 1 |

FOOTNOTES

- ① ALL COSTS ASSOCIATED WITH INSTALLING, STORING, COVERING, AND RE-INSTALLING ALL TRAFFIC CONTROL DEVICES AND SIGNS (EXISTING AND TEMPORARY) DURING AND BETWEEN THE DIFFERENT TRAFFIC CONTROL PHASES WILL BE INCLUDED IN THE BID ITEM OF EACH ITEM. DURING THE TIME BETWEEN DIFFERENT TRAFFIC CONTROL PHASES, THE CONTRACTOR SHALL STORE ALL TRAFFIC CONTROL DEVICES IN A PROPER LOCATION THAT WILL NOT INTERFERE WITH THE TRAFFIC FLOW. ALL WORK MUST MEET THE FULL APPROVAL OF THE TDOT ENGINEER.
- ② TO INCREASE VISIBILITY AND ATTENTION TO TRAFFIC CONTROL.
- ③ THIS ITEM INCLUDES THE COST OF INSTALLATION, INSPECTION AND MAINTENANCE OF ANY SIGN SHEETING AND SUPPORTS AND TRAFFIC CONTROL APPURTENANCES DURING THE CONSTRUCTION OPERATIONS.
- ④ TO SUPPORT LANE SHIFTS AND CHANGES TO TRAVEL PATH FOR THE LANE CLOSURES REQUIRED FOR CONSTRUCTION. ITEM INCLUDES APPLICATION AND REMOVAL OF TEMPORARY MARKINGS FOR EACH CONSTRUCTION PHASE. THIS ALSO INCLUDES ALL MAINTENANCE TO KEEP THE TAPE ON THE ROADWAY. SHOULD THE TAPE NEED REPLACING, THIS WILL BE CONSIDERED TYPICAL MAINTENANCE AND INCIDENTAL TO THE WORK ITEM. NO ADDITIONAL PAYMENT WILL BE MADE FOR REPLACING AND/OR MAINTAINING ALREADY PLACED TAPE STRIPING.
- ⑤ ITEM CAN BE INCREASED OR DECREASED AS DIRECTED BY THE TDOT ENGINEER.
- ⑥ ITEM INCLUDES ALL EFFORT AND MATERIALS NECESSARY TO COVER OR MODIFY EXISTING OVERHEAD SIGNS INCLUDING REQUIRED TRAFFIC CONTROL AND EQUIPMENT.
- ⑦ FOR INTERSTATE QUEUE PROTECTION ON I-40, I-275, SR-158, AND SR-1. SEE TDOT SP712PTQ.
- ⑧ THIS ITEM INCLUDES ANY PREPARING (SMOOTHING) AND REPLACING OF RUMBLE STRIPS SHOULD THEY CONFLICT WITH THE WHEEL PATHS FOR TEMPORARY TRAFFIC.
- ⑨ FOR DETAILS, SEE TDOT SP716CPM.
- ⑩ CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION.

UTILITY OWNERS

ELECTRIC:

KNOXVILLE UTILITIES BOARD
P.O. BOX 59017
4505 MIDDLEBROOK PIKE
KNOXVILLE, TN 37921-5599
CONTACT: DANIEL MCELYEA
OFFICE PHONE: (865) 558-2699
CELL PHONE: (865) 216-0752
EMAIL: daniel.mcelyea@kub.org

GAS:

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KNOXVILLE, TN 37921-5599
CONTACT: TIM KELLEY; AARON BOTT
OFFICE PHONE: (865) 558-2577
CELL PHONE: (865) 320-2681; (423) 341-2565
EMAIL: timothy.kelly@kub.org; aaron.bott@kub.org

WATER, SEWER:

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4505 MIDDLEBROOK PIKE
KNOXVILLE, TN 37921-5599
CONTACT: NOAH NEWPORT
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TELEPHONE/ COMMUNICATIONS:

AT&T
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CONTACT: JAY FRAZIER
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EMAIL: jf092g@att.com

MCIVERIZON/BROOKS
603 WAREHOUSE PARK LANE
KNOXVILLE, TN 37932
CONTACT: STANLEY MAJKA
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WINDSTREAM COMMUNICATIONS
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LINDEN, TN 37096
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WOW!
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KNOXVILLE, TN 37932
CONTACT: CARL DIGGS
OFFICE PHONE: (865) 382-7702
EMAIL: carl.diggs@wowinc.com

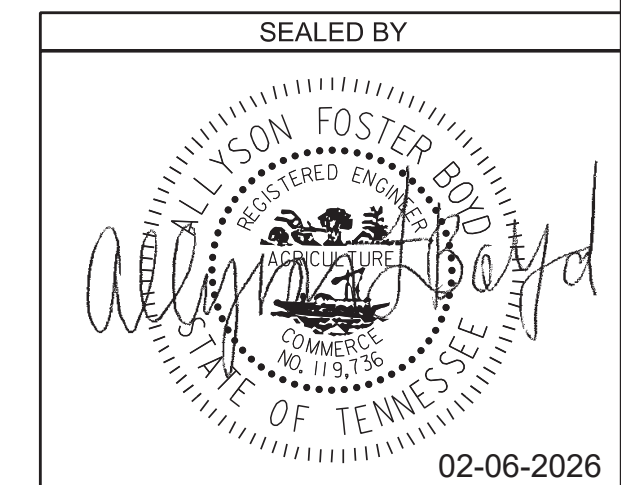
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CONTACT: JAMES MCCAWLEY
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OFFICE PHONE: (865) 382-7702
EMAIL: carl.diggs@wowinc.com

UTILITY NOTES

UTILITY
(1) NO UTILITY CONFLICTS ARE ANTICIPATED BASED ON THE SCOPE OF WORK.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED ROADWAY QUANTITIES & UTILITY OWNERS

GENERAL NOTES

MISCELLANEOUS

- (1) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

ROAD CLOSURE

- (1) NO LESS THAN SEVEN (7) DAYS PRIOR TO THE CLOSURE OF THE ROAD, THE CONTRACTOR SHALL NOTIFY THE FOLLOWING INDIVIDUALS OR AGENCIES COMPLETELY DESCRIBING THE AFFECTED ROADS AND THE APPROXIMATE DURATION OF THE CONSTRUCTION: THESE PARTIES INCLUDE, BUT ARE NOT LIMITED TO: (1) LOCAL LAW ENFORCEMENT OFFICE, (2) LOCAL FIRE DEPARTMENT, (3) AMBULANCE SERVICE, (4) LOCAL SCHOOL SUPERINTENDENT, (5) UNITED STATES POSTAL SERVICE, AND (6) LOCAL ROAD SUPERINTENDENT.

PAVEMENT MARKINGS

FINAL PAVEMENT MARKING

- (1) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (1) BEFORE OPENING THE ROADWAY TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE) PER L.F. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LUMP SUM.

TRAFFIC CONTROL DIRECTIONAL SIGNING

- (1) ON ALL ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL UTILIZE ALL EXISTING DIRECTIONAL SIGNING FOR AS LONG AS POSSIBLE. THESE EXISTING SIGNS CAN BE MOVED USING TEMPORARY SUPPORTS AS NEEDED. AS SOON AS THESE EXISTING DIRECTIONAL SIGNS COME DOWN PERMANENTLY, THE CONTRACTOR SHALL HAVE UP AT LEAST ONE NEW TEMPORARY "ADVANCE GUIDE SIGN" AND ONE NEW TEMPORARY "EXIT DIRECTIONAL SIGN" AT ALL EXIT RAMPS. THESE SIGNS ARE TO BE MAINTAINED WITHIN CLEAR VIEW OF THE PUBLIC ON THE RIGHT SIDE OF THE HIGHWAY AND SHALL BE REPLACED IF DAMAGED, DURING ALL PHASES OF CONSTRUCTION, AS DIRECTED BY THE ENGINEER.
- (2) THE SIZE OF THESE NEW TEMPORARY SIGNS WILL BE DETERMINED BY THE MESSAGE. THE MESSAGE SHALL BE THE SAME AS THE EXISTING SIGN THAT THESE NEW TEMPORARY SIGNS WILL BE REPLACING. THE LETTER SIZE SHALL BE A MINIMUM OF 8 INCH, "D" UPPER CASE LETTER. THE DIRECTIONAL ARROW WILL BE A "B" ARROW AT A 45 DEGREE ANGLE (SAME ANGLE AS THE EXISTING ARROW). THE MATERIAL SHALL BE 0.100 INCH SHEET ALUMINUM; THE COLOR SHALL BE A REFLECTIVE GREEN BACKGROUND WITH REFLECTIVE WHITE COPY.
- (3) ALL WORK AND MATERIAL TO MAKE THESE NEW TEMPORARY DIRECTIONAL SIGNS ALONG WITH ADEQUATE SUPPORTS AND TO MOVE THEM AS NEEDED DURING EACH PHASE OF CONSTRUCTION WILL BE PAID FOR UNDER ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F. , AS DIRECTED BY THE ENGINEER.
- (4) SOME OF THESE DIRECTIONAL SIGNS WILL NEED AN INTERSTATE, U.S., OR A STATE HIGHWAY SHIELD, A CARDINAL DIRECTION, AND A DIRECTION ARROW TO ACCOMPANY THE DIRECTIONAL SIGN. THESE SIGNS SHALL BE MOUNTED BELOW THE DIRECTIONAL SIGN.

- (5) ALL EXISTING "EMERGENCY REFERENCE MARKERS" AND "HOSPITAL SIGNS" SHALL BE MAINTAINED WITHIN FULL VIEW OF THE MOTORING PUBLIC THROUGHOUT ALL PHASES OF CONSTRUCTION. ALL WORK IN MOVING AND TEMPORARY SUPPORTS SHALL BE PAID FOR UNDER ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F. .

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

SPECIAL NOTES

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

ENVIRONMENTAL SPECIAL NOTES

PROJECT COMMITMENTS

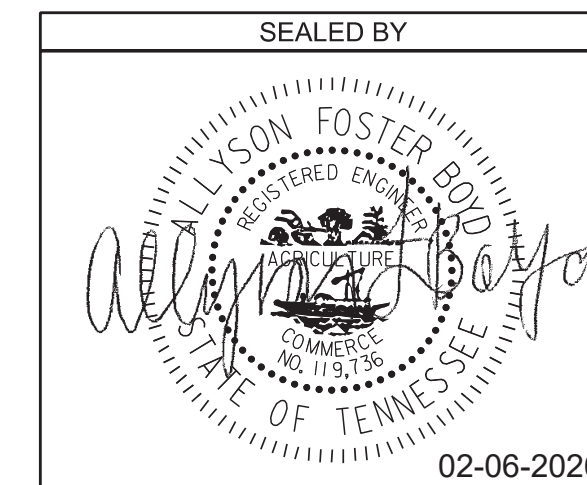
- (1) SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (1) THIS PROJECT IS A BRIDGE REPAIR PROJECT TO RESTORE THE I-40/ SR-158 (JAMES WHITE PARKWAY) BRIDGES OVER NORFOLK SOUTHERN RAILWAY, WILLOW AVE. AND JACKSON AVE. THAT ARE IN NEED OF REPAIRS.

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 2B |
| PS&E | 2026 | 471040-M3-012 | 2C |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES,
SPECIAL NOTES,
AND
ENVIRONMENTAL
SPECIAL NOTES

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 2A1 |
| PS&E | 2026 | 471040-M3-012 | 2F |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

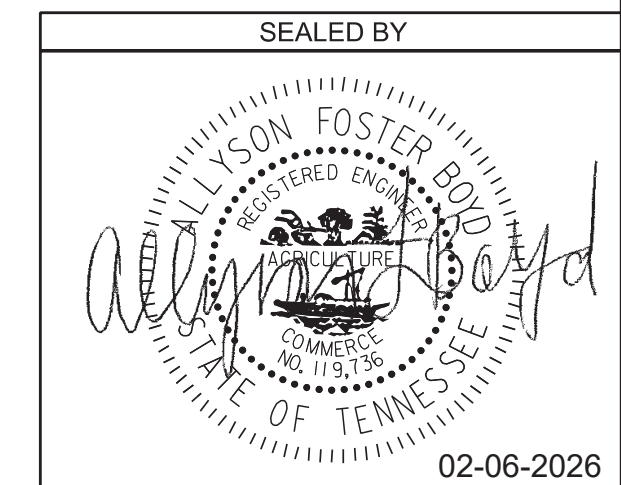
| I-40 WB TO HALL OF FAME DRIVE CLOSURE AND DETOUR (SHEETS 4-4B) | | | | |
|--|-----------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W20-1 | ROAD WORK AHEAD | 48X48 | 1 | 16 |
| CUSTOM | DETOUR | 48x60 | 6 | 120 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 182 |

| BAXTER AVENUE TO I-275 SB DETOUR (SHEETS 4K-4L) | | | | |
|---|-------------------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M1-1 | INTERSTATE - 3 DIGITS | 30X24 | 8 | 40 |
| M3-3 | SOUTH - SUPPLEMENTAL | 24X12 | 8 | 16 |
| M4-8 | DETOUR | 24X12 | 8 | 16 |
| M4-8A | END DETOUR | 24X18 | 1 | 3 |
| M5-1L | ADVANCE ARROW - 90 DEGREES LT | 21X15 | 1 | 3 |
| M5-1R | ADVANCE ARROW - 90 DEGREES RT | 21X15 | 2 | 5 |
| M6-1L | DIRECTIONAL ARROW - LT | 21X15 | 1 | 3 |
| M6-1R | DIRECTIONAL ARROW - RT | 21X15 | 2 | 5 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 21X15 | 2 | 5 |
| R3-1 | NO RIGHT TURN | 36X36 | 1 | 9 |
| R3-2 | NO LEFT TURN | 36X36 | 1 | 9 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W20-1 | ROAD WORK AHEAD | 48X48 | 1 | 16 |
| W20-3 | RAMP CLOSED AHEAD | 48X48 | 2 | 32 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 208 |

| HALL OF FAME DRIVE DETOUR (SHEETS 4C-4F) | | | | |
|--|-------------------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M4-8 | DETOUR | 24X12 | 3 | 6 |
| M5-1R | ADVANCE ARROW - 90 DEGREES RT | 21X15 | 1 | 3 |
| M6-1R | DIRECTIONAL ARROW - RT | 21X15 | 1 | 3 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 21X15 | 1 | 3 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| CUSTOM | STREET NAME | 36X12 | 6 | 18 |
| CUSTOM | DETOUR | 48x60 | 5 | 100 |
| TOTAL | | | | 143 |

| I-275 SB TO I-40 EB CLOSURE AND DETOUR (SHEETS 4M-4P) | | | | |
|---|-------------------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M1-1 | INTERSTATE - 2 DIGITS | 36x36 | 5 | 45 |
| M3-2 | EAST- SUPPLEMENTAL | 36X18 | 5 | 23 |
| M4-8 | DETOUR | 30x15 | 4 | 13 |
| M5-1R | ADVANCE ARROW - 90 DEGREES RT | 30X21 | 1 | 5 |
| M6-1R | DIRECTIONAL ARROW - RT | 30X21 | 1 | 5 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 30X21 | 2 | 9 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W20-3 | RAMP CLOSED AHEAD | 48X48 | 1 | 16 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 162 |

| I-40 EB CLOSURE (SHEETS 4G-4J) | | | | |
|---------------------------------|-----------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W20-1 | ROAD WORK AHEAD | 48X48 | 3 | 48 |
| W20-3 | RAMP CLOSED AHEAD | 48X48 | 2 | 32 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 126 |



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| PS&E | 2026 | 471040-M3-012 | 2F1 |
| | | | |
| | | | |

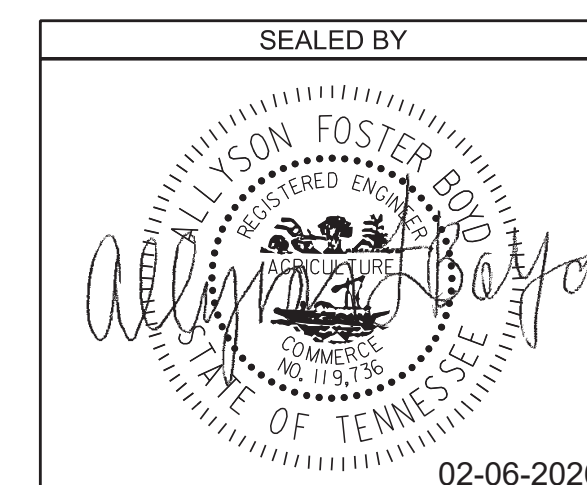
KNOX COUNTY I-40 (JAMES WHITE PKWY)

| HENLEY STREET TO I-40 EB CLOSURE AND DETOUR (SHEETS 4R-4T) TRAFFIC CONTROL SIGN QUANTITIES | | | | |
|---|-------------------------------|-------|----------|-------------|
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M1-1 | INTERSTATE -2 DIGITS | 24X24 | 3 | 12 |
| M3-2 | EAST- SUPPLEMENTAL | 24X12 | 3 | 6 |
| M4-8 | DETOUR | 24X12 | 2 | 4 |
| M5-1L | ADVANCE ARROW - 90 DEGREES LT | 21X15 | 1 | 3 |
| M6-1L | DIRECTIONAL ARROW - LT | 21X15 | 1 | 3 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W20-1 | ROAD WORK AHEAD | 36X36 | 1 | 9 |
| W20-3 | RAMP CLOSED AHEAD | 36X36 | 1 | 9 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 92 |

| NEYLAND DRIVE TO I-40 EB/WB CLOSURE AND DETOUR (SHEETS 5-5D) TRAFFIC CONTROL SIGN QUANTITIES | | | | |
|---|-----------------------------------|-------|----------|-------------|
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M1-1 | INTERSTATE - 2 DIGITS | 36X36 | 2 | 18 |
| M4-8 | DETOUR | 30X15 | 2 | 7 |
| M5-2R | ADVANCE ARROW - 45 DEGREES RT | 30X21 | 1 | 5 |
| M6-2R | DIRECTIONAL ARROW - 45 DEGREES RT | 30X21 | 1 | 5 |
| R11-2 | ROAD CLOSED | 48X30 | 1 | 10 |
| TN-44 | WORKERS PRESENT REDUCE SPEED | 78X60 | 1 | 33 |
| W1-6 | ONE DIRECTION SYMBOL - RIGHT | 48X24 | 1 | 8 |
| W4-2L | LANE ENDS MERGE SYMBOL - LEFT | 48X48 | 1 | 16 |
| W12-1 | DOUBLE ARROW FOR OBSTACLE | 48X48 | 1 | 16 |
| W20-1 | ROAD WORK AHEAD | 48X48 | 1 | 16 |
| W20-1 | ROAD WORK 1 MILE | 48X48 | 1 | 16 |
| W20-5L | LEFT LANE CLOSED 1/2 MILE | 48X48 | 1 | 16 |
| W20-5L | LEFT LANE CLOSED 1500 FEET | 48X48 | 1 | 16 |
| TOTAL | | | | 182 |

| SUMMIT HILL DRIVE TO JAMES WHITE PARKWAY DETOUR (SHEET 4U) TRAFFIC CONTROL SIGN QUANTITIES | | | | |
|---|-------------------------------|-------|----------|-------------|
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M4-8 | DETOUR | 24X12 | 5 | 10 |
| M5-1L | ADVANCE ARROW - 90 DEGREES LT | 21X15 | 1 | 3 |
| M5-1R | ADVANCE ARROW - 90 DEGREES RT | 21X15 | 1 | 3 |
| M6-1L | DIRECTIONAL ARROW - LT | 21X15 | 1 | 3 |
| M6-1R | DIRECTIONAL ARROW - RT | 21X15 | 1 | 3 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 21X15 | 1 | 3 |
| CUSTOM | STREET NAME | 36X12 | 5 | 15 |
| TOTAL | | | | 40 |

| HALL OF FAME DRIVE TO I-40 EB/WB CLOSURE AND DETOUR (SHEETS 5E-5G) TRAFFIC CONTROL SIGN QUANTITIES | | | | |
|---|-------------------------------|-------|----------|-------------|
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| G20-2 | END ROAD WORK | 36X18 | 2 | 9 |
| M1-1 | INTERSTATE - 2 DIGITS | 24X24 | 4 | 16 |
| M4-8 | DETOUR | 24X12 | 8 | 16 |
| M4-8A | END DETOUR | 24X18 | 1 | 3 |
| M5-1R | ADVANCE ARROW - 90 DEGREES RT | 21X15 | 1 | 3 |
| M6-1R | DIRECTIONAL ARROW - RT | 21X15 | 1 | 3 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 21X15 | 4 | 9 |
| R3-2 | NO LEFT TURN | 36X36 | 1 | 9 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 3 | 30 |
| W1-6 | ONE DIRECTION SYMBOL - RIGHT | 48X24 | 1 | 8 |
| W20-1 | ROAD WORK AHEAD | 36X36 | 2 | 18 |
| W20-3 | RAMP CLOSED AHEAD | 36X36 | 3 | 27 |
| TOTAL | | | | 151 |



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| PS&E | 2026 | 471040-M3-012 | 2F2 |
| | | | |

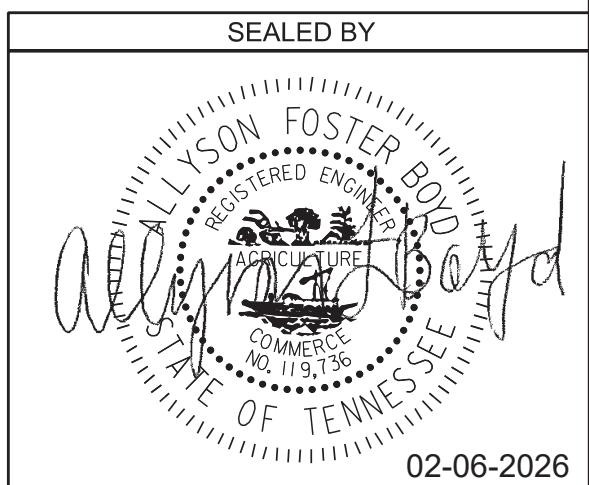
KNOX COUNTY I-40 (JAMES WHITE PKWY)

| MAIN STREET TO I-40 EB/WB CLOSURE AND DETOUR (SHEET 5H) | | | | |
|---|--------------------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M1-1 | INTERSTATE - 2 DIGITS | 24X24 | 2 | 8 |
| M4-8 | DETOUR | 24X12 | 3 | 6 |
| M6-1L | DIRECTIONAL ARROW - LT | 21X15 | 1 | 3 |
| M6-1R | DIRECTIONAL ARROW - RT | 21X15 | 1 | 3 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 21X15 | 1 | 3 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W1-6 | ONE DIRECTION SYMBOL - LEFT | 48X24 | 2 | 16 |
| W4-2R | LANE ENDS MERGE SYMBOL - RIGHT | 36X36 | 1 | 9 |
| W20-1 | ROAD WORK AHEAD | 36X36 | 3 | 27 |
| W20-3 | RAMP CLOSED AHEAD | 36X36 | 1 | 9 |
| W20-5 | RIGHT LANE CLOSED AHEAD | 36X36 | 2 | 18 |
| CUSTOM | STREET NAME | 36X12 | 2 | 6 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 154 |

| I-40 CLOSURE AND DETOUR (SHEETS 5J-5S) | | | | |
|--|-------------------------------|-------|----------|-------------|
| TRAFFIC CONTROL SIGN QUANTITIES | | | | |
| SIGN NO. | DESCRIPTION | SIZE | QUANTITY | AREA (S.F.) |
| M1-1 | INTERSTATE - 2 DIGITS | 36X36 | 3 | 27 |
| M4-8 | DETOUR | 30X15 | 3 | 10 |
| M6-3 | DIRECTIONAL ARROW - STRAIGHT | 30X21 | 3 | 14 |
| R1-2 | YIELD | 60X60 | 1 | 25 |
| R11-2 | ROAD CLOSED | 48X30 | 1 | 10 |
| R11-2 MOD. | RAMP CLOSED | 48X30 | 1 | 10 |
| W3-2 | YIELD AHEAD SYMBOL | 48X48 | 1 | 16 |
| W4-2L | LANE ENDS MERGE SYMBOL - LEFT | 48X48 | 2 | 32 |
| W20-1 | ROAD WORK AHEAD | 48X48 | 5 | 80 |
| W20-5L | LEFT LANE CLOSED 1/2 MILE | 48X48 | 2 | 32 |
| W20-5L | LEFT LANE CLOSED 1500 FEET | 48X48 | 2 | 32 |
| W20-3 | RAMP CLOSED AHEAD | 48X48 | 1 | 16 |
| CUSTOM | RAMP CLOSURE DURATION | 72X72 | 1 | 36 |
| TOTAL | | | | 340 |

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

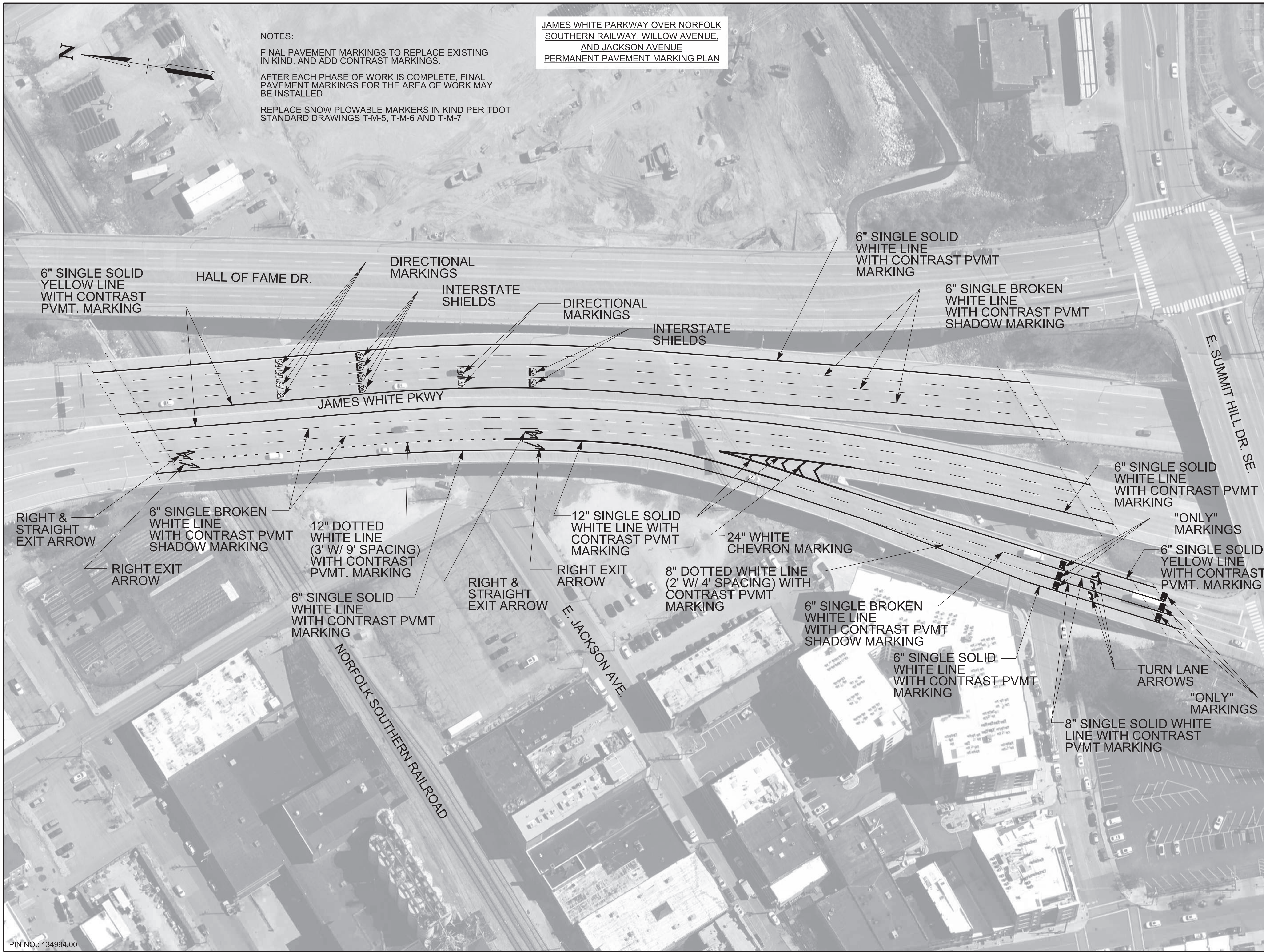
TABULATED
QUANTITIES

**JAMES WHITE PARKWAY OVER NORFOLK SOUTHERN RAILWAY, WILLOW AVENUE, AND JACKSON AVENUE
PERMANENT PAVEMENT MARKING PLAN**

NOTES:
FINAL PAVEMENT MARKINGS TO REPLACE EXISTING IN KIND, AND ADD CONTRAST MARKINGS.
AFTER EACH PHASE OF WORK IS COMPLETE, FINAL PAVEMENT MARKINGS FOR THE AREA OF WORK MAY BE INSTALLED.
REPLACE SNOW PLOWABLE MARKERS IN KIND PER TDOT STANDARD DRAWINGS T-M-5, T-M-6 AND T-M-7.

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 3 |
| PS&E | 2026 | 471040-M3-012 | 3 |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



6" SINGLE SOLID YELLOW LINE WITH CONTRAST PVMT. MARKING

HALL OF FAME DR.

DIRECTIONAL MARKINGS

INTERSTATE SHIELDS

DIRECTIONAL MARKINGS

INTERSTATE SHIELDS

6" SINGLE SOLID WHITE LINE WITH CONTRAST PVMT MARKING

6" SINGLE BROKEN WHITE LINE WITH CONTRAST PVMT SHADOW MARKING

JAMES WHITE PKWY

E. SUMMIT HILL DR. S.E.

6" SINGLE SOLID WHITE LINE WITH CONTRAST PVMT MARKING

"ONLY" MARKINGS

6" SINGLE SOLID YELLOW LINE WITH CONTRAST PVMT. MARKING

RIGHT & STRAIGHT EXIT ARROW

6" SINGLE BROKEN WHITE LINE WITH CONTRAST PVMT SHADOW MARKING

12" DOTTED WHITE LINE (3' W/ 9' SPACING) WITH CONTRAST PVMT. MARKING

12" SINGLE SOLID WHITE LINE WITH CONTRAST PVMT MARKING

24" WHITE CHEVRON MARKING

8" DOTTED WHITE LINE (2' W/ 4' SPACING) WITH CONTRAST PVMT MARKING

RIGHT EXIT ARROW

6" SINGLE SOLID WHITE LINE WITH CONTRAST PVMT MARKING

RIGHT & STRAIGHT EXIT ARROW

RIGHT EXIT ARROW

6" SINGLE BROKEN WHITE LINE WITH CONTRAST PVMT SHADOW MARKING

6" SINGLE SOLID WHITE LINE WITH CONTRAST PVMT MARKING

TURN LANE ARROWS

"ONLY" MARKINGS

8" SINGLE SOLID WHITE LINE WITH CONTRAST PVMT MARKING

NORFOLK SOUTHERN RAILROAD

E. JACKSON AVE.

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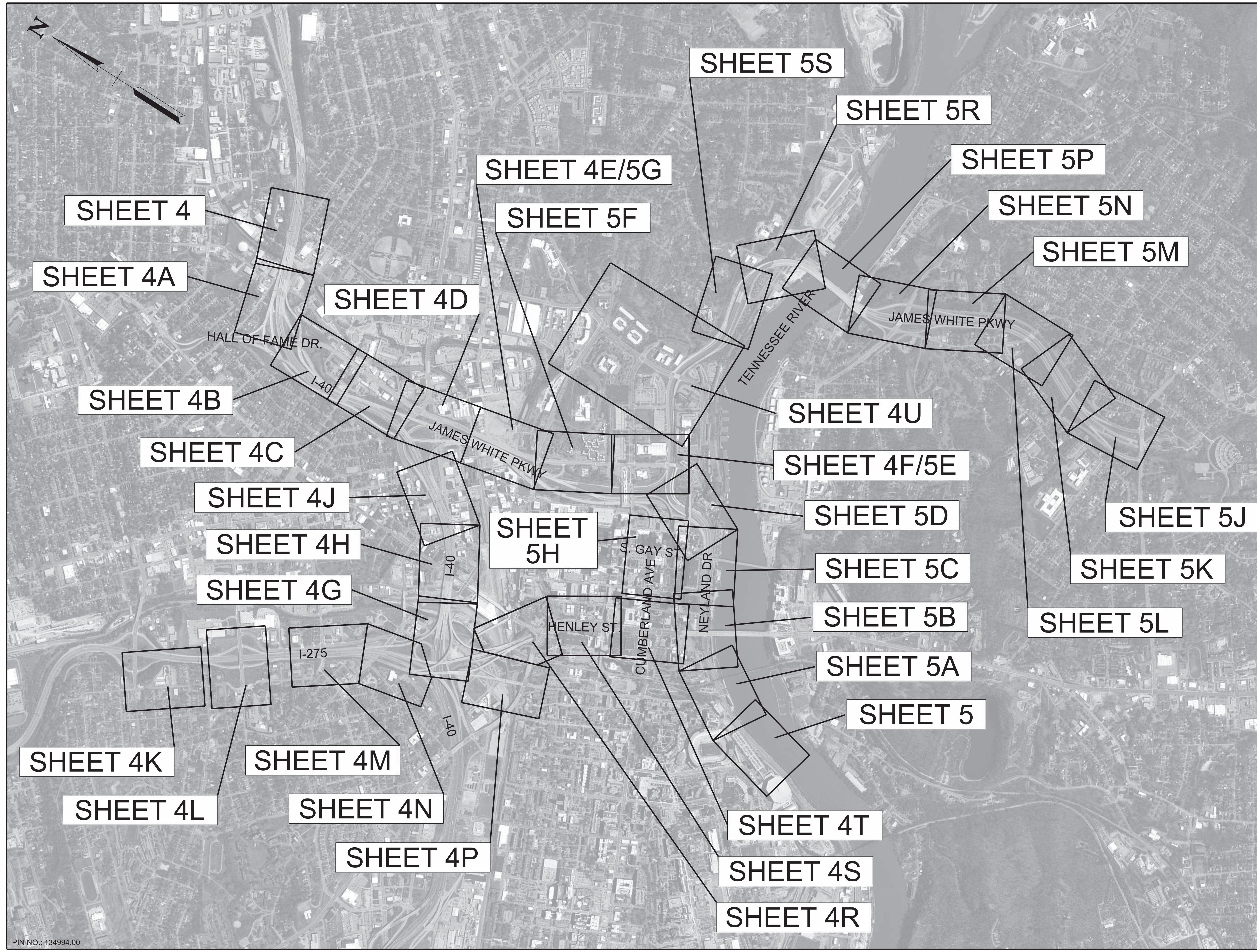
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PERMANENT PAVEMENT MARKING PLAN
BR. NO. 47-10040-19.30

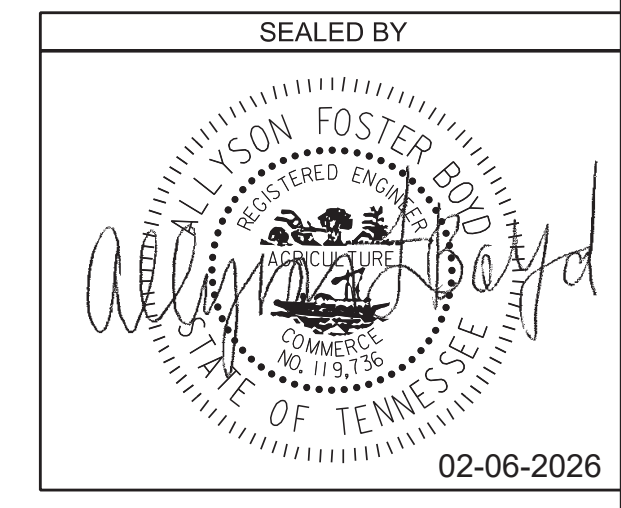
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|-------------|------|-------------------------|-----------|
| L&G | 2025 | 471040-M3-012 | 3A |
| PS&E | 2026 | 471040-M3-012 | 3A |
| KNOX COUNTY | | I-40 (JAMES WHITE PKWY) | |



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STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 KEY MAP
 1"=800'

1/29/2026 11:43:50 AM
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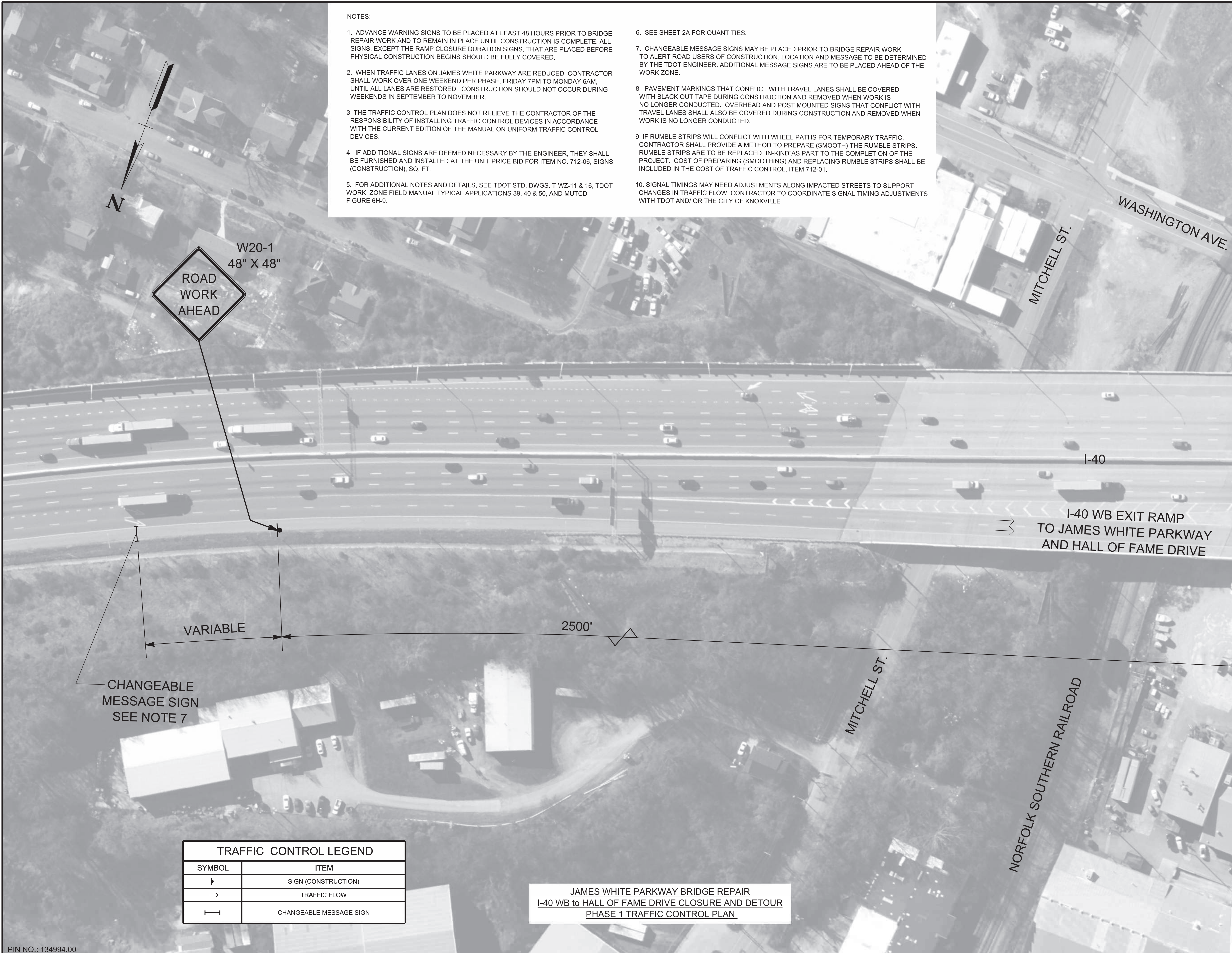
PIN NO.: 134994.00

NOTES:

1. ADVANCE WARNING SIGNS TO BE PLACED AT LEAST 48 HOURS PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. ALL SIGNS, EXCEPT THE RAMP CLOSURE DURATION SIGNS, THAT ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS SHOULD BE FULLY COVERED.
2. WHEN TRAFFIC LANES ON JAMES WHITE PARKWAY ARE REDUCED, CONTRACTOR SHALL WORK OVER ONE WEEKEND PER PHASE, FRIDAY 7PM TO MONDAY 6AM, UNTIL ALL LANES ARE RESTORED. CONSTRUCTION SHOULD NOT OCCUR DURING WEEKENDS IN SEPTEMBER TO NOVEMBER.
3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWGS. T-WZ-11 & 16, TDOT WORK ZONE FIELD MANUAL TYPICAL APPLICATIONS 39, 40 & 50, AND MUTCD FIGURE 6H-9.
6. SEE SHEET 2A FOR QUANTITIES.
7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED. OVERHEAD AND POST MOUNTED SIGNS THAT CONFLICT WITH TRAVEL LANES SHALL ALSO BE COVERED DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.
9. IF RUMBLE STRIPS WILL CONFLICT WITH WHEEL PATHS FOR TEMPORARY TRAFFIC, CONTRACTOR SHALL PROVIDE A METHOD TO PREPARE (SMOOTH) THE RUMBLE STRIPS. RUMBLE STRIPS ARE TO BE REPLACED "IN-KIND" AS PART TO THE COMPLETION OF THE PROJECT. COST OF PREPARING (SMOOTHING) AND REPLACING RUMBLE STRIPS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL, ITEM 712-01.
10. SIGNAL TIMINGS MAY NEED ADJUSTMENTS ALONG IMPACTED STREETS TO SUPPORT CHANGES IN TRAFFIC FLOW. CONTRACTOR TO COORDINATE SIGNAL TIMING ADJUSTMENTS WITH TDOT AND/OR THE CITY OF KNOXVILLE

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4 |
| PS&E | 2026 | 471040-M3-012 | 4 |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



MATCH LINE SEE SHEET NO. 4A

W20-1
48" X 48"
**ROAD
WORK
AHEAD**

CHANGEABLE
MESSAGE SIGN
SEE NOTE 7

VARIABLE

2500'

I-40
I-40 WB EXIT RAMP
TO JAMES WHITE PARKWAY
AND HALL OF FAME DRIVE

| TRAFFIC CONTROL LEGEND | |
|------------------------|-------------------------|
| SYMBOL | ITEM |
| ⬇ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ⎯ | CHANGEABLE MESSAGE SIGN |

**JAMES WHITE PARKWAY BRIDGE REPAIR
 I-40 WB to HALL OF FAME DRIVE CLOSURE AND DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN**

SEALED BY

02-06-2026

**STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION**

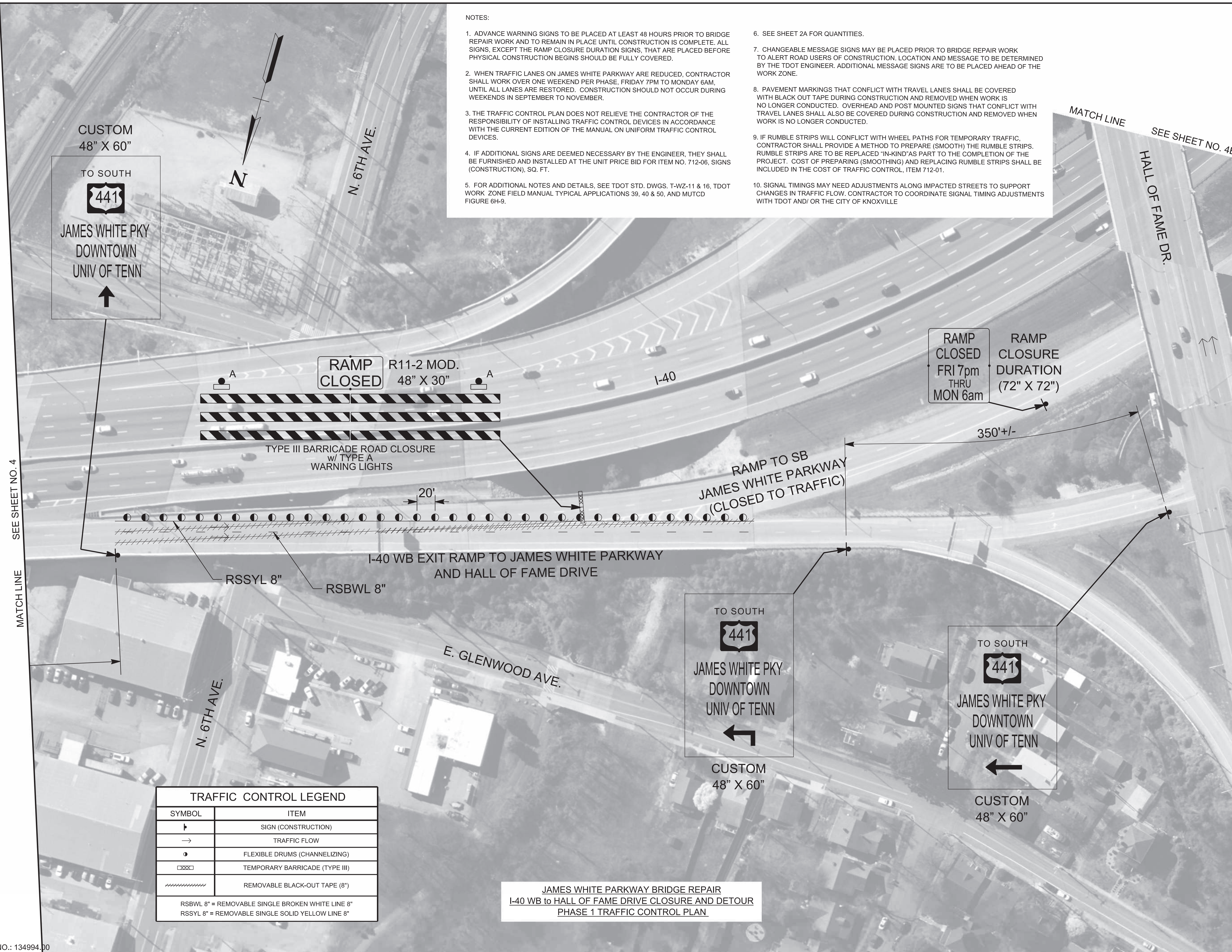
**TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE I
 1"=50'**

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4A |
| PS&E | 2026 | 471040-M3-012 | 4A |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

NOTES:

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| TRAFFIC CONTROL LEGEND | |
|------------------------|--------------------------------|
| ⊣ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| ⊠ | TEMPORARY BARRICADE (TYPE III) |
| //// | REMOVABLE BLACK-OUT TAPE (8") |

RSBWL 8" = REMOVABLE SINGLE BROKEN WHITE LINE 8"
 RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8"

JAMES WHITE PARKWAY BRIDGE REPAIR
 I-40 WB to HALL OF FAME DRIVE CLOSURE AND DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

 02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE I
 1"=50'

1/29/2026 11:45:03 AM
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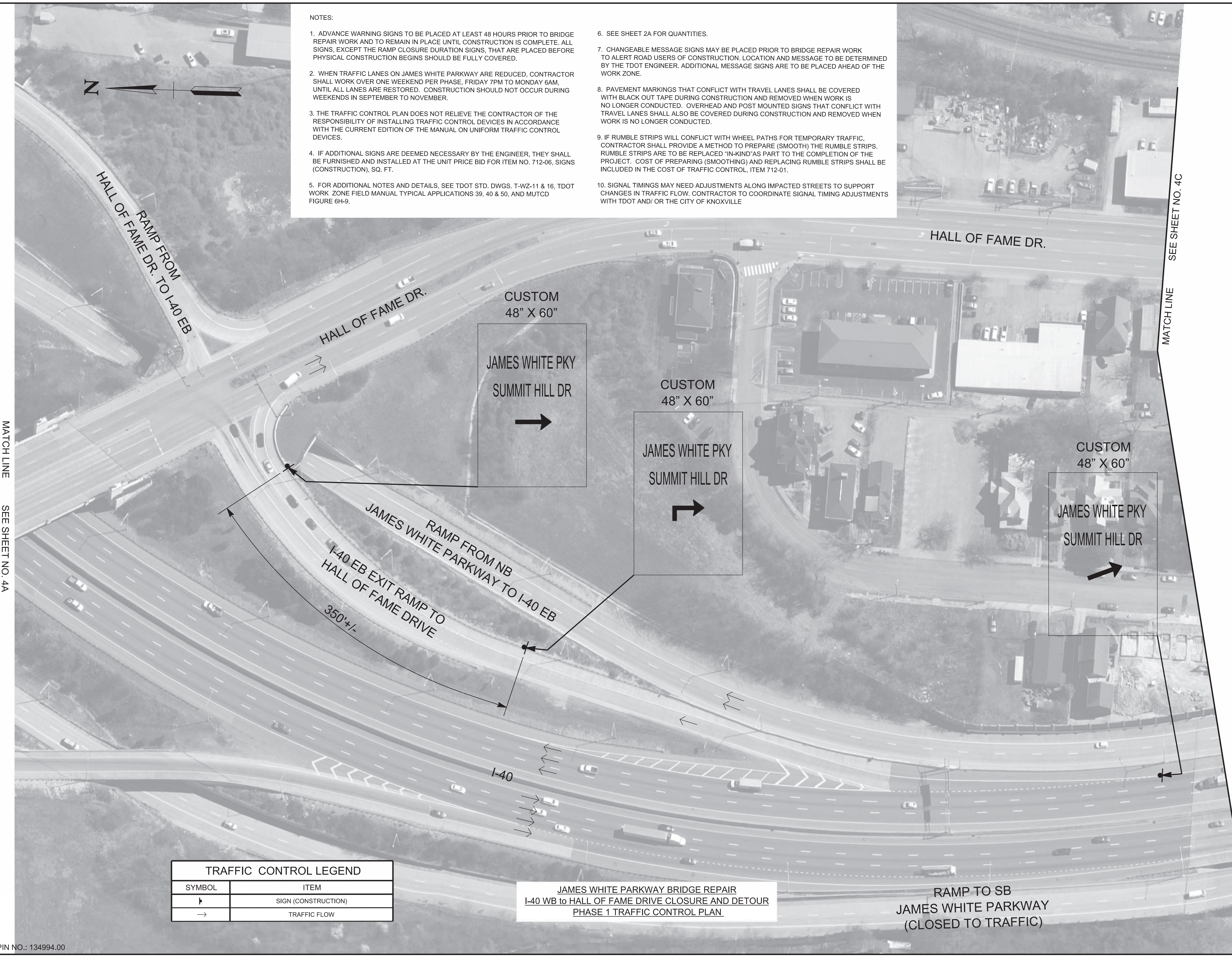
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4B |
| PS&E | 2026 | 471040-M3-012 | 4B |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ▬ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 WB to HALL OF FAME DRIVE CLOSURE AND DETOUR
PHASE 1 TRAFFIC CONTROL PLAN

RAMP TO SB
JAMES WHITE PARKWAY
(CLOSED TO TRAFFIC)

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

1/29/2026 11:46:03 AM
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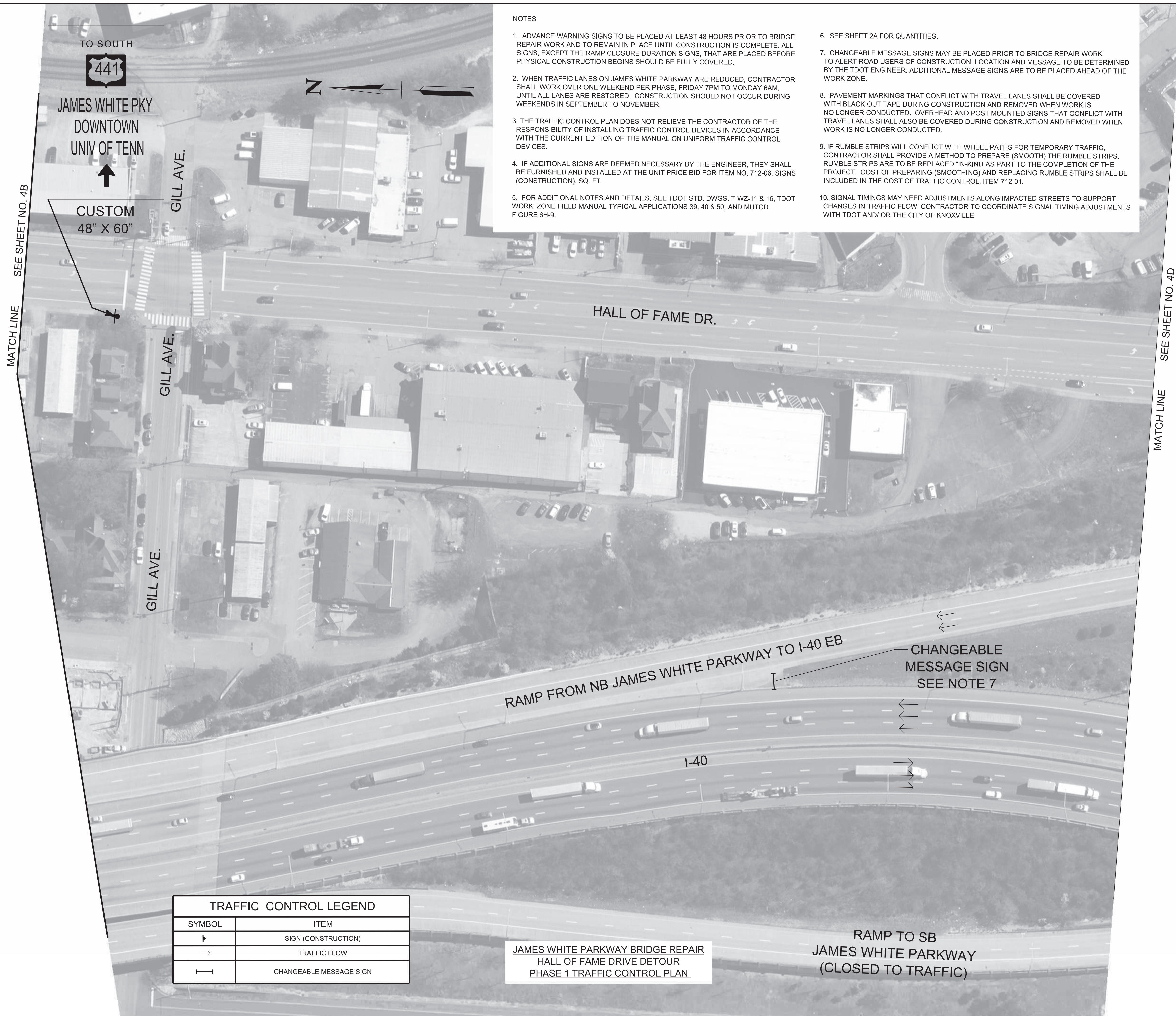
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4C |
| PS&E | 2026 | 471040-M3-012 | 4C |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



NOTES:




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MATCH LINE SEE SHEET NO. 4B

MATCH LINE SEE SHEET NO. 4D


TO SOUTH

 JAMES WHITE PKY
 DOWNTOWN
 UNIV OF TENN

 CUSTOM
 48" X 60"

| TRAFFIC CONTROL LEGEND | |
|---|-------------------------|
| SYMBOL | ITEM |
|  | SIGN (CONSTRUCTION) |
|  | TRAFFIC FLOW |
|  | CHANGEABLE MESSAGE SIGN |

JAMES WHITE PARKWAY BRIDGE REPAIR
 HALL OF FAME DRIVE DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN

RAMP TO SB
 JAMES WHITE PARKWAY
 (CLOSED TO TRAFFIC)

SEALED BY



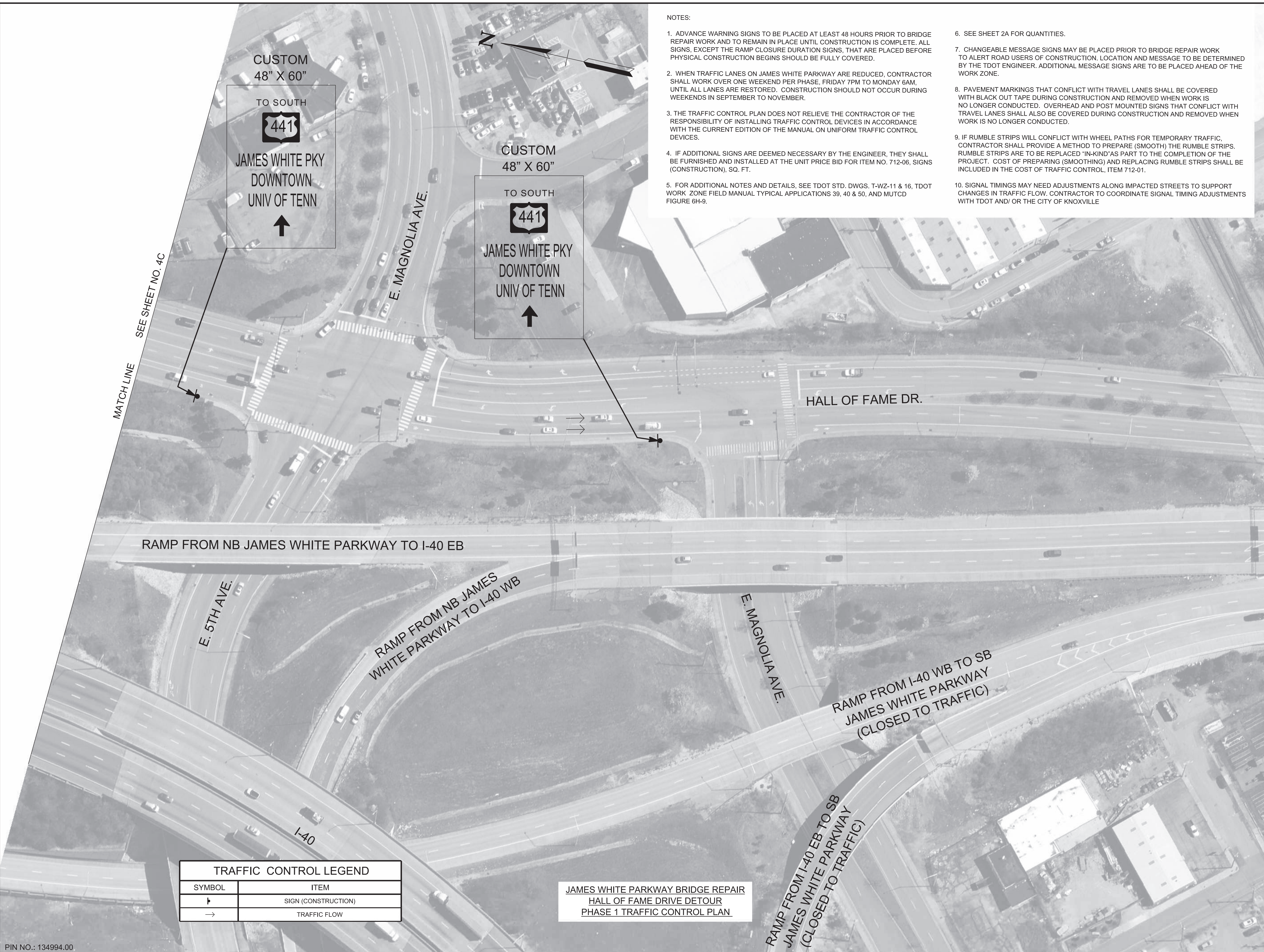
02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE I
 1"=50'

1/29/2026 11:48:26 AM
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PIN NO.: 134994.00



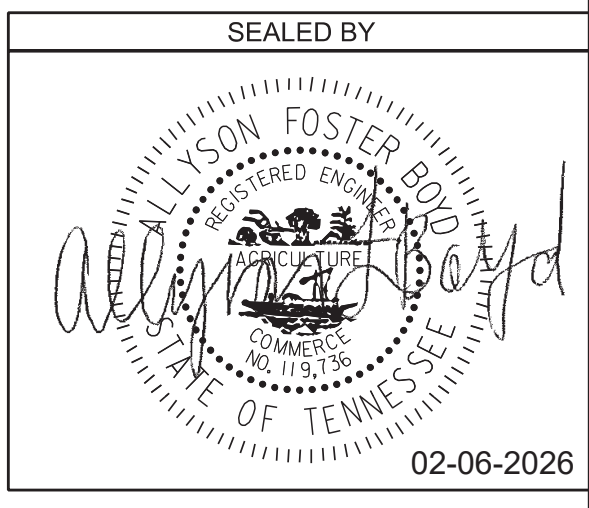
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|-------------|------|-------------------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4D |
| PS&E | 2026 | 471040-M3-012 | 4D |
| KNOX COUNTY | | I-40 (JAMES WHITE PKWY) | |

| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
 HALL OF FAME DRIVE DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

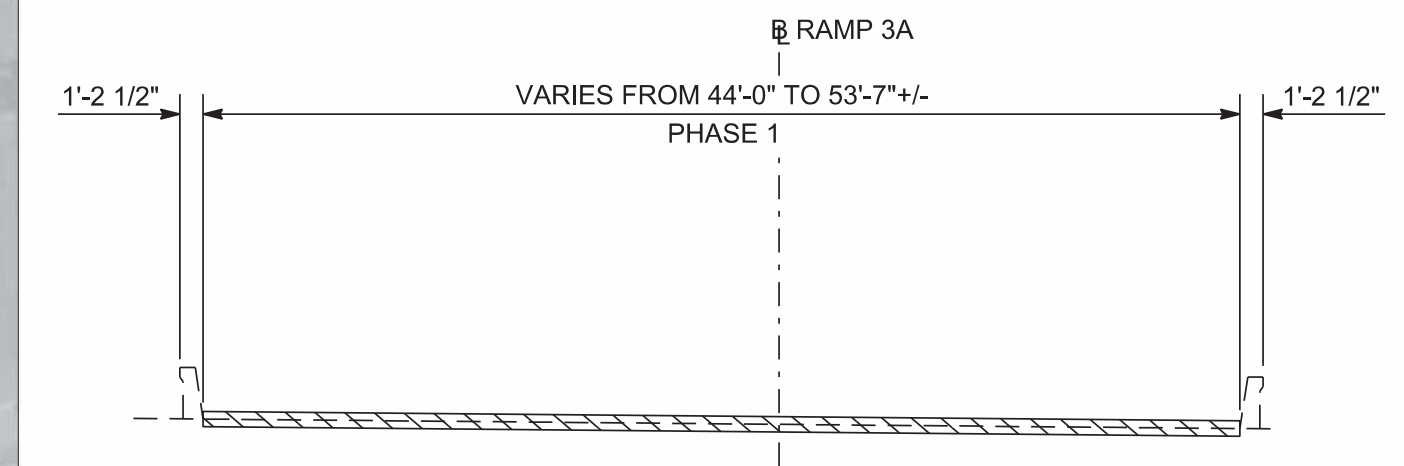
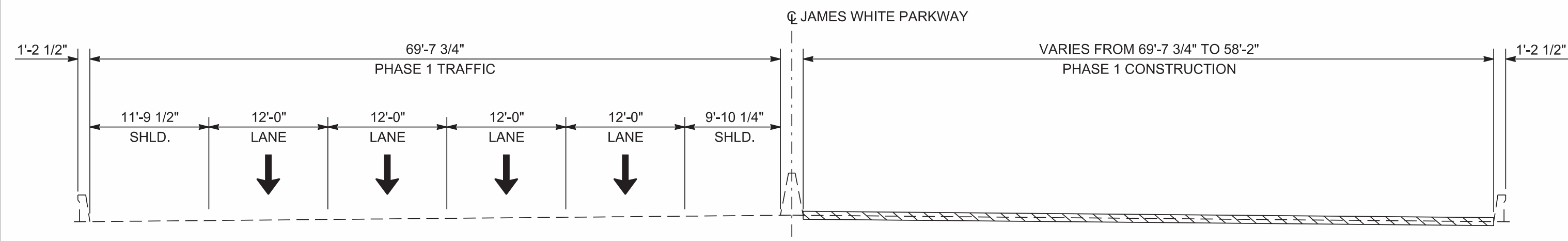
TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE I
 1"=50'

MATCH LINE SEE SHEET NO. 4C

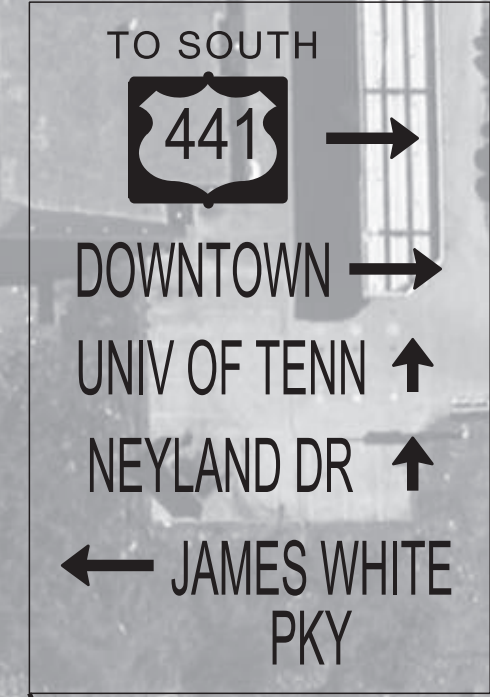
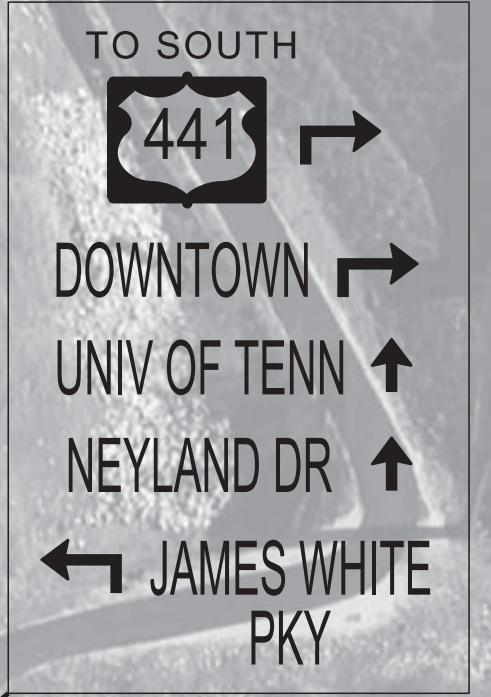
MATCH LINE SEE SHEET NO. 4E

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4E |
| PS&E | 2026 | 471040-M3-012 | 4E |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



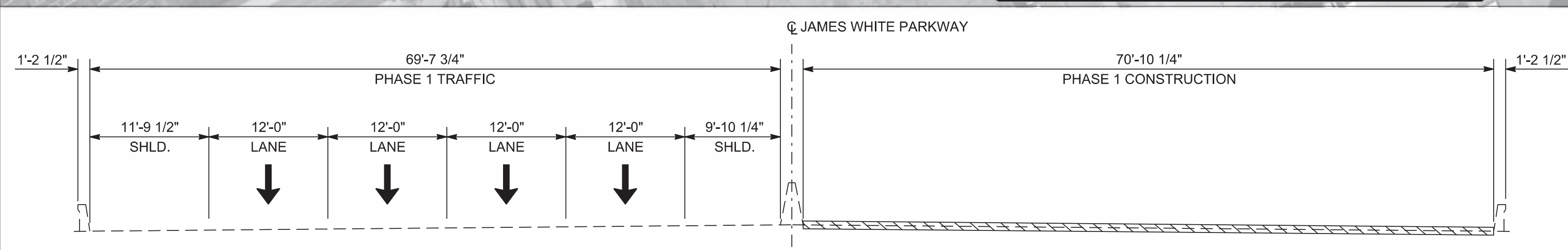
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SEE SHEET 4U FOR
ADDITIONAL SUMMIT
HILL DR TO
JAMES WHITE PKY
DETOUR SIGNAGE



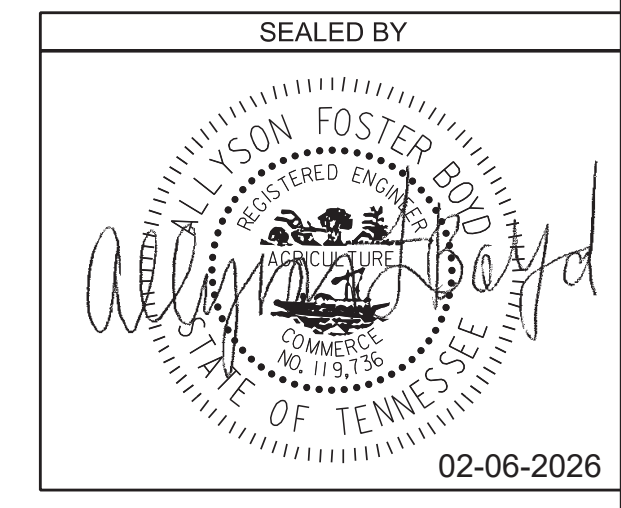
| TRAFFIC CONTROL LEGEND | |
|------------------------|--------------------------------|
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ▨ | TEMPORARY BARRICADE (TYPE III) |
| ▨ | WORK SPACE |



MATCH LINE SEE SHEET NO. 4D

1/29/2026 11:49:26 AM c:\pw_p11\peplwta\d5113642\004E_MOT_JWP_Phase1_FC.dgn

PIN NO.: 134994.00



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

JAMES WHITE PARKWAY BRIDGE REPAIR
HALL OF FAME DRIVE DETOUR
PHASE 1 TRAFFIC CONTROL PLAN

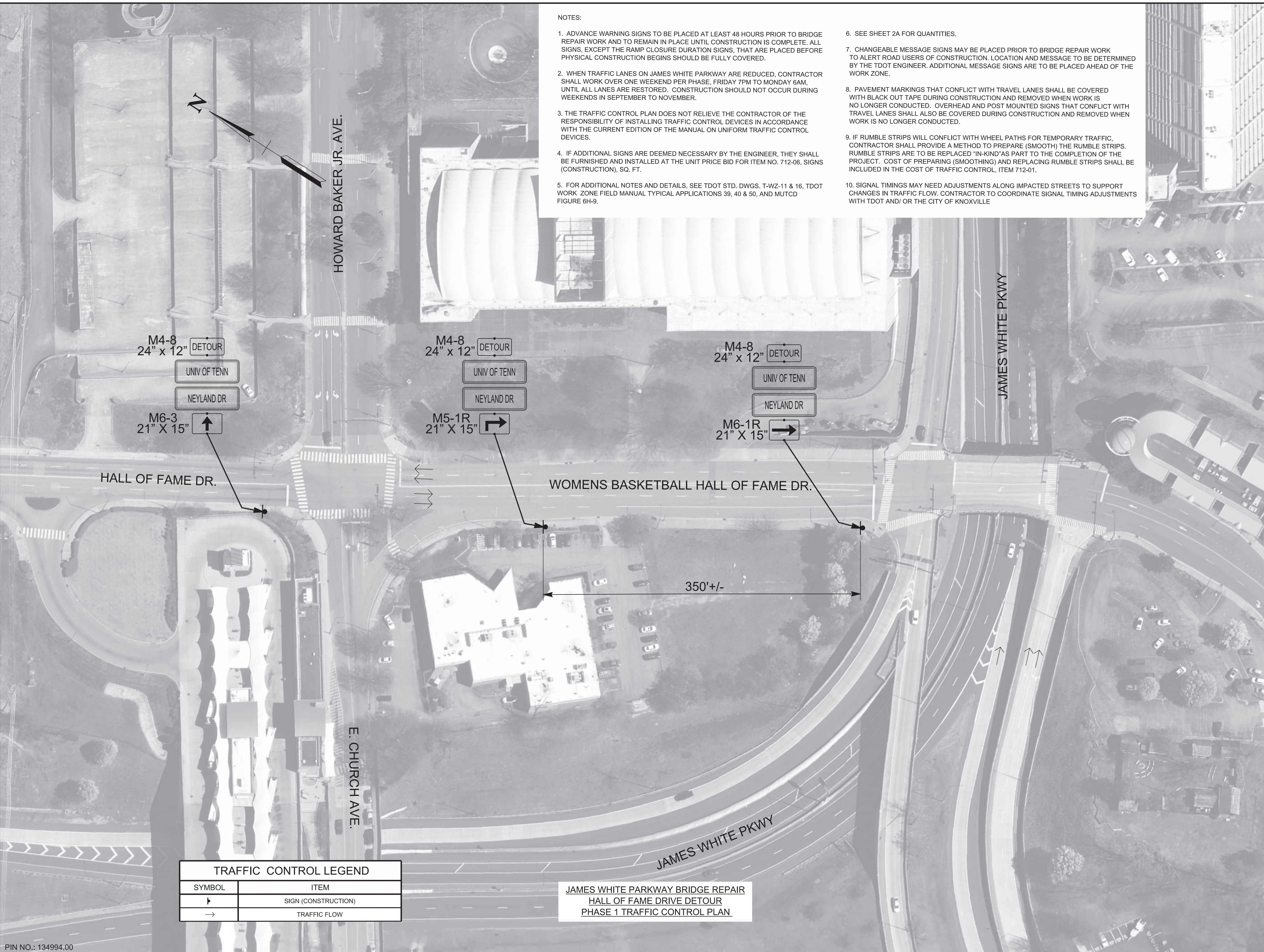
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 2. WHEN TRAFFIC LANES ON JAMES WHITE PARKWAY ARE REDUCED, CONTRACTOR SHALL WORK OVER ONE WEEKEND PER PHASE, FRIDAY 7PM TO MONDAY 6AM, UNTIL ALL LANES ARE RESTORED. CONSTRUCTION SHOULD NOT OCCUR DURING WEEKENDS IN SEPTEMBER TO NOVEMBER.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWGS. T-WZ-11 & 16, TDOT WORK ZONE FIELD MANUAL TYPICAL APPLICATIONS 39, 40 & 50, AND MUTCD FIGURE 6H-9.
 6. SEE SHEET 2A FOR QUANTITIES.
 7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
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 10. SIGNAL TIMINGS MAY NEED ADJUSTMENTS ALONG IMPACTED STREETS TO SUPPORT CHANGES IN TRAFFIC FLOW. CONTRACTOR TO COORDINATE SIGNAL TIMING ADJUSTMENTS WITH TDOT AND/OR THE CITY OF KNOXVILLE

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4F |
| PS&E | 2026 | 471040-M3-012 | 4F |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
 HALL OF FAME DRIVE DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

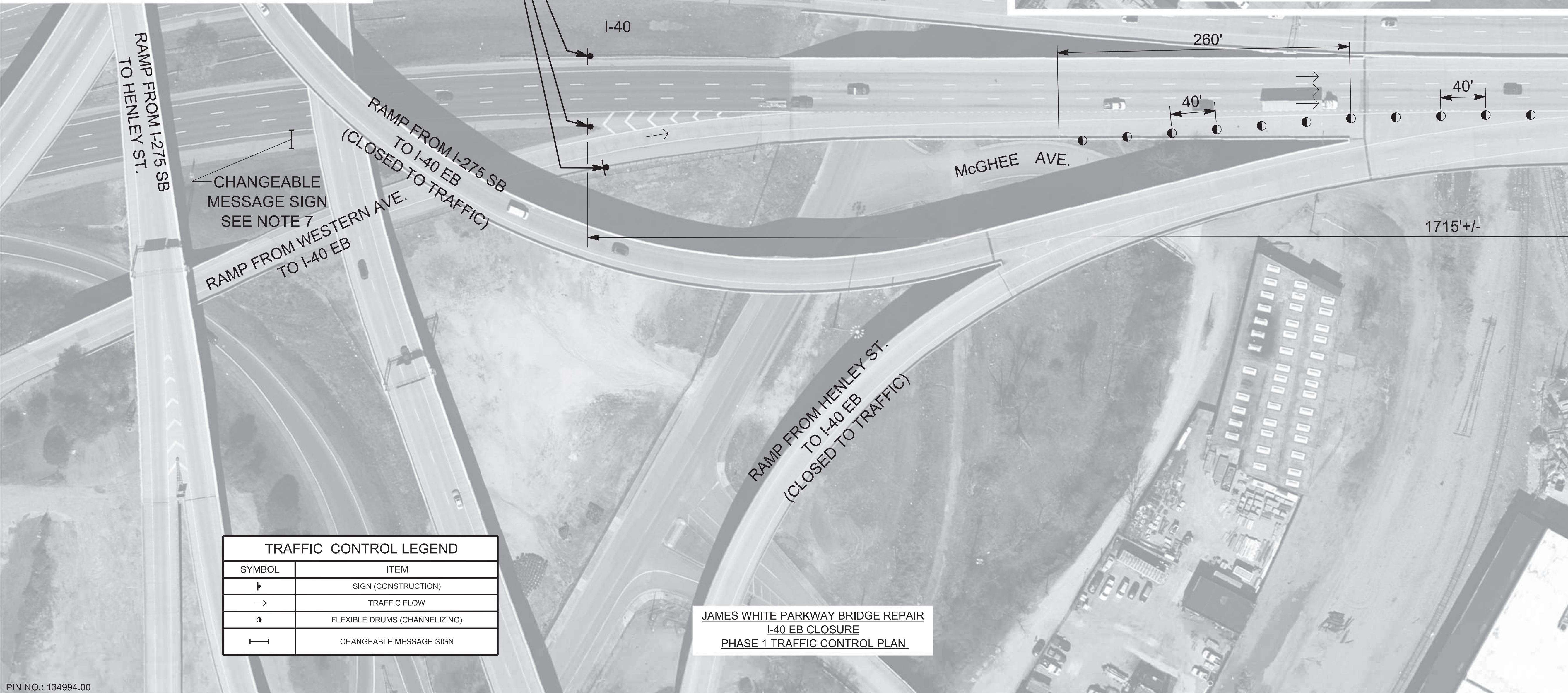
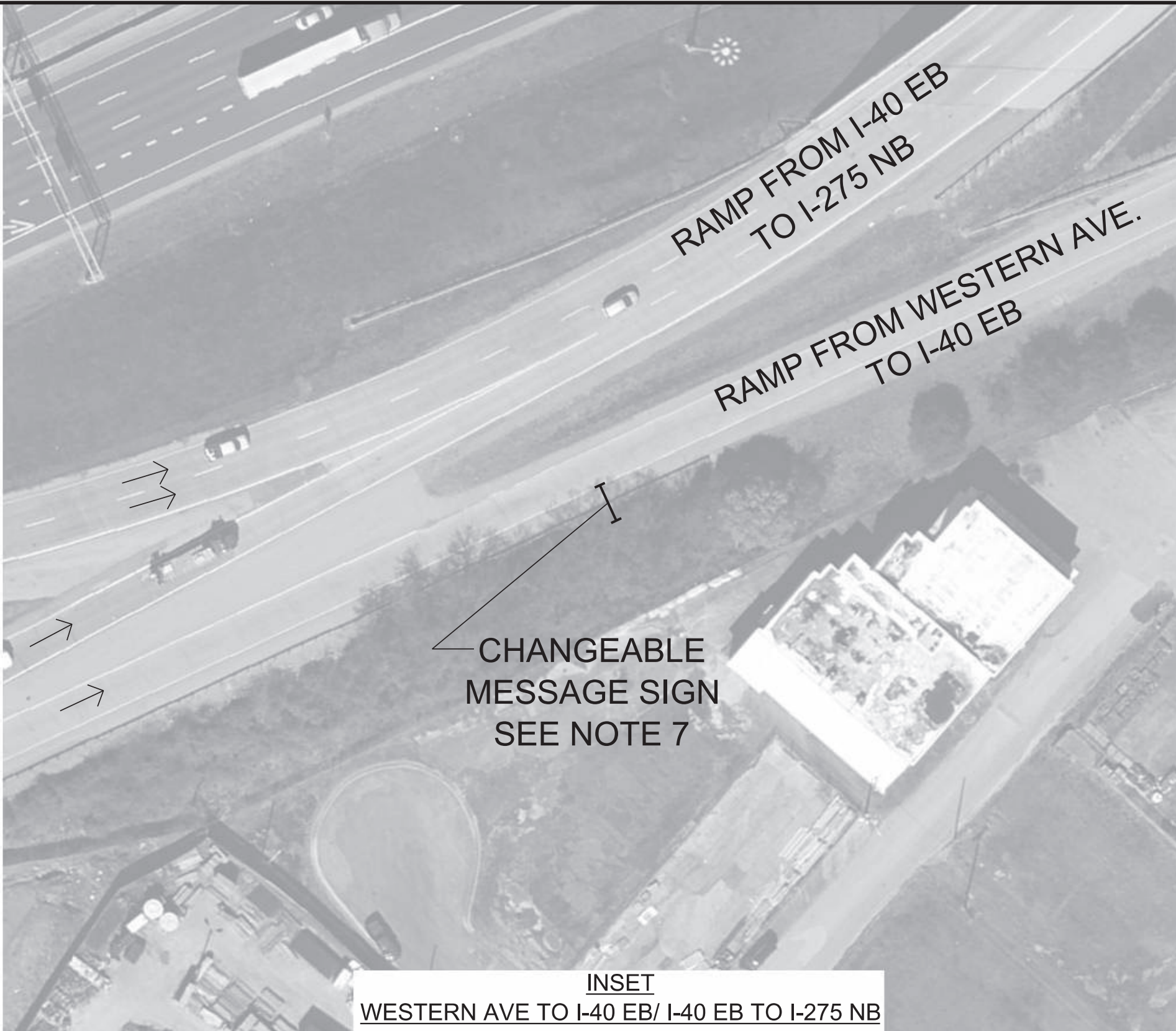
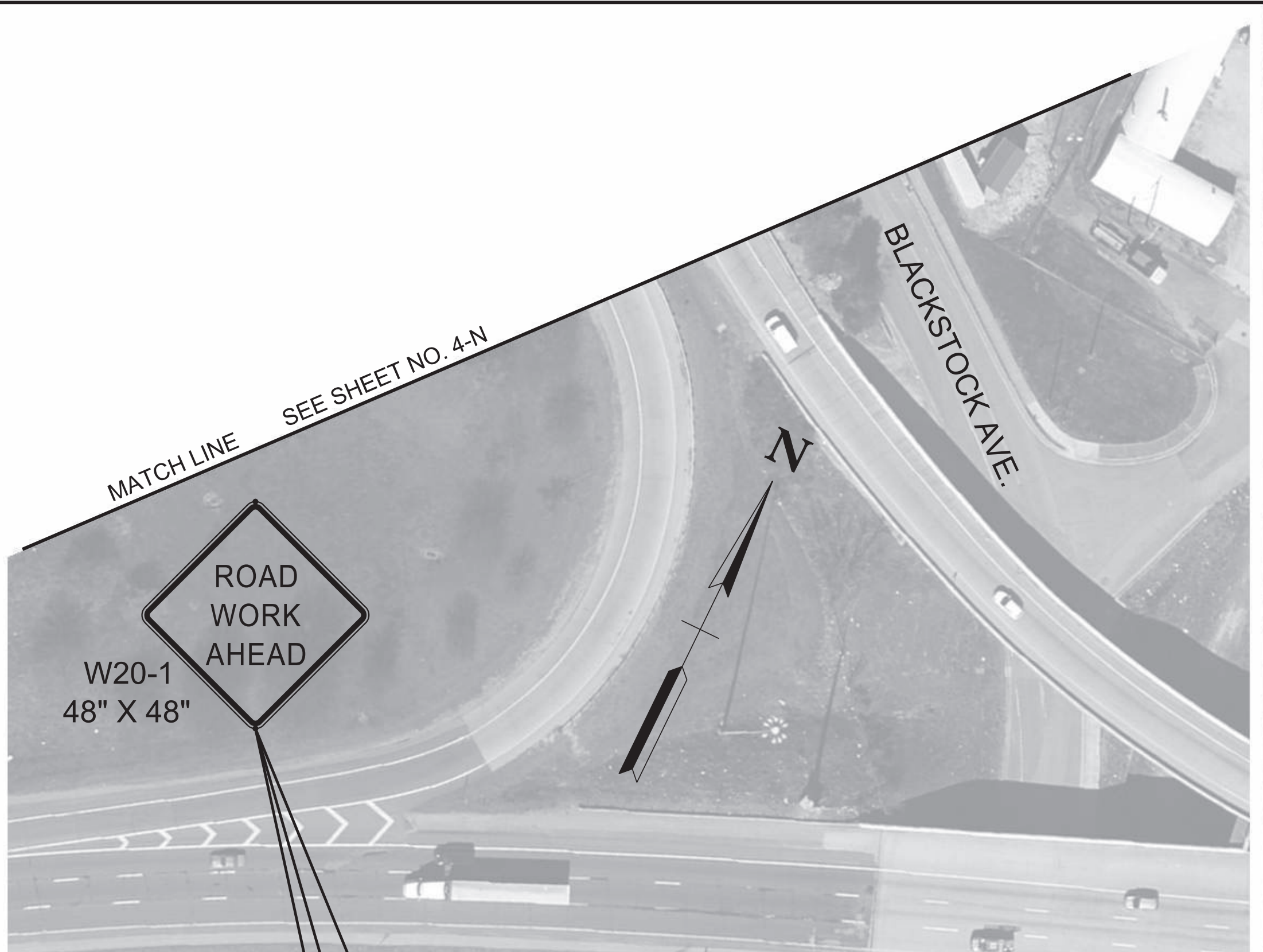
TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE I
 1"=50'

NOTES:

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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4G |
| PS&E | 2026 | 471040-M3-012 | 4G |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



| TRAFFIC CONTROL LEGEND | |
|------------------------|-------------------------------|
| ▶ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| — | CHANGEABLE MESSAGE SIGN |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 EB CLOSURE
PHASE 1 TRAFFIC CONTROL PLAN

MATCH LINE SEE SHEET NO. 4H

SEALED BY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

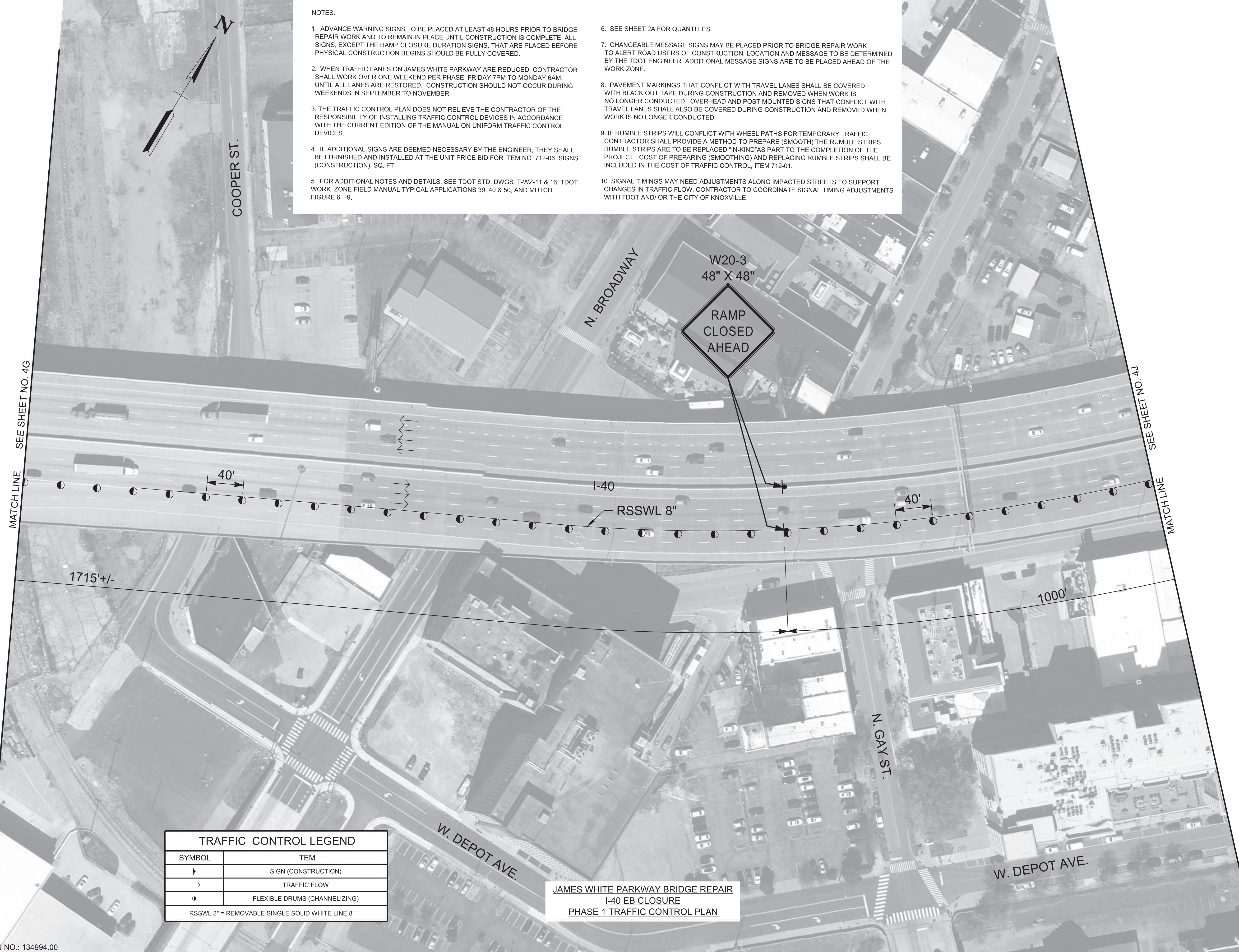
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4H |
| PS&E | 2026 | 471040-M3-012 | 4H |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|---|-------------------------------|
| SYMBOL | ITEM |
| | SIGN (CONSTRUCTION) |
| | TRAFFIC FLOW |
| | FLEXIBLE DRUMS (CHANNELIZING) |
| RSSLW 8" = REMOVABLE SINGLE SOLID WHITE LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 EB CLOSURE
PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

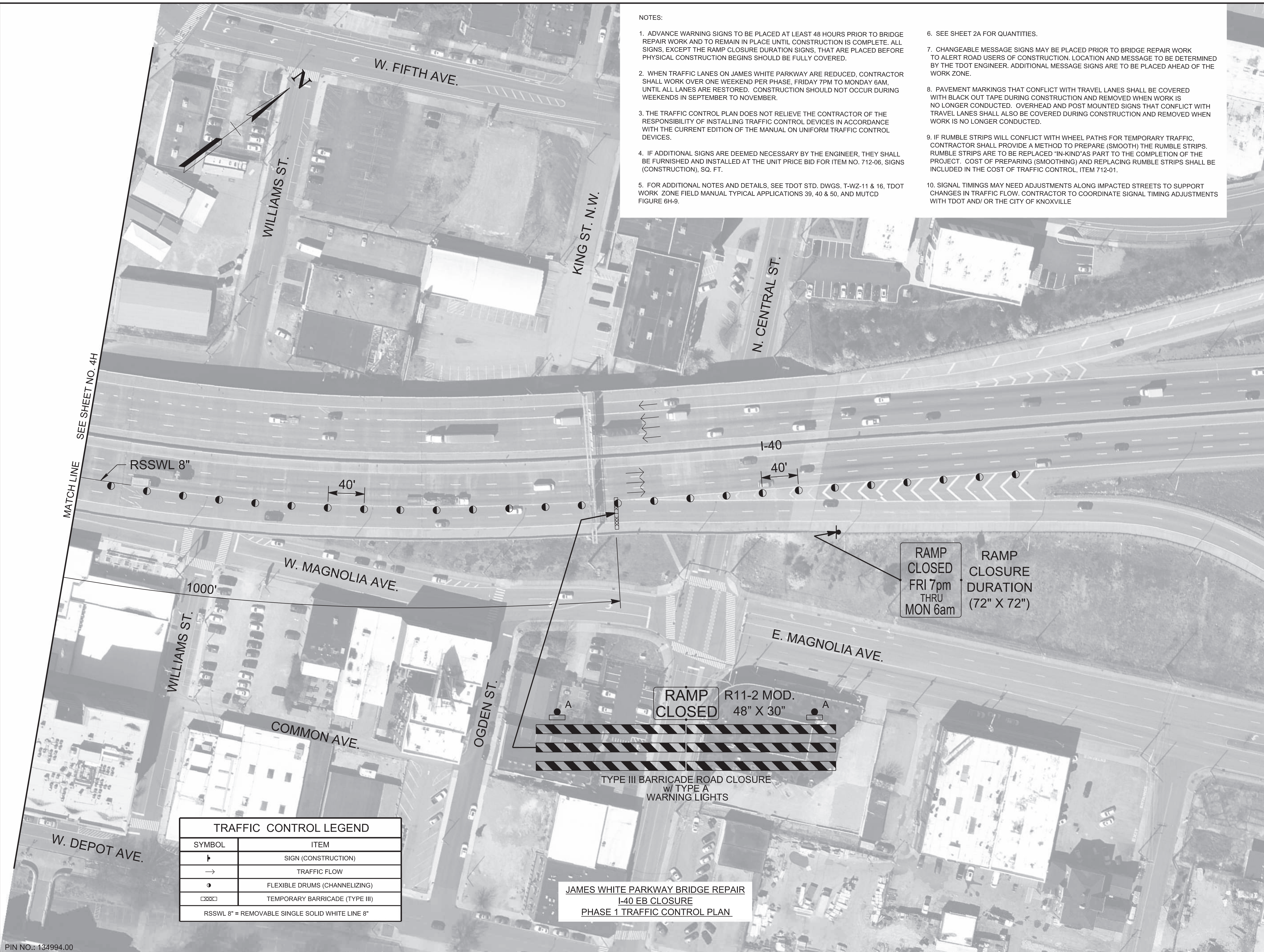
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|-------------|------|---------------|-------------------------|
| L&G | 2025 | 471040-M3-012 | 4J |
| PS&E | 2026 | 471040-M3-012 | 4J |
| KNOX COUNTY | | | I-40 (JAMES WHITE PKWY) |

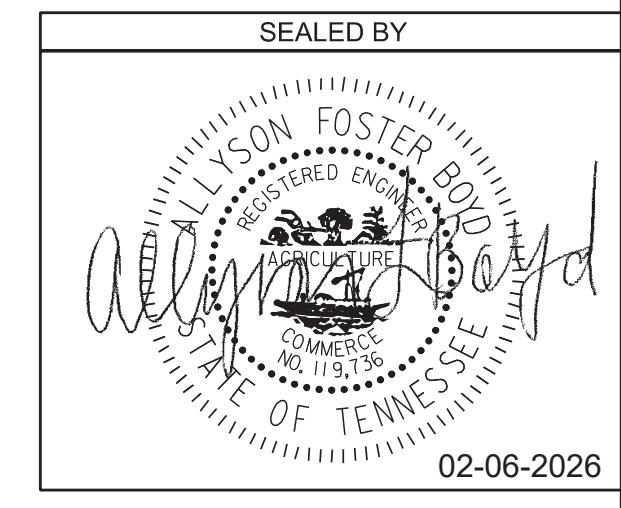
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| TRAFFIC CONTROL LEGEND | |
|---|--------------------------------|
| SYMBOL | ITEM |
| ⊠ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| ▭ | TEMPORARY BARRICADE (TYPE III) |
| RSSWL 8" = REMOVABLE SINGLE SOLID WHITE LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 EB CLOSURE
PHASE 1 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

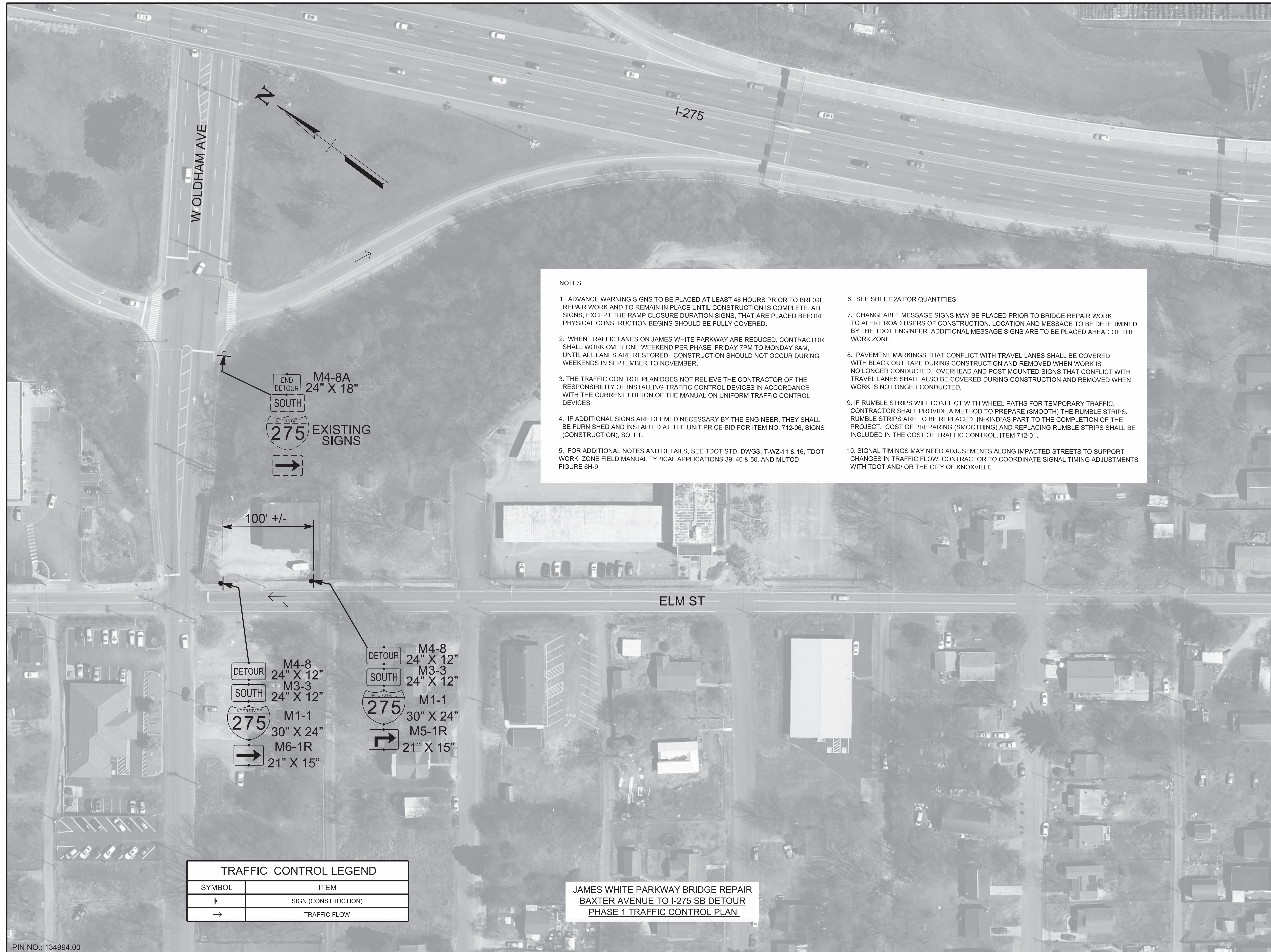
TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4K |
| PS&E | 2026 | 471040-M3-012 | 4K |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

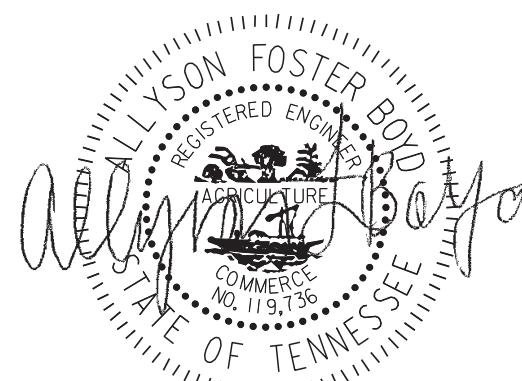


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| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| → | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

**JAMES WHITE PARKWAY BRIDGE REPAIR
BAXTER AVENUE TO I-275 SB DETOUR
PHASE 1 TRAFFIC CONTROL PLAN**

SEALED BY

 02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

1/29/2026 11:57:33 AM
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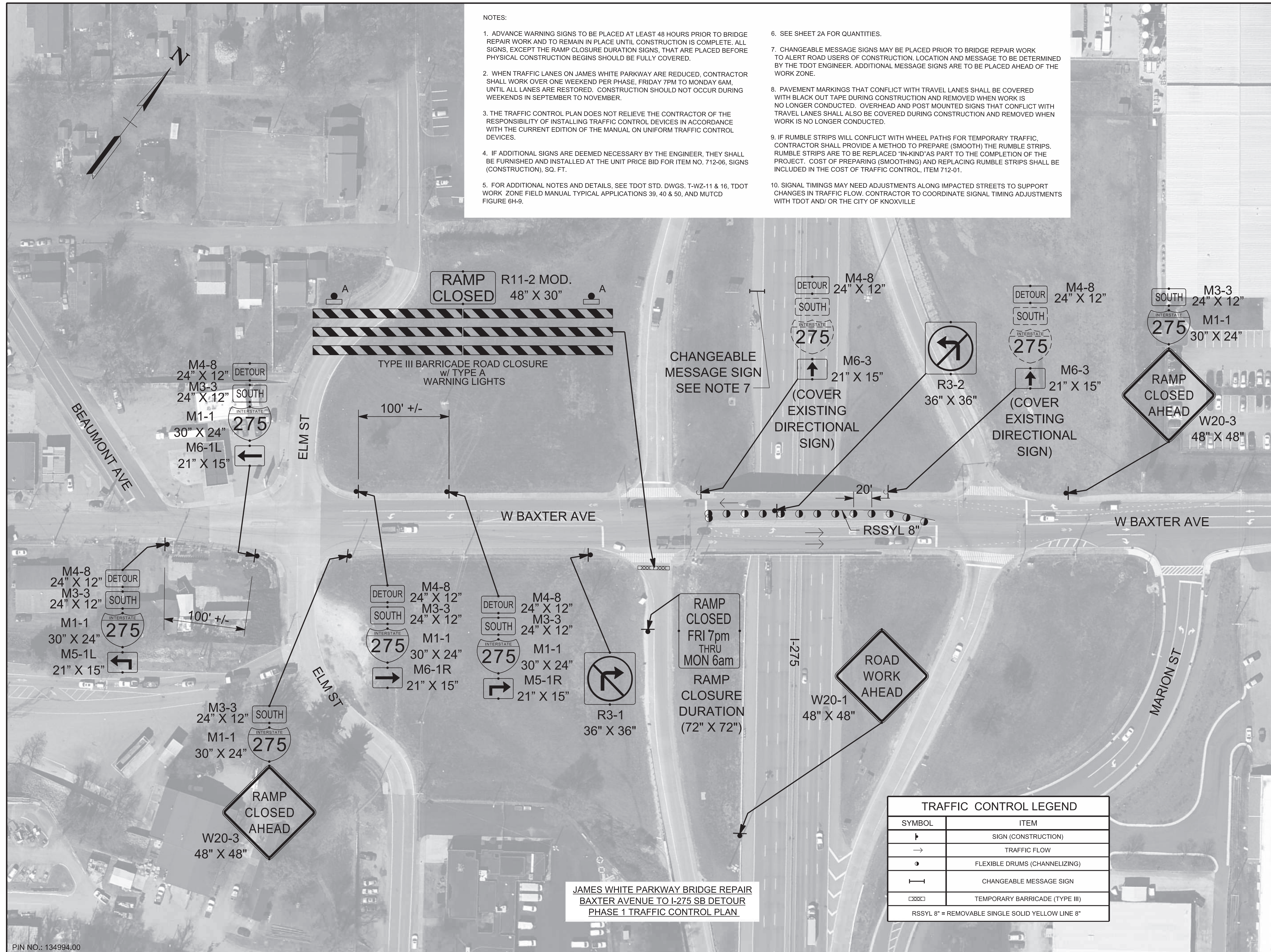
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4L |
| PS&E | 2026 | 471040-M3-012 | 4L |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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JAMES WHITE PARKWAY BRIDGE REPAIR
BAXTER AVENUE TO I-275 SB DETOUR
PHASE 1 TRAFFIC CONTROL PLAN

| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| SYMBOL | ITEM |
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| • | FLEXIBLE DRUMS (CHANNELIZING) |
| — | CHANGEABLE MESSAGE SIGN |
| XXXX | TEMPORARY BARRICADE (TYPE III) |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

SEALED BY

 02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

1/29/2026 11:58:48 AM
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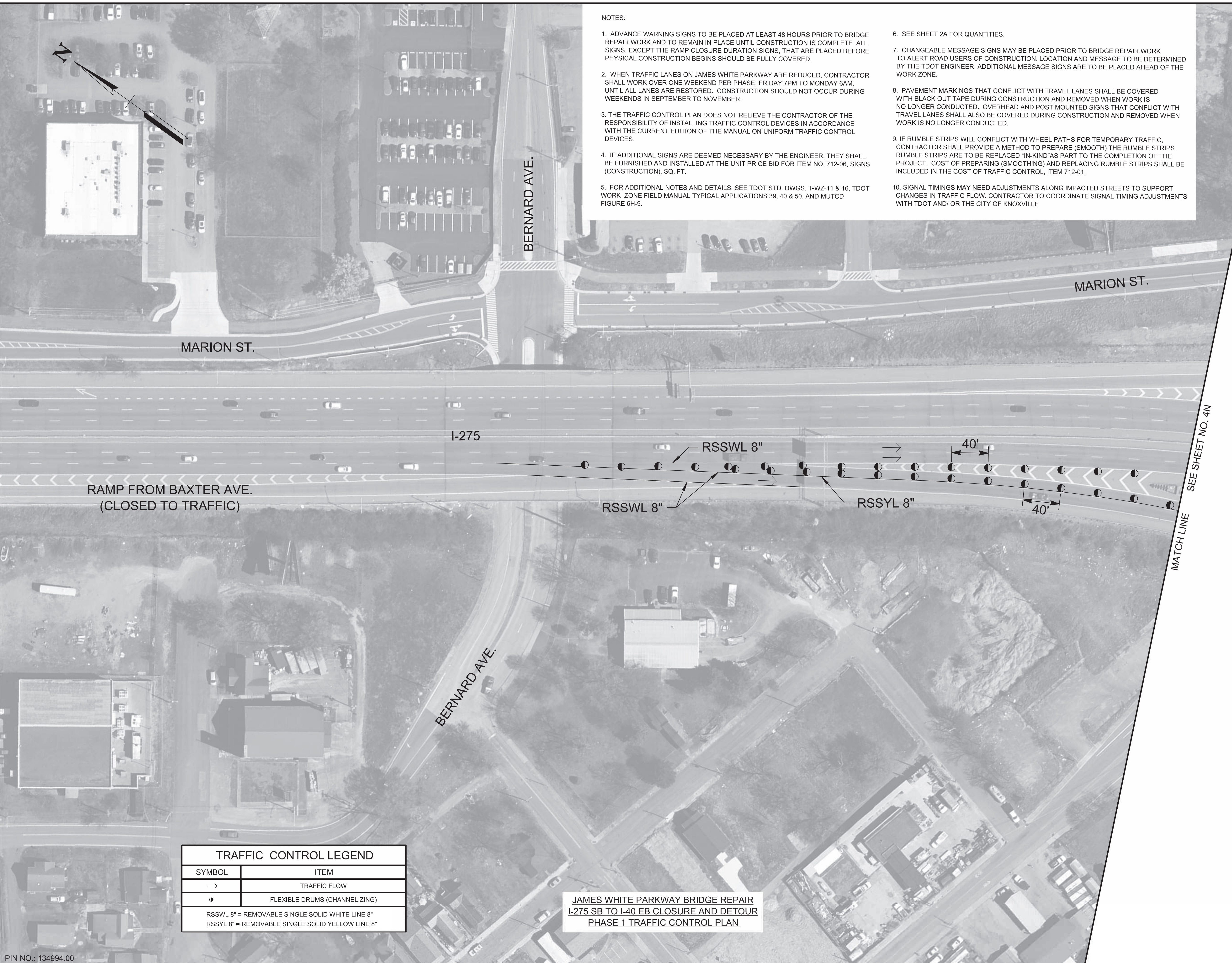
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10. SIGNAL TIMINGS MAY NEED ADJUSTMENTS ALONG IMPACTED STREETS TO SUPPORT CHANGES IN TRAFFIC FLOW, CONTRACTOR TO COORDINATE SIGNAL TIMING ADJUSTMENTS WITH TDOT AND/OR THE CITY OF KNOXVILLE

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4M |
| PS&E | 2026 | 471040-M3-012 | 4M |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



| TRAFFIC CONTROL LEGEND | |
|--|-------------------------------|
| SYMBOL | ITEM |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| RSSLW 8" = REMOVABLE SINGLE SOLID WHITE LINE 8" | |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
 I-275 SB TO I-40 EB CLOSURE AND DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

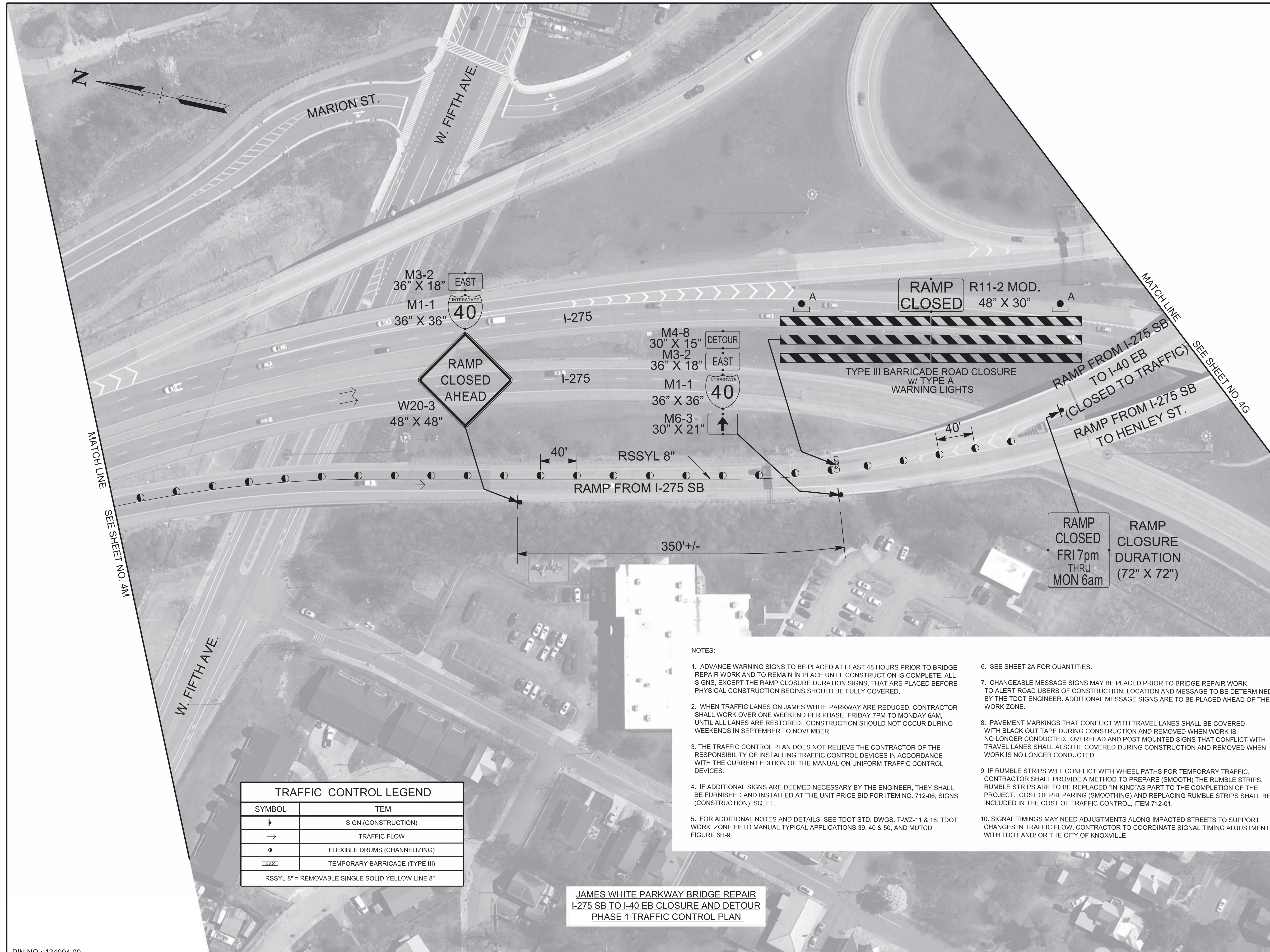
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE I
 1"=50'

MATCH LINE
 SEE SHEET NO. 4N

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4N |
| PS&E | 2026 | 471040-M3-012 | 4N |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



NOTES:

- ADVANCE WARNING SIGNS TO BE PLACED AT LEAST 48 HOURS PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. ALL SIGNS, EXCEPT THE RAMP CLOSURE DURATION SIGNS, THAT ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS SHOULD BE FULLY COVERED.
- WHEN TRAFFIC LANES ON JAMES WHITE PARKWAY ARE REDUCED, CONTRACTOR SHALL WORK OVER ONE WEEKEND PER PHASE, FRIDAY 7PM TO MONDAY 6AM, UNTIL ALL LANES ARE RESTORED. CONSTRUCTION SHOULD NOT OCCUR DURING WEEKENDS IN SEPTEMBER TO NOVEMBER.
- THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
- FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWGS. T-WZ-11 & 16, TDOT WORK ZONE FIELD MANUAL TYPICAL APPLICATIONS 39, 40 & 50, AND MUTCD FIGURE 6H-9.
- SEE SHEET 2A FOR QUANTITIES.
- CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AHEAD OF THE WORK ZONE.
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| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| ▬ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| ▬▬▬ | TEMPORARY BARRICADE (TYPE III) |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

**JAMES WHITE PARKWAY BRIDGE REPAIR
I-275 SB TO I-40 EB CLOSURE AND DETOUR
PHASE 1 TRAFFIC CONTROL PLAN**

SEALED BY

 02-06-2026

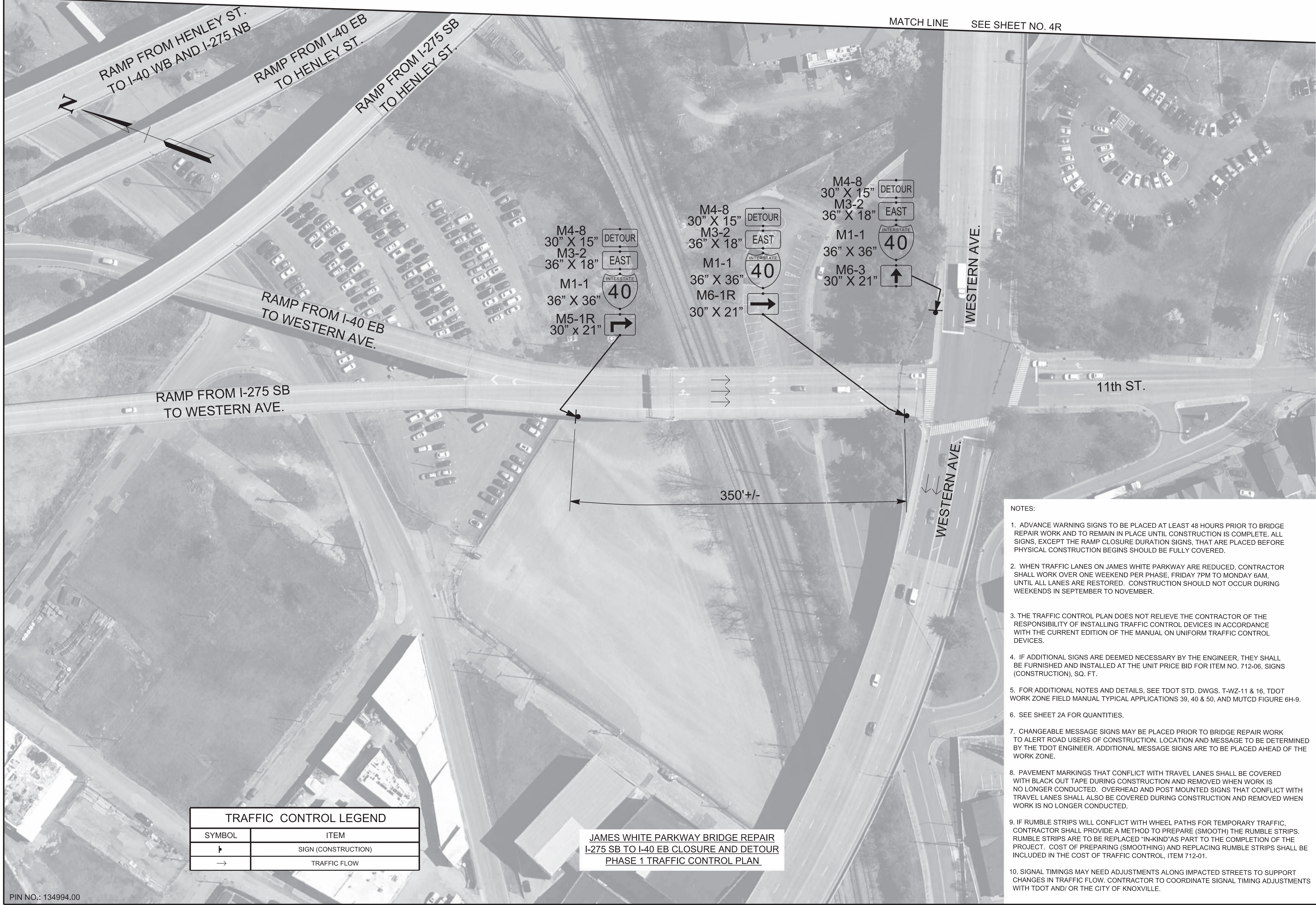
**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'**

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PIN NO.: 134994.00

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|-------------|------|-------------------------|-----------|
| TYPE | YEAR | PROJECT NO. | SHEET NO. |
| L&G | 2025 | 471040-M3-012 | 4P |
| PS&E | 2026 | 471040-M3-012 | 4P |
| KNOX COUNTY | | I-40 (JAMES WHITE PKWY) | |



MATCH LINE SEE SHEET NO. 4R

| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ⬇ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

**JAMES WHITE PARKWAY BRIDGE REPAIR
I-275 SB TO I-40 EB CLOSURE AND DETOUR
PHASE 1 TRAFFIC CONTROL PLAN**

- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED AT LEAST 48 HOURS PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. ALL SIGNS, EXCEPT THE RAMP CLOSURE DURATION SIGNS, THAT ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS SHOULD BE FULLY COVERED.
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SEALED BY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

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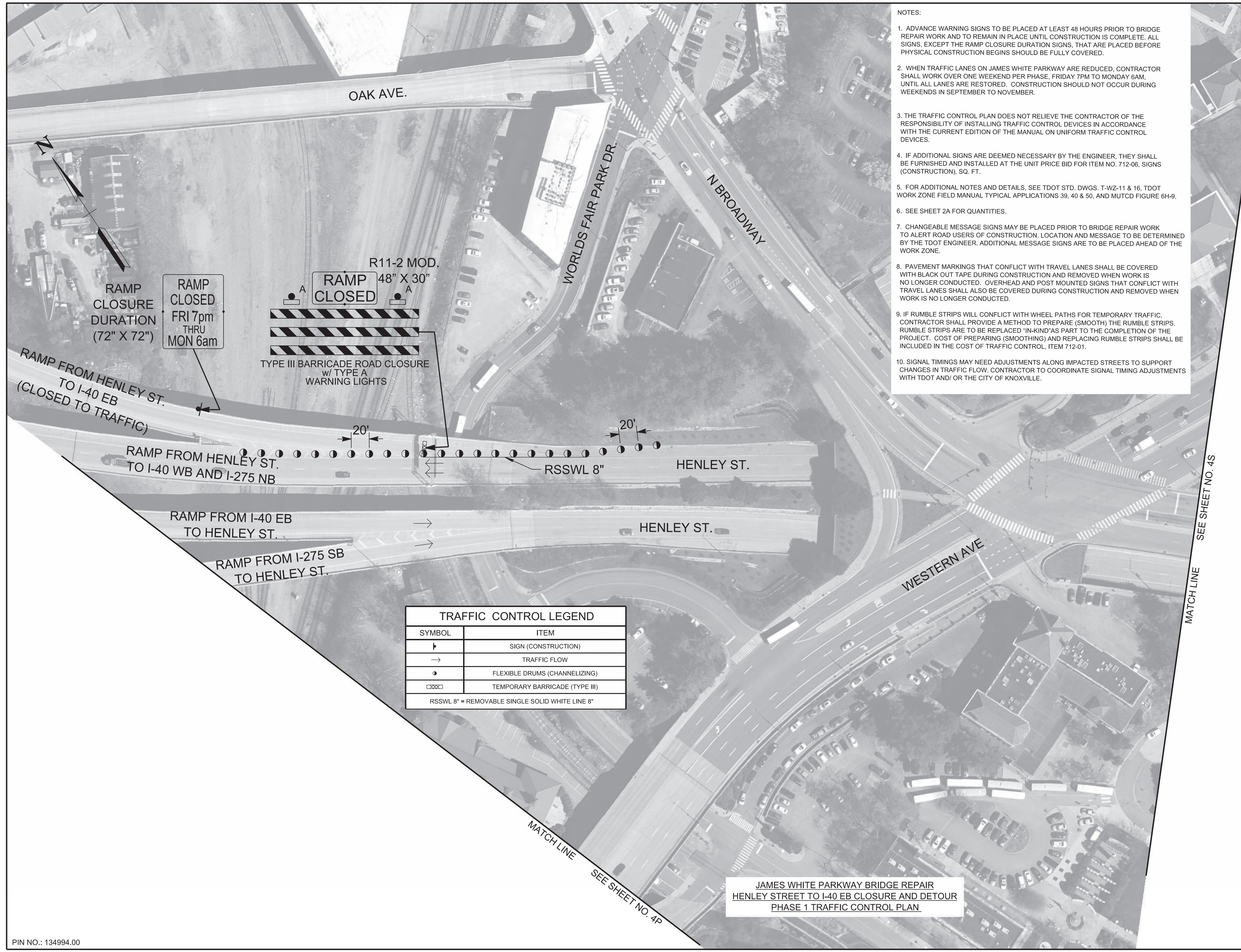
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4R |
| PS&E | 2026 | 471040-M3-012 | 4R |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|---|--------------------------------|
| SYMBOL | ITEM |
| ⊣ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| ▣ | TEMPORARY BARRICADE (TYPE III) |
| RSSWL 8" = REMOVABLE SINGLE SOLID WHITE LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
HENLEY STREET TO I-40 EB CLOSURE AND DETOUR
PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY
William Foster Boyd
WILLIAM FOSTER BOYD
REGISTERED ENGINEER
NO. 119,236
STATE OF TENNESSEE
02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

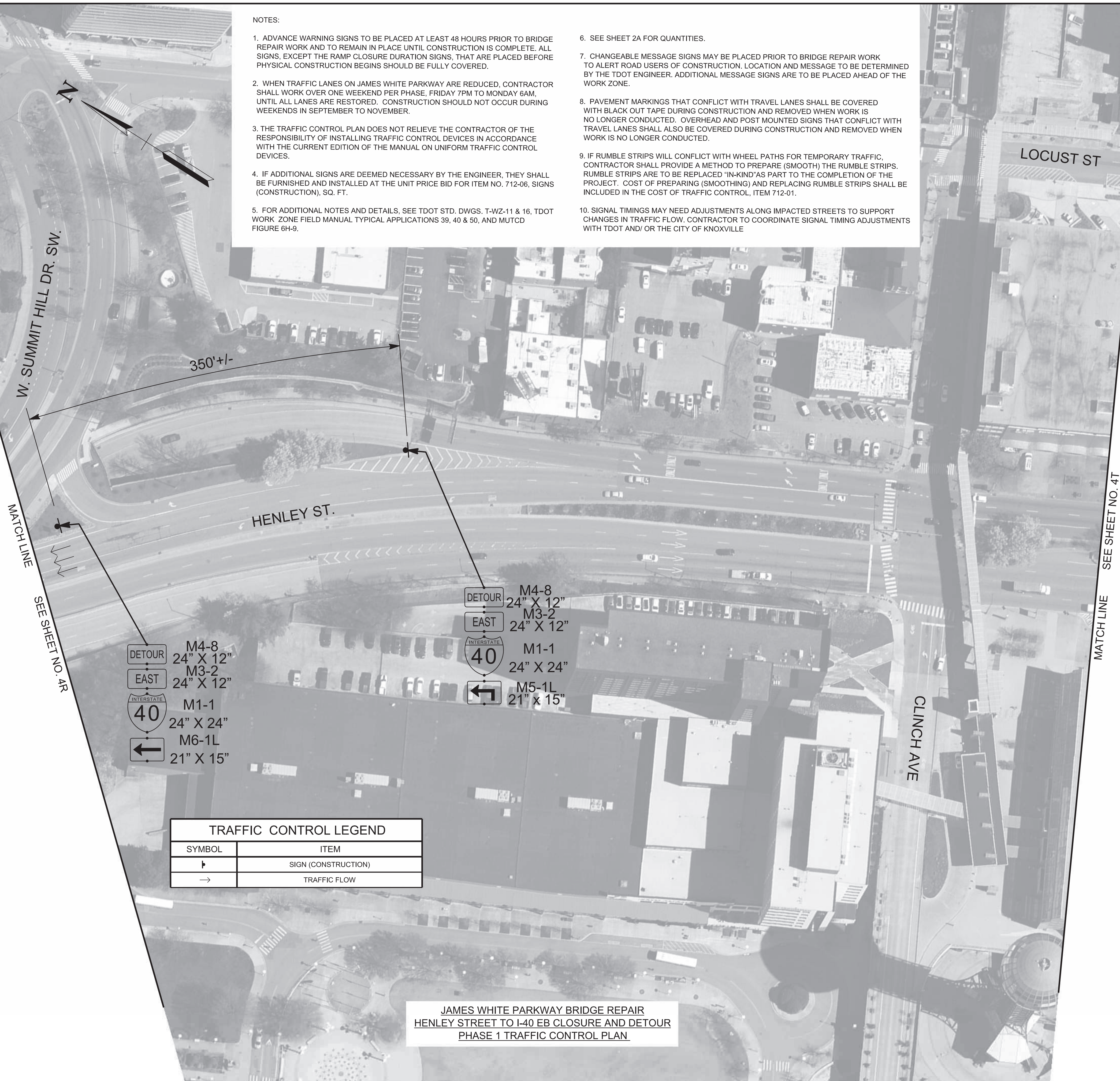
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 4S |
| PS&E | 2026 | 471040-M3-012 | 4S |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED AT LEAST 48 HOURS PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. ALL SIGNS, EXCEPT THE RAMP CLOSURE DURATION SIGNS, THAT ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS SHOULD BE FULLY COVERED.
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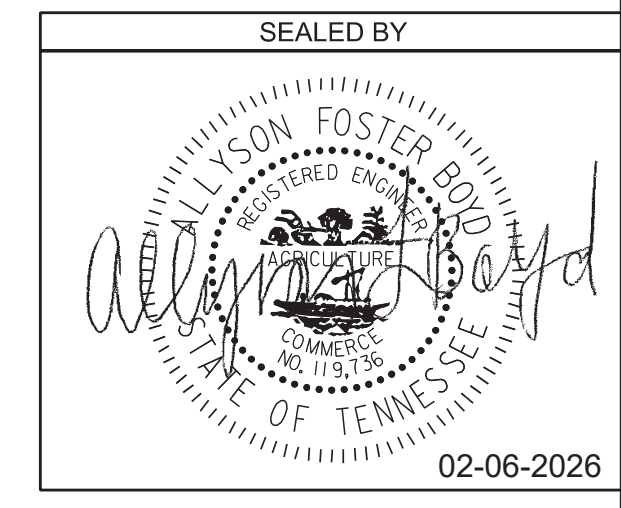


- DETOUR M4-8 24" X 12"
- EAST M3-2 24" X 12"
- INTERSTATE 40 M1-1 24" X 24"
- ← M6-1L 21" X 15"

- DETOUR M4-8 24" X 12"
- EAST M3-2 24" X 12"
- INTERSTATE 40 M1-1 24" X 24"
- ↩ M5-1L 21" X 15"

| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ⬇ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
HENLEY STREET TO I-40 EB CLOSURE AND DETOUR
PHASE 1 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE I
1"=50'

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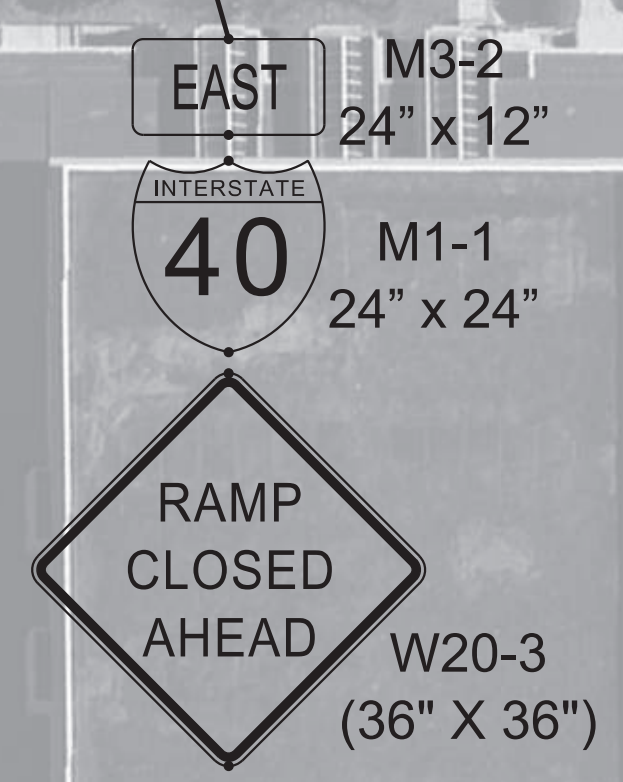
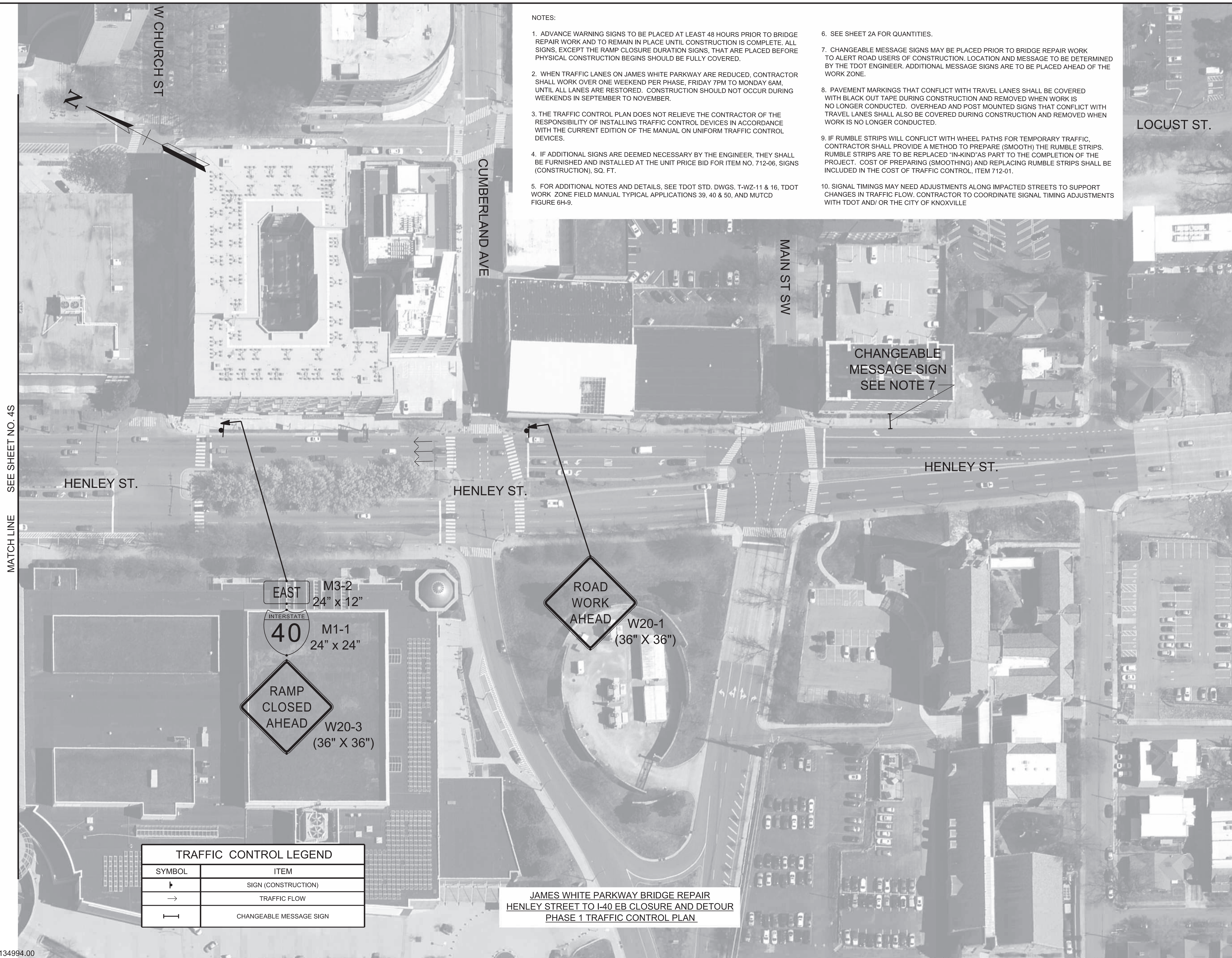
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| PS&E | 2026 | 471040-M3-012 | 4T |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

NOTES:

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| TRAFFIC CONTROL LEGEND | |
|------------------------|-------------------------|
| SYMBOL | ITEM |
| ▶ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ⎓ | CHANGEABLE MESSAGE SIGN |

JAMES WHITE PARKWAY BRIDGE REPAIR
HENLEY STREET TO I-40 EB CLOSURE AND DETOUR
PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
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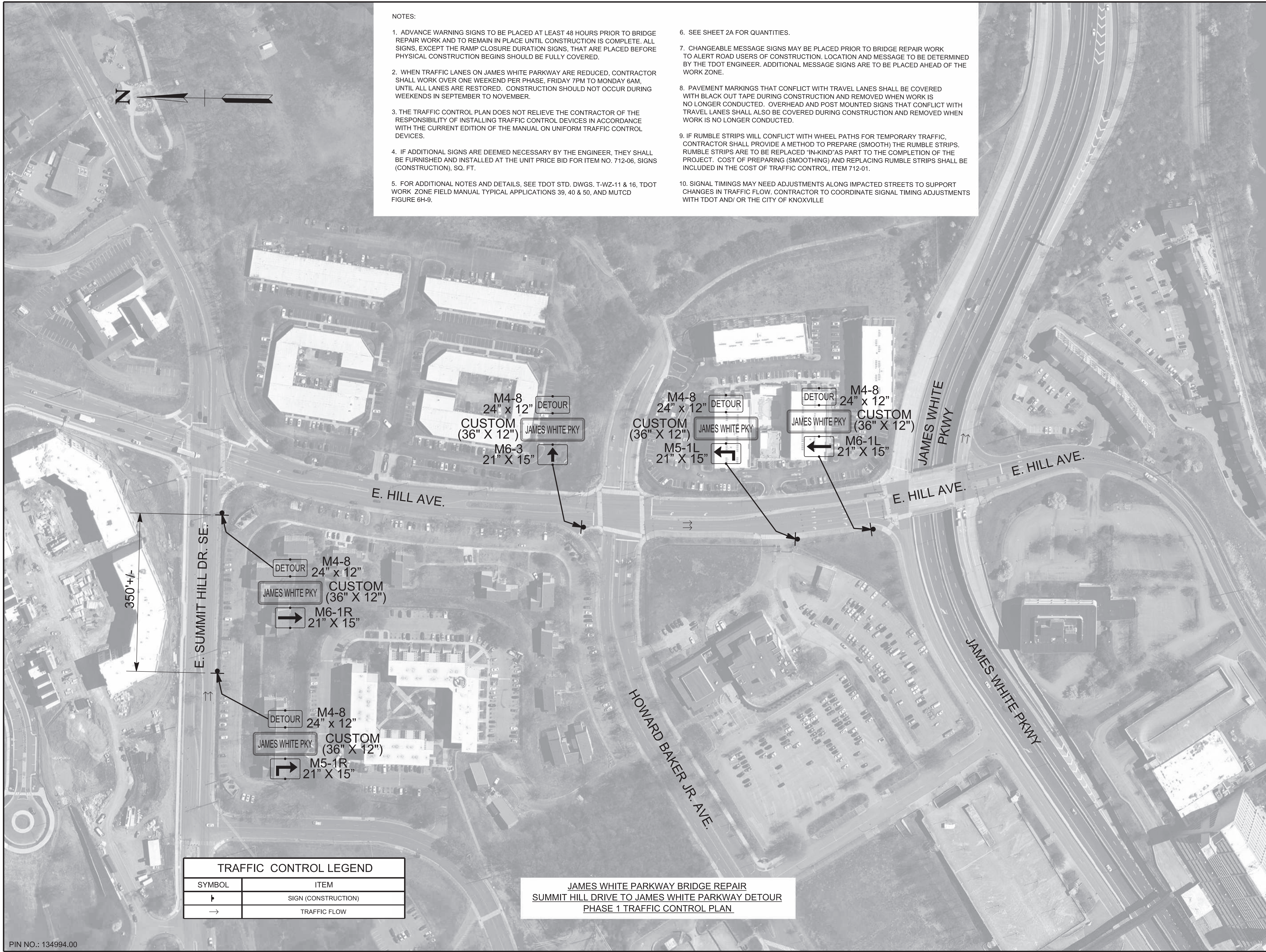
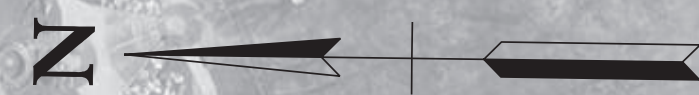
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| PS&E | 2026 | 471040-M3-012 | 4U |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
 SUMMIT HILL DRIVE TO JAMES WHITE PARKWAY DETOUR
 PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

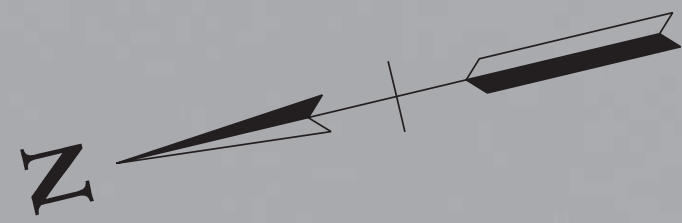
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 BR. NO. 47-10040-19.30
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
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| PS&E | 2026 | 471040-M3-012 | 5 |
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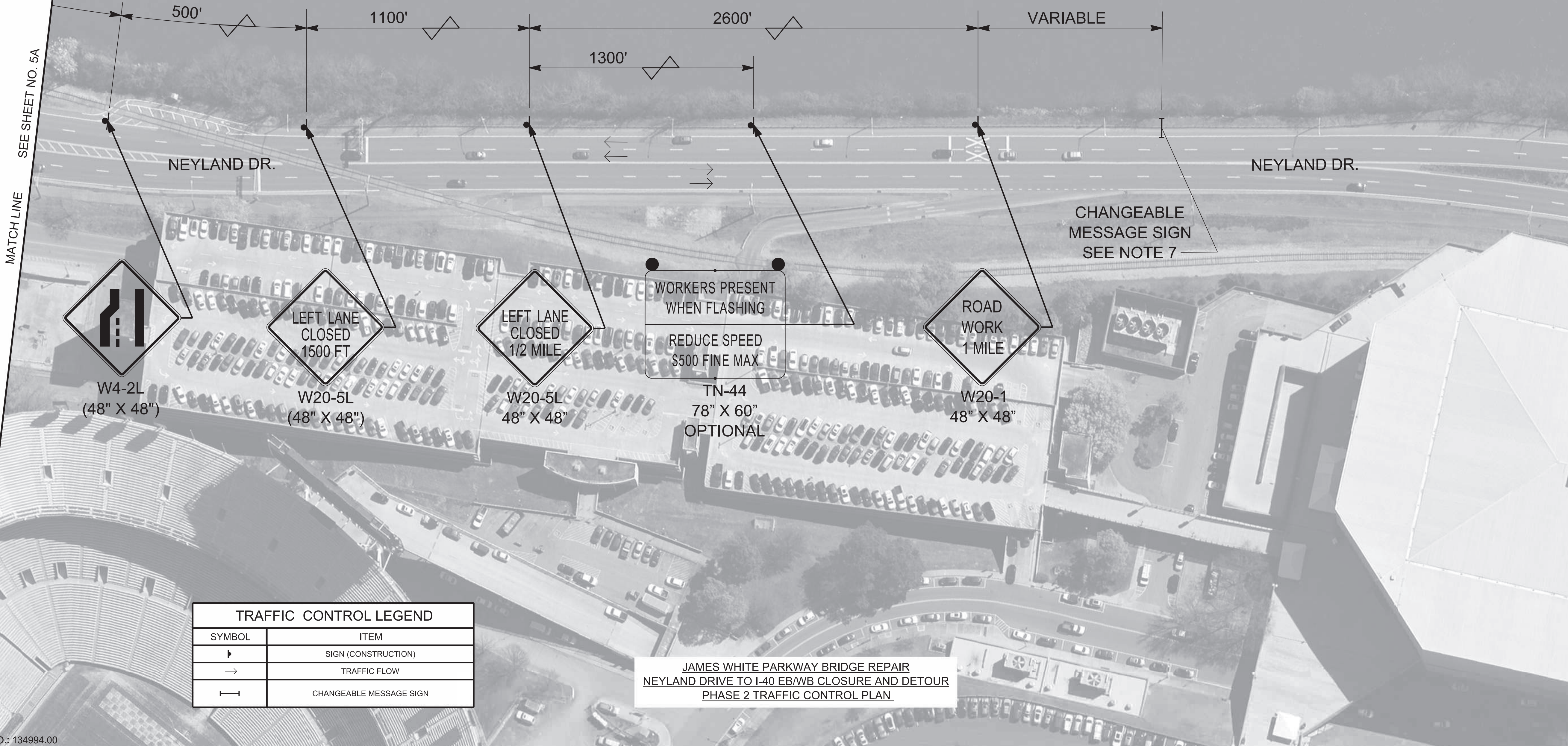
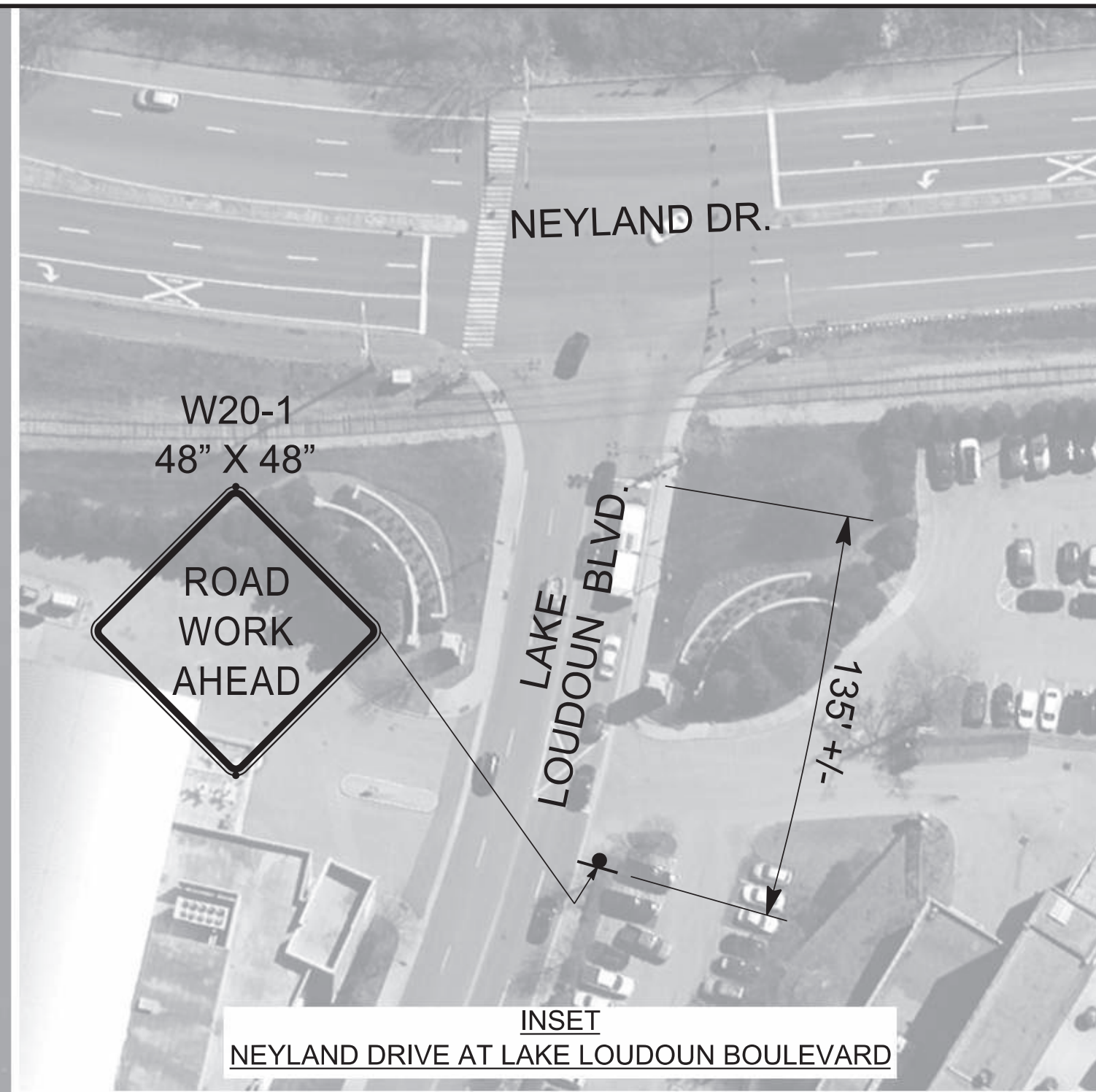
KNOX COUNTY I-40 (JAMES WHITE PKWY)



NOTES:

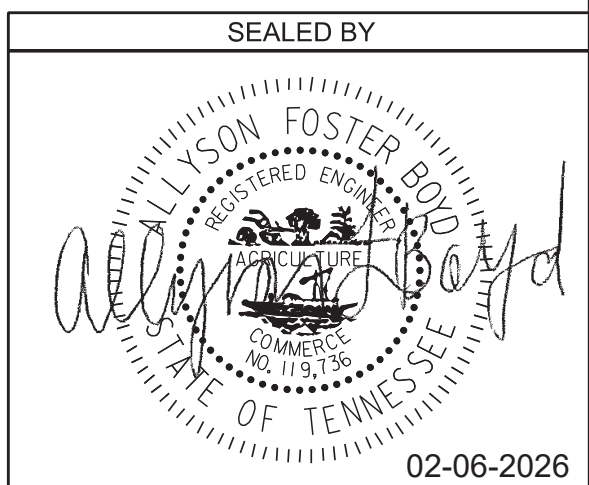
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- THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
- FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWGS. T-WZ-11 & 16, TDOT WORK ZONE FIELD MANUAL TYPICAL APPLICATIONS 39, 40 & 50, AND MUTCD FIGURE 6H-9.
- SEE SHEET 2A FOR QUANTITIES.
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TENNESSEE RIVER



| TRAFFIC CONTROL LEGEND | |
|------------------------|-------------------------|
| SYMBOL | ITEM |
| | SIGN (CONSTRUCTION) |
| | TRAFFIC FLOW |
| | CHANGEABLE MESSAGE SIGN |

JAMES WHITE PARKWAY BRIDGE REPAIR
 NEYLAND DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
 PHASE 2 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE 2
 1"=50'

1/29/2026 11:04:03 AM
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5A |
| PS&E | 2026 | 471040-M3-012 | 5A |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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TENNESSEE RIVER

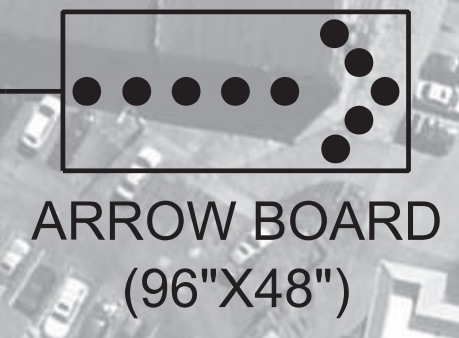
1000'

NEYLAND DR.

NEYLAND DR.

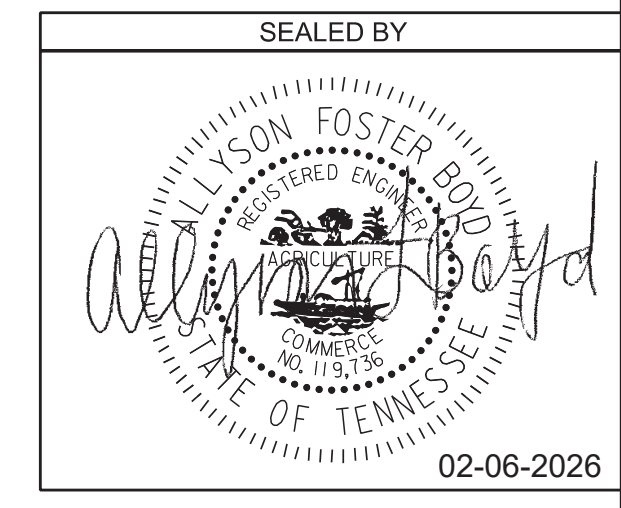
MATCH LINE SEE SHEET NO. 5B

MATCH LINE SEE SHEET NO. 5



| TRAFFIC CONTROL LEGEND | |
|--|-----------------------------------|
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| //// | REMOVABLE BLACK-OUT TAPE (8") |
| ●●●● | ARROW BOARD TYPE C (SINGLE ARROW) |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
 NEYLAND DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
 PHASE 2 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE 2
 1"=50'

1/29/2026 12:07:44 PM
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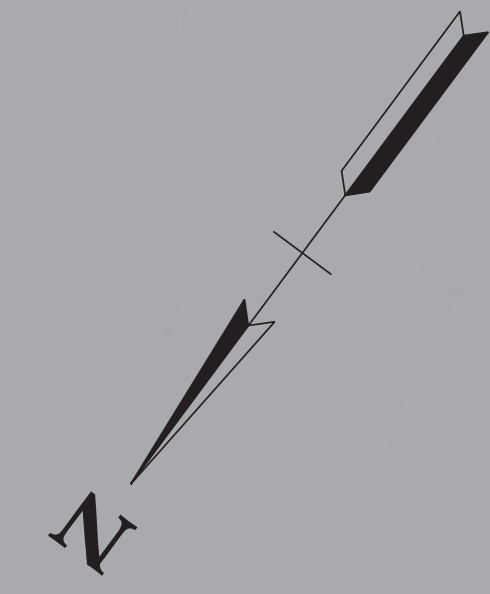
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
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| PS&E | 2026 | 471040-M3-012 | 5B |
| | | | |

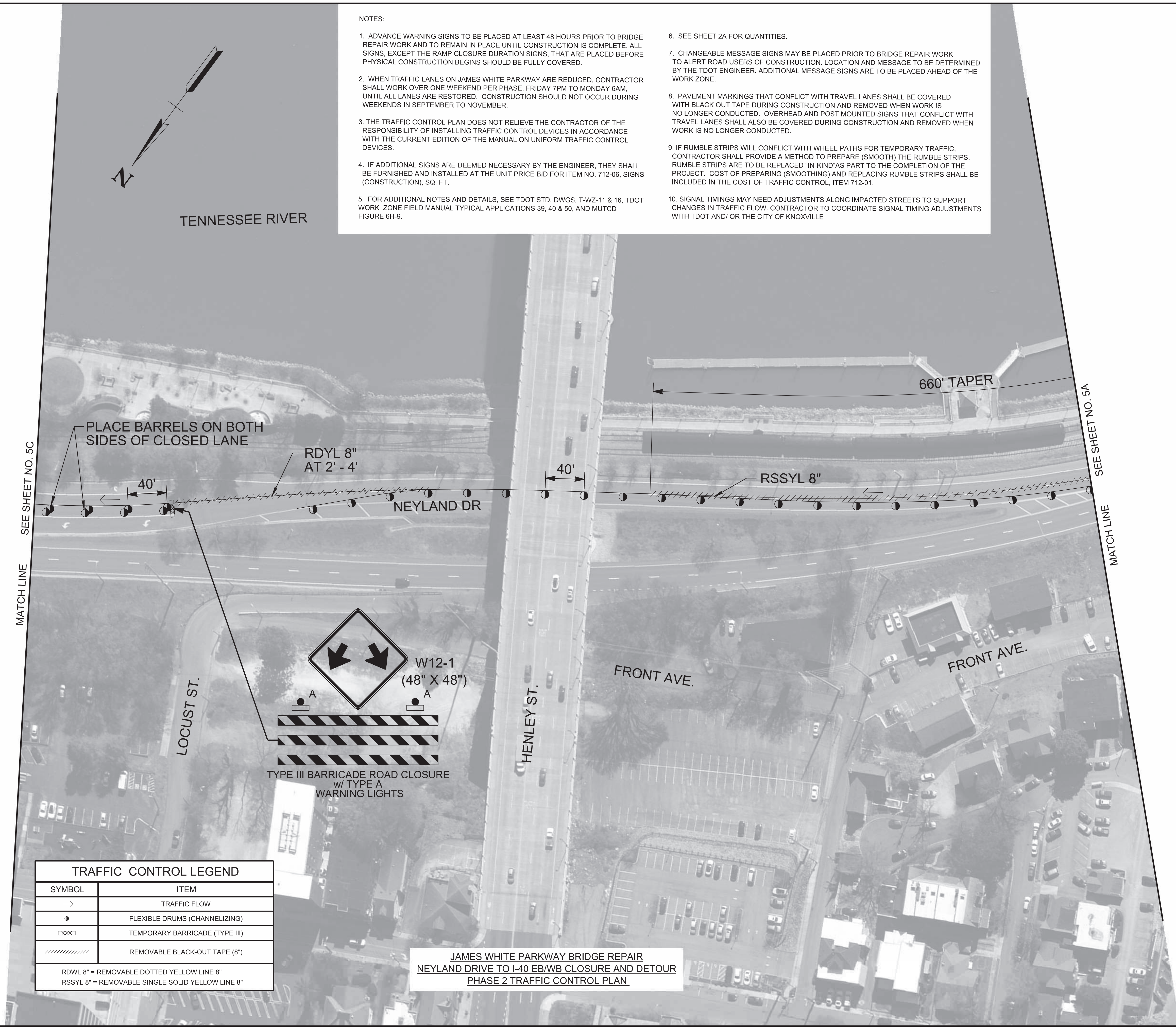
KNOX COUNTY I-40 (JAMES WHITE PKWY)

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TENNESSEE RIVER



MATCH LINE SEE SHEET NO. 5C

MATCH LINE SEE SHEET NO. 5A

PLACE BARRELS ON BOTH SIDES OF CLOSED LANE

RDYL 8" AT 2' - 4'

NEYLAND DR

HENLEY ST.

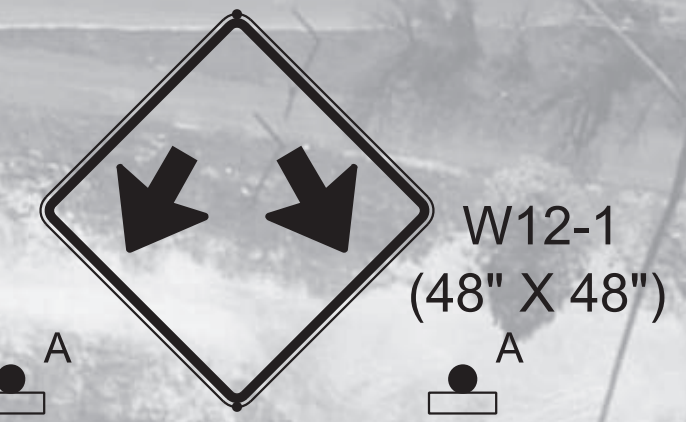
FRONT AVE.

FRONT AVE.

660' TAPER

RSSYL 8"

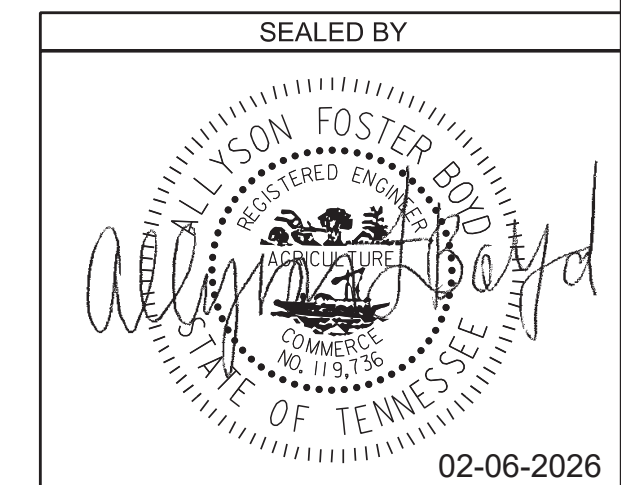
LOCUST ST.



TYPE III BARRICADE ROAD CLOSURE w/ TYPE A WARNING LIGHTS

| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| SYMBOL | ITEM |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| □XXX□ | TEMPORARY BARRICADE (TYPE III) |
| //// | REMOVABLE BLACK-OUT TAPE (8") |
| RDWL 8" = REMOVABLE DOTTED YELLOW LINE 8" | |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
NEYLAND DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

1/29/2026 11:08:51 AM
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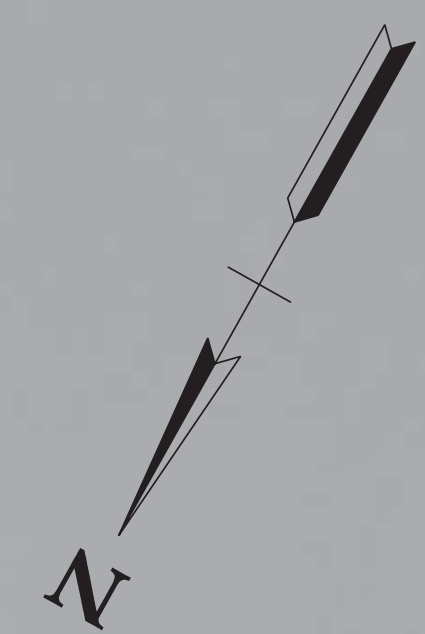
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5C |
| PS&E | 2026 | 471040-M3-012 | 5C |

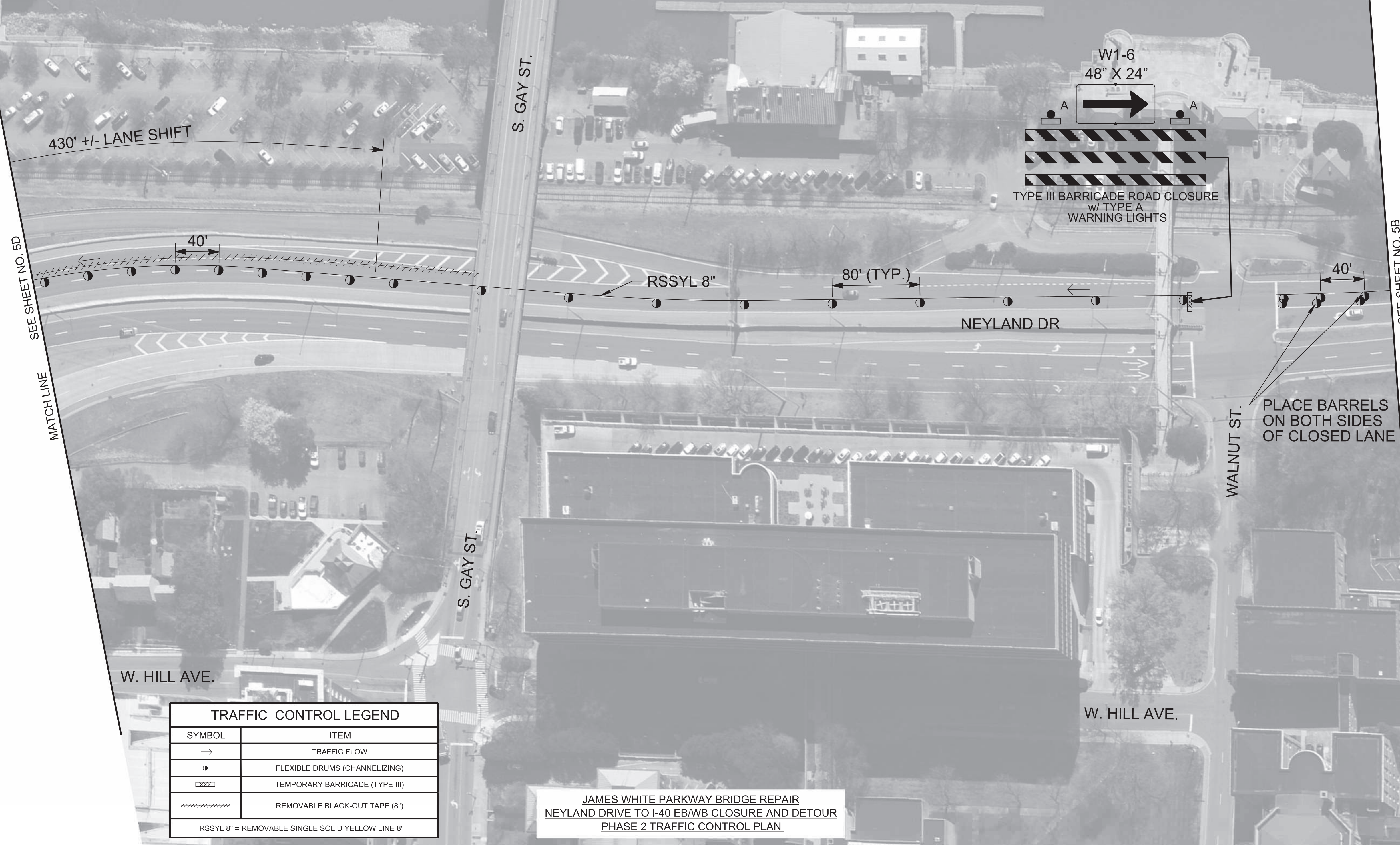
KNOX COUNTY I-40 (JAMES WHITE PKWY)

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TENNESSEE RIVER



| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| SYMBOL | ITEM |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| ▣ | TEMPORARY BARRICADE (TYPE III) |
| //// | REMOVABLE BLACK-OUT TAPE (8") |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
NEYLAND DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

1/29/2026 11:10:09 AM
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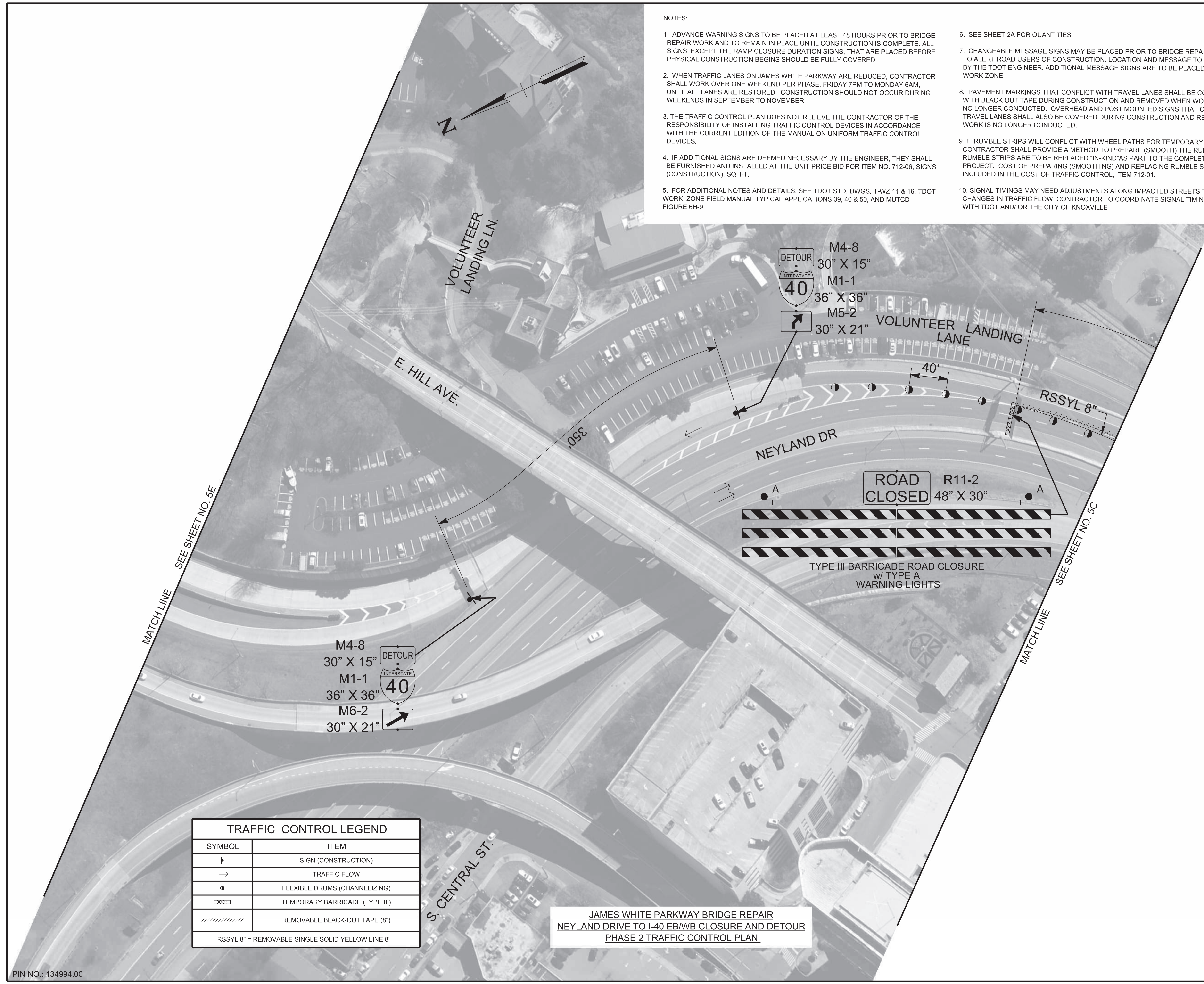
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5D |
| PS&E | 2026 | 471040-M3-012 | 5D |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| → | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| XXXX | TEMPORARY BARRICADE (TYPE III) |
| //// | REMOVABLE BLACK-OUT TAPE (8") |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
 NEYLAND DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
 PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

 02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE 2
 1"=50'

1/29/2026 11:11:25 AM
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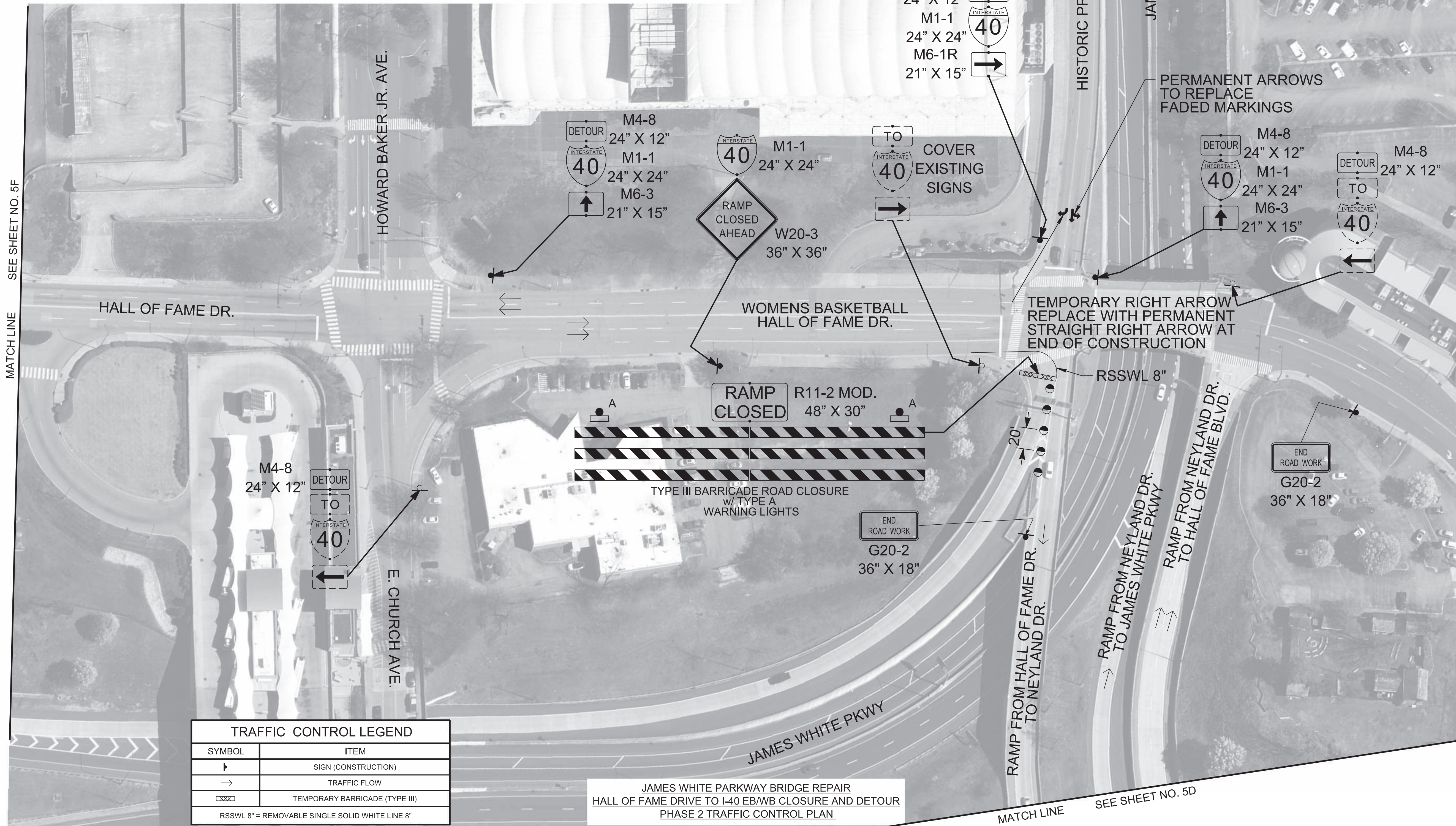
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5E |
| PS&E | 2026 | 471040-M3-012 | 5E |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

NOTES:

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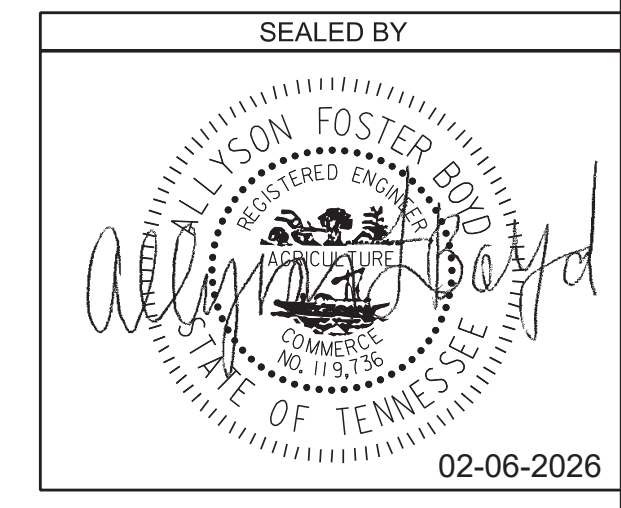


| TRAFFIC CONTROL LEGEND | |
|---|--------------------------------|
| SYMBOL | ITEM |
| → | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| XXXX | TEMPORARY BARRICADE (TYPE III) |
| RSSWL 8" = REMOVABLE SINGLE SOLID WHITE LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
HALL OF FAME DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN

MATCH LINE SEE SHEET NO. 5F

MATCH LINE SEE SHEET NO. 5D



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

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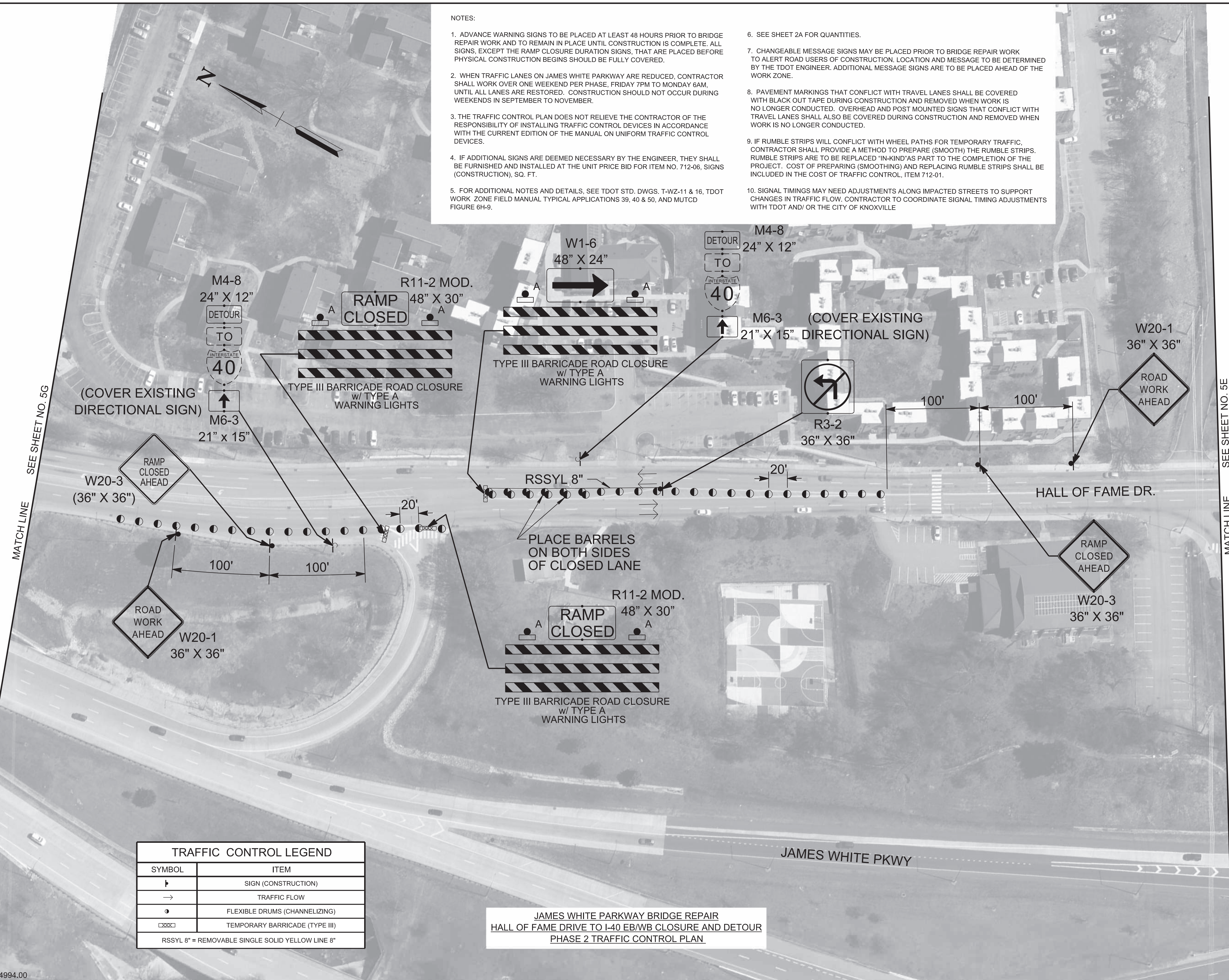
PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5F |
| PS&E | 2026 | 471040-M3-012 | 5F |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

NOTES:

- ADVANCE WARNING SIGNS TO BE PLACED AT LEAST 48 HOURS PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. ALL SIGNS, EXCEPT THE RAMP CLOSURE DURATION SIGNS, THAT ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS SHOULD BE FULLY COVERED.
- WHEN TRAFFIC LANES ON JAMES WHITE PARKWAY ARE REDUCED, CONTRACTOR SHALL WORK OVER ONE WEEKEND PER PHASE, FRIDAY 7PM TO MONDAY 6AM, UNTIL ALL LANES ARE RESTORED. CONSTRUCTION SHOULD NOT OCCUR DURING WEEKENDS IN SEPTEMBER TO NOVEMBER.
- THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
- FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWGS. T-WZ-11 & 16, TDOT WORK ZONE FIELD MANUAL TYPICAL APPLICATIONS 39, 40 & 50, AND MUTCD FIGURE 6H-9.
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| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| ⬇ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ○ | FLEXIBLE DRUMS (CHANNELIZING) |
| ▬▬▬▬ | TEMPORARY BARRICADE (TYPE III) |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
 HALL OF FAME DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
 PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

 02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

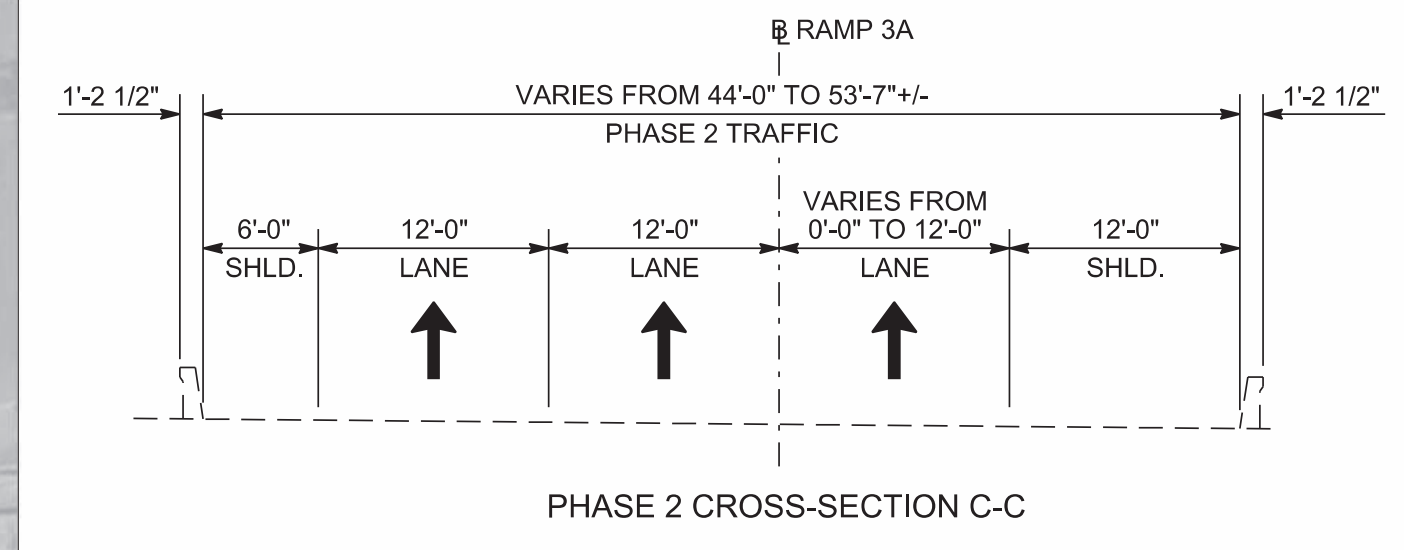
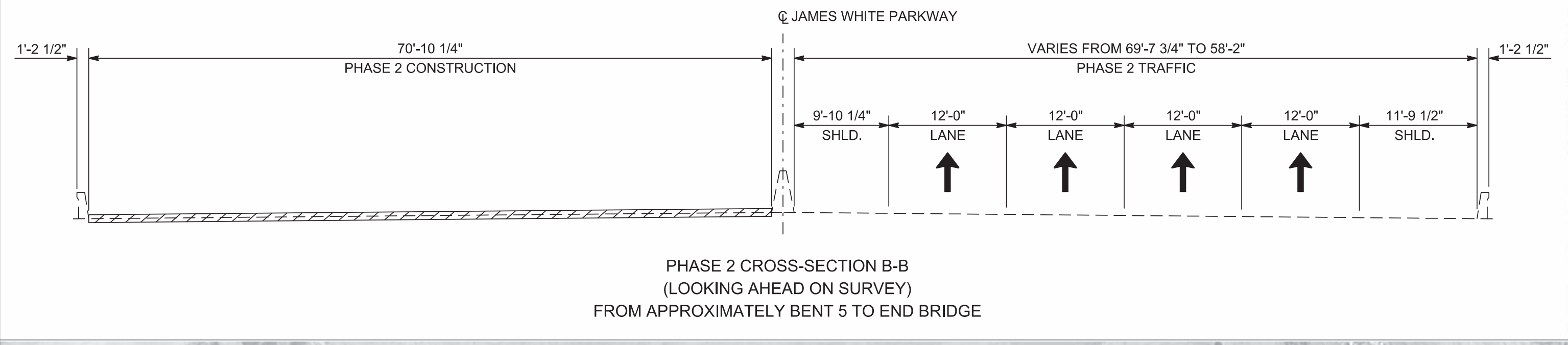
TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE 2
 1"=50'

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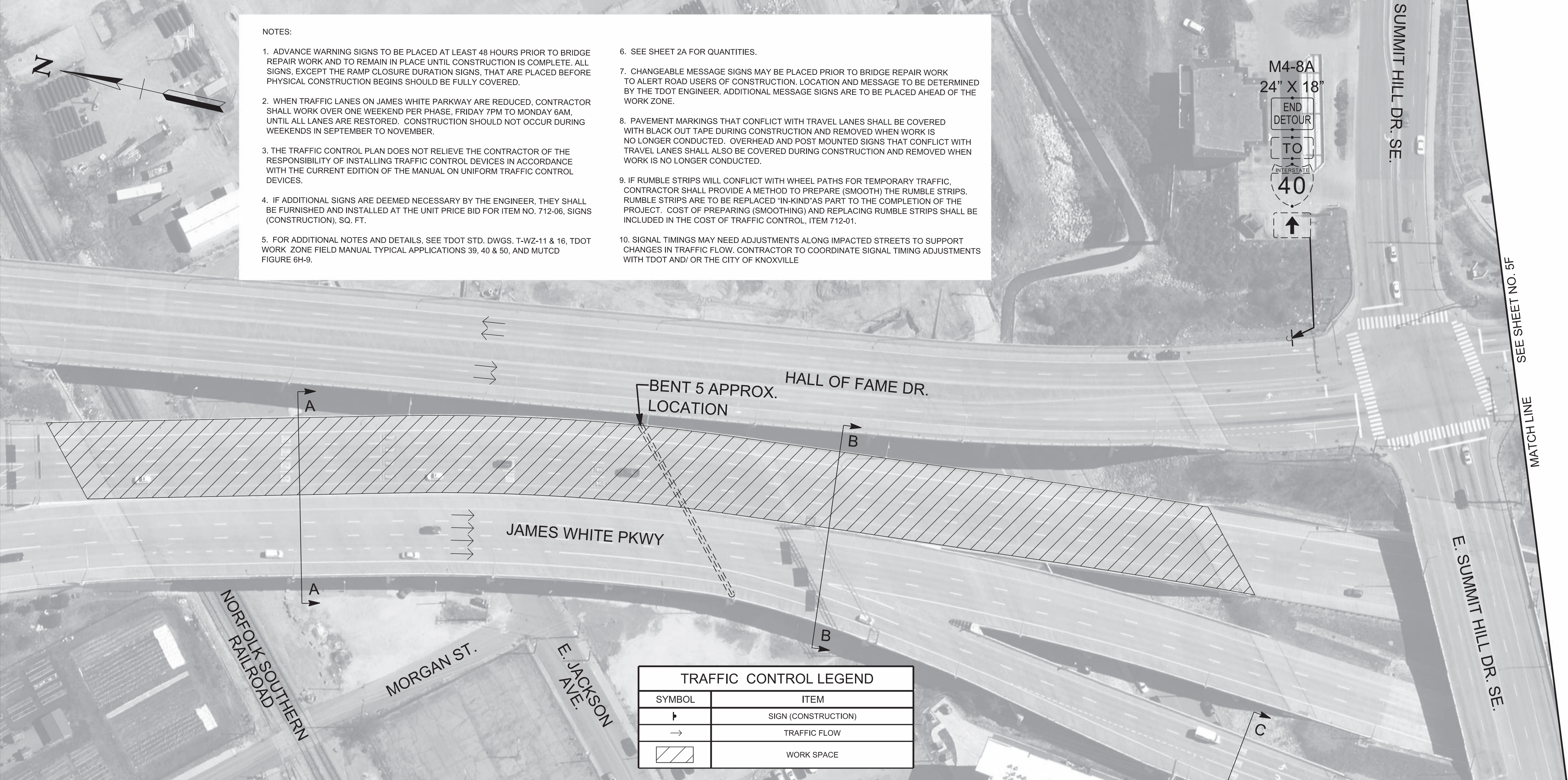
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
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| PS&E | 2026 | 471040-M3-012 | 5G |

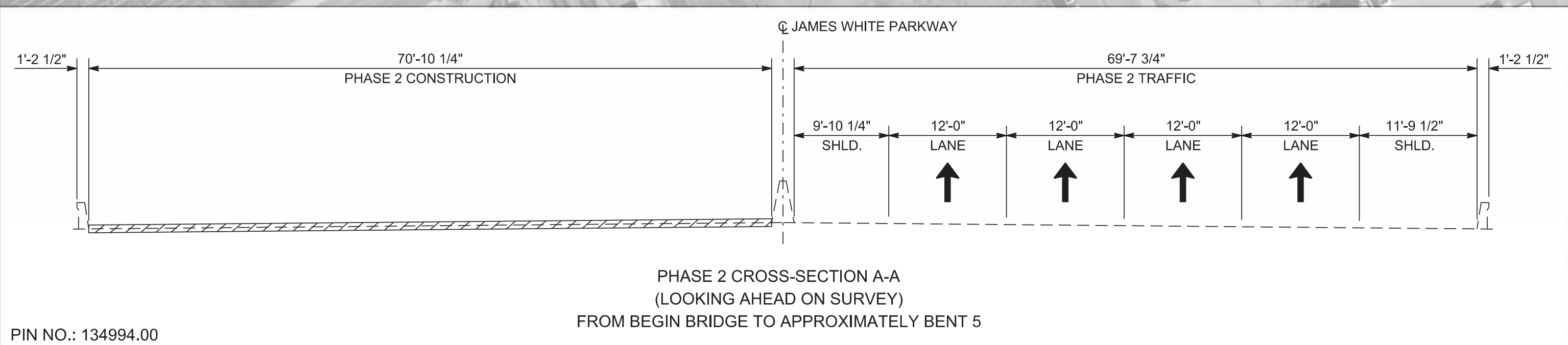
KNOX COUNTY I-40 (JAMES WHITE PKWY)



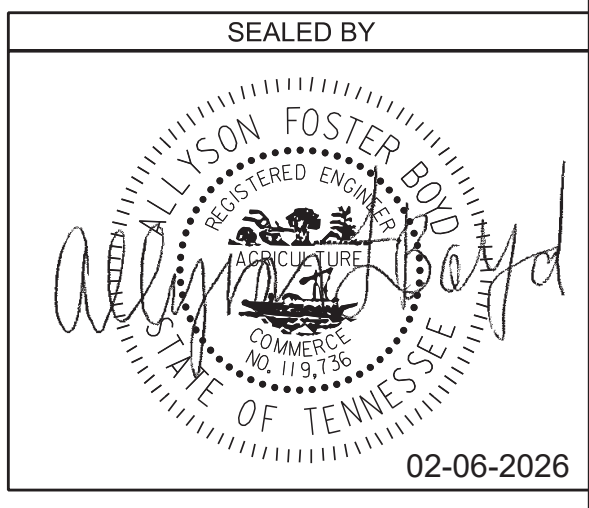
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| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ▨ | WORK SPACE |



JAMES WHITE PARKWAY BRIDGE REPAIR
HALL OF FAME DRIVE TO I-40 EB/WB CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

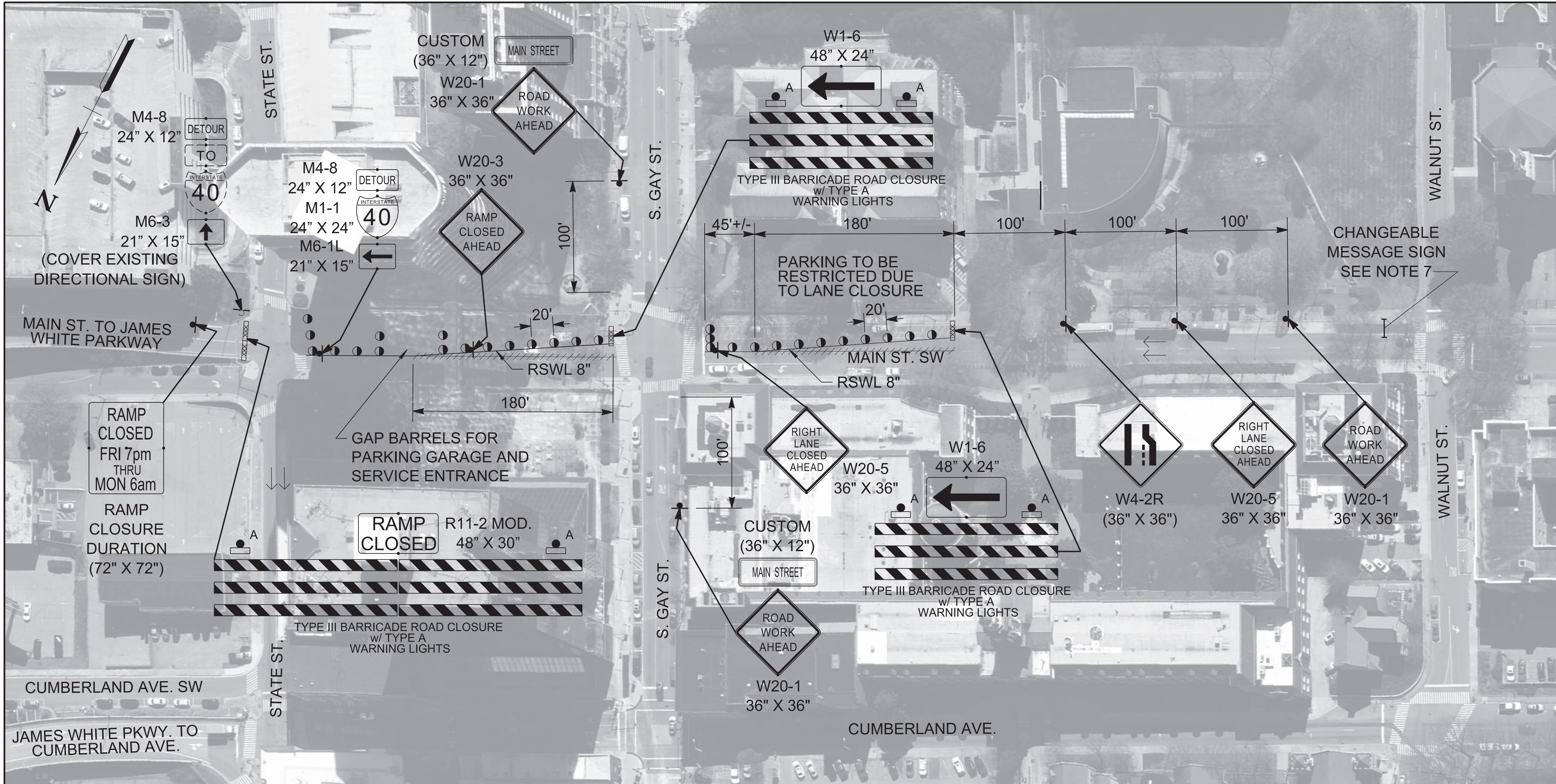
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5H |
| PS&E | 2026 | 471040-M3-012 | 5H |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



MAIN ST. TO JAMES WHITE PARKWAY

RAMP CLOSED
FRI 7pm
THRU
MON 6am

RAMP CLOSURE DURATION
(72" X 72")

RAMP CLOSED
R11-2 MOD.
48" X 30"

TYPE III BARRICADE ROAD CLOSURE
w/ TYPE A
WARNING LIGHTS

CUMBERLAND AVE. SW

JAMES WHITE PKWY. TO CUMBERLAND AVE.

M4-8
24" X 12"
M1-1
24" X 24"
M6-1R
21" X 15"

| TRAFFIC CONTROL LEGEND | |
|------------------------|--------------------------------|
| | SIGN (CONSTRUCTION) |
| | TRAFFIC FLOW |
| | FLEXIBLE DRUMS (CHANNELIZING) |
| | TEMPORARY BARRICADE (TYPE III) |
| | REMOVABLE BLACK-OUT TAPE (8") |
| | CHANGEABLE MESSAGE SIGN |

W. CHURCH AVE.

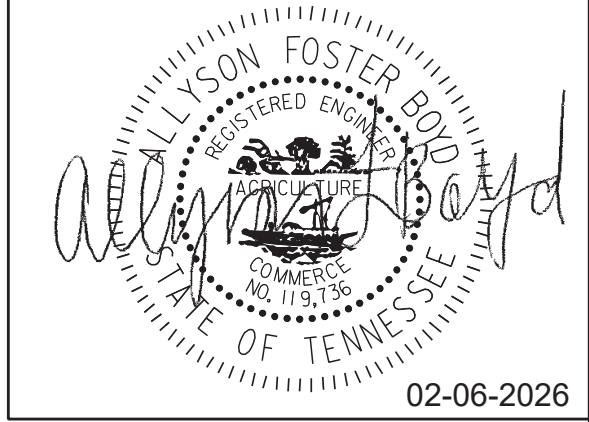
W. CHURCH AVE.

JAMES WHITE PARKWAY BRIDGE REPAIR
MAIN STREET TO I-40 EB/WB CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN

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SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
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1"=50'

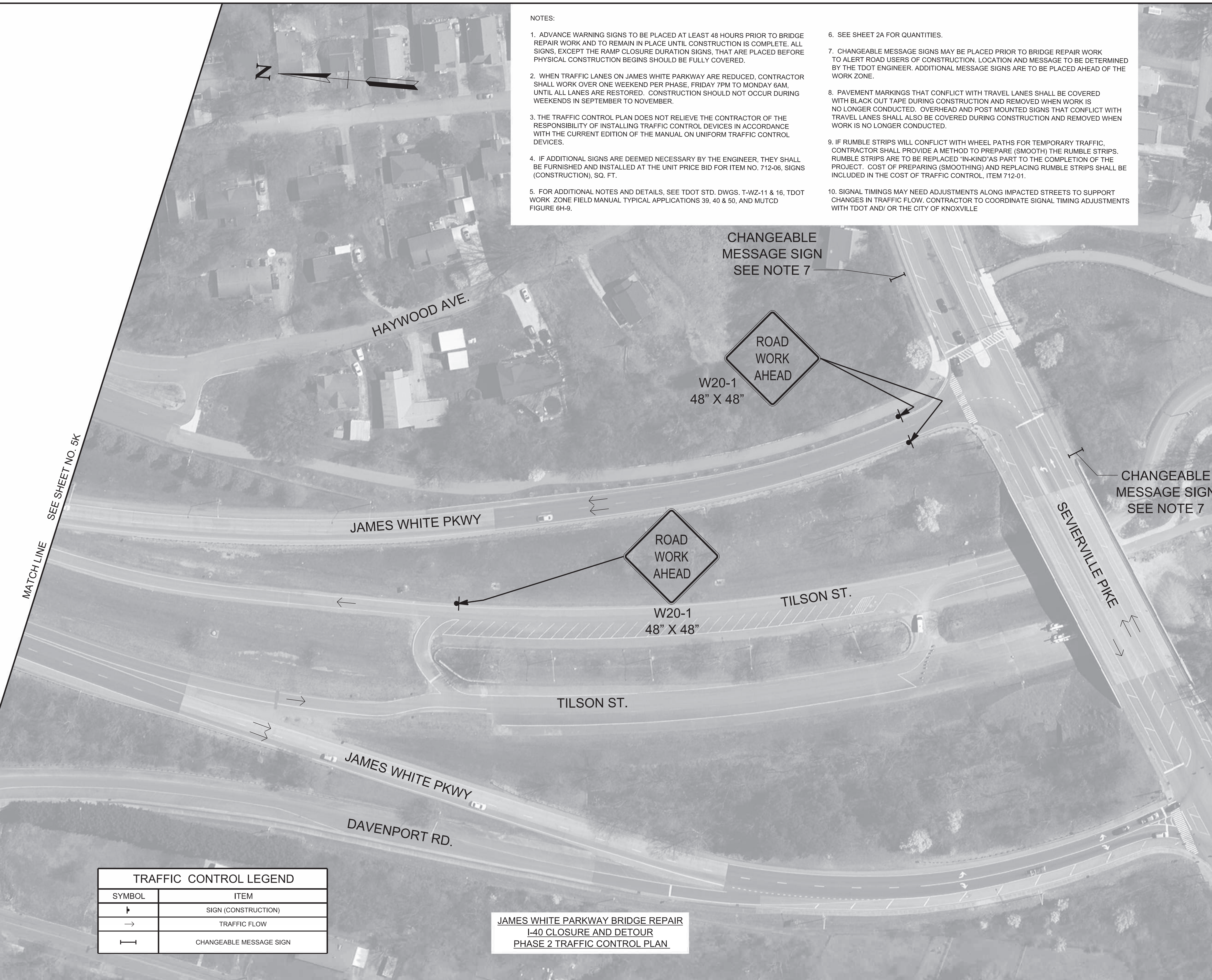
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5J |
| PS&E | 2026 | 471040-M3-012 | 5J |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|------------------------|-------------------------|
| SYMBOL | ITEM |
| ⊣ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ⊢ | CHANGEABLE MESSAGE SIGN |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

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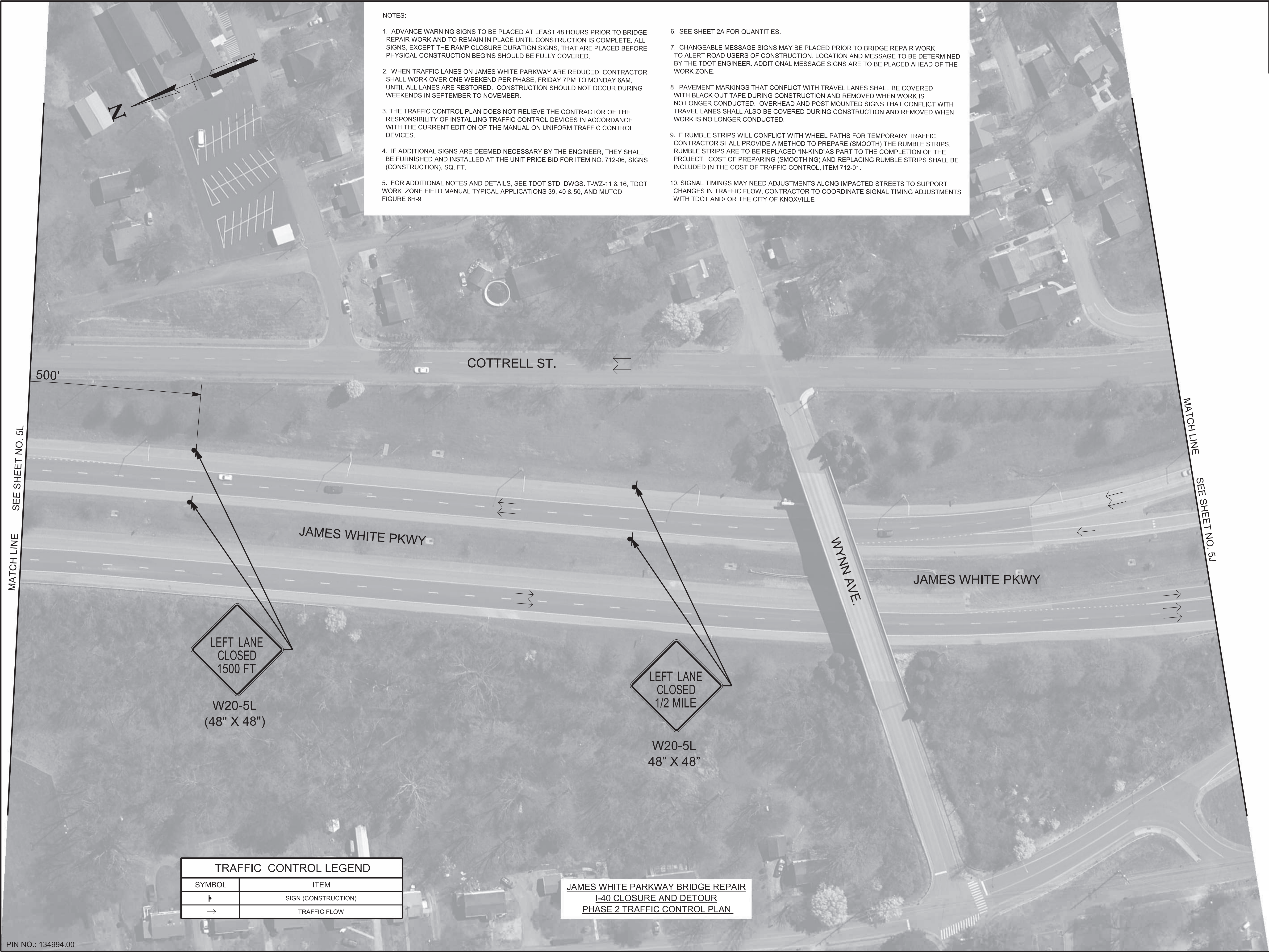
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5K |
| PS&E | 2026 | 471040-M3-012 | 5K |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ▬ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
 I-40 CLOSURE AND DETOUR
 PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

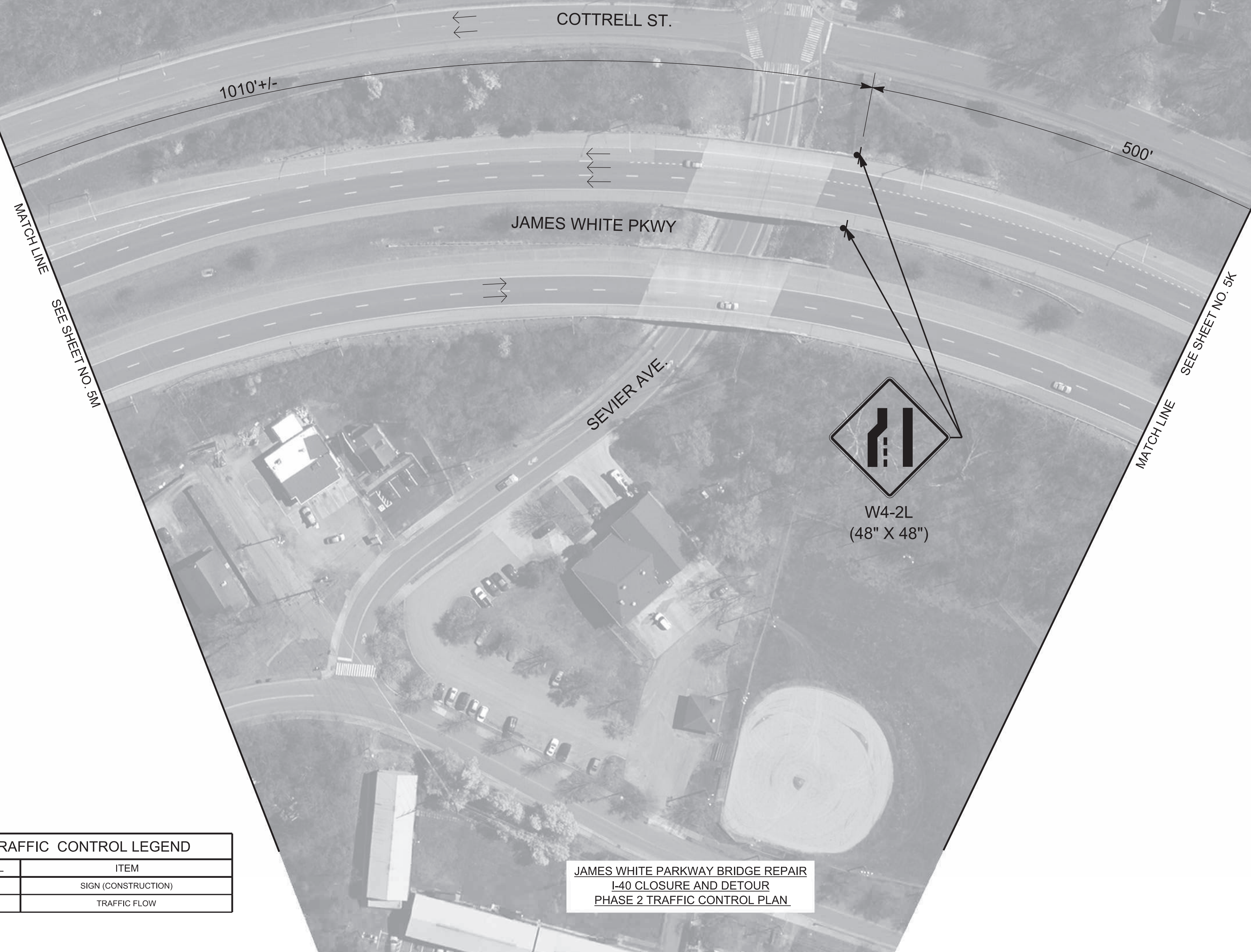
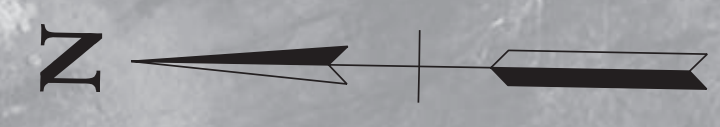
TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE 2
 1"=50'

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5L |
| PS&E | 2026 | 471040-M3-012 | 5L |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|------------------------|---------------------|
| SYMBOL | ITEM |
| ⬇ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

1/29/2026 11:20:50 AM
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PIN NO.: 134994.00

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MATCH LINE SEE SHEET NO. 5N

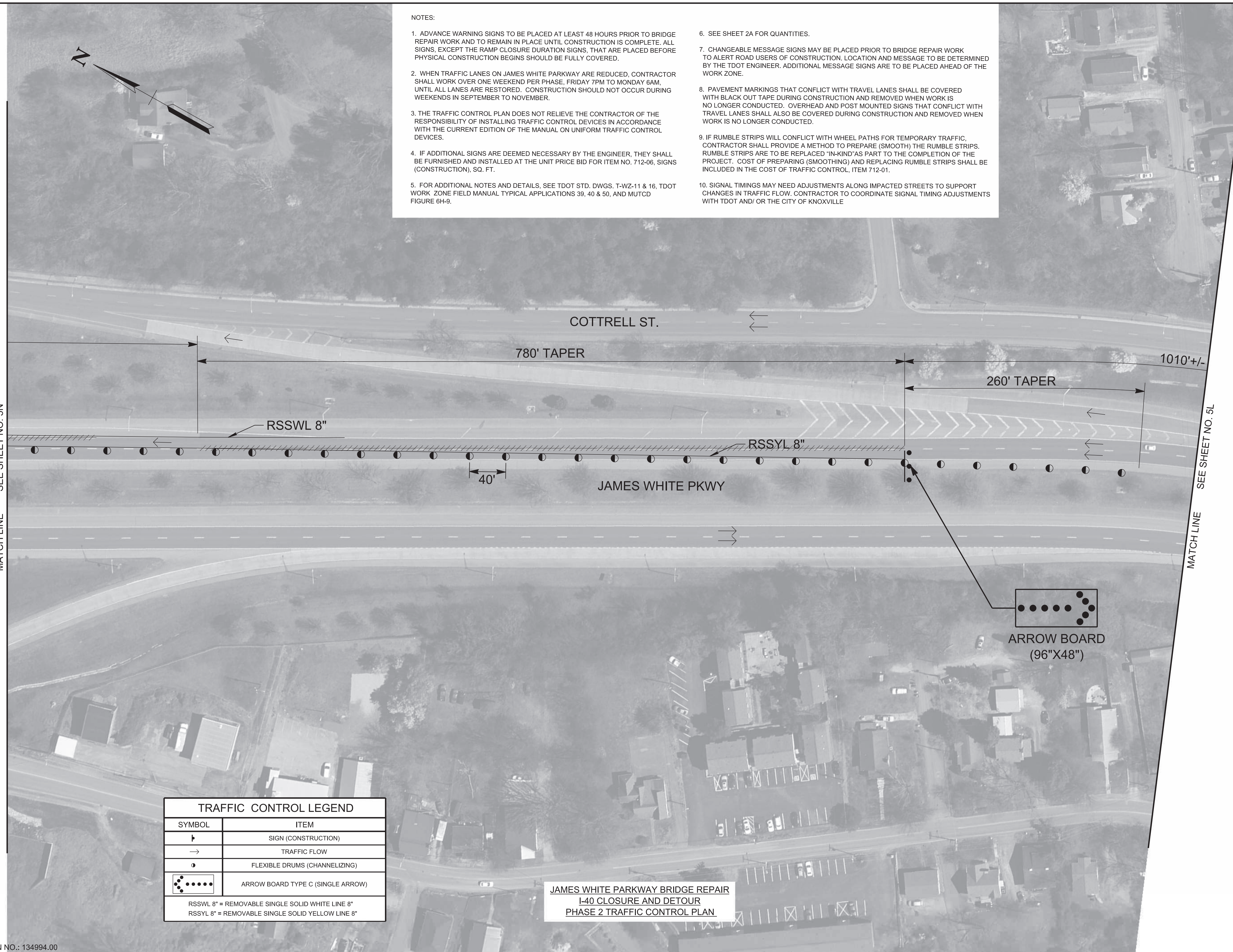
MATCH LINE SEE SHEET NO. 5L

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4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5M |
| PS&E | 2026 | 471040-M3-012 | 5M |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



| TRAFFIC CONTROL LEGEND | |
|------------------------|-----------------------------------|
| | SIGN (CONSTRUCTION) |
| | TRAFFIC FLOW |
| | FLEXIBLE DRUMS (CHANNELIZING) |
| | ARROW BOARD TYPE C (SINGLE ARROW) |

RSSLW 8" = REMOVABLE SINGLE SOLID WHITE LINE 8"
 RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8"

**JAMES WHITE PARKWAY BRIDGE REPAIR
 I-40 CLOSURE AND DETOUR
 PHASE 2 TRAFFIC CONTROL PLAN**

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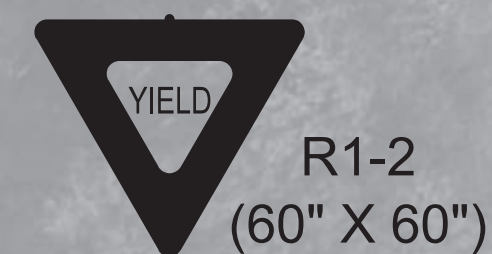
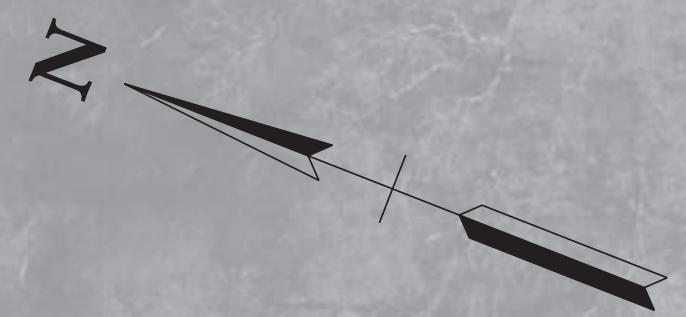
02-06-2026

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
 BR. NO. 47-10040-19.30
 PHASE 2
 1"=50'

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5N |
| PS&E | 2026 | 471040-M3-012 | 5N |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



R1-2
(60" X 60")



W3-2
(48" X 48")



W20-1
(48" X 48")

RAMP FROM ANITA DR.
TO JAMES WHITE PKWY

260'+/-

500'

COTTRELL ST.

780' LANE SHIFT

RSSLW 8"

RSSL 8"

40'

JAMES WHITE PKWY

JAMES WHITE PKWY

ANITA DR.

MATCHLINE SEE SHEET NO. 5L

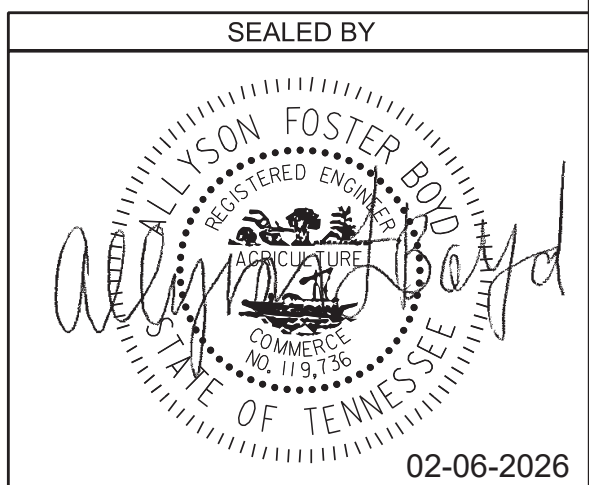
MATCHLINE SEE SHEET NO. 5M

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| TRAFFIC CONTROL LEGEND | |
|---|-------------------------------|
| SYMBOL | ITEM |
| ▬ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| RSSLW 8" = REMOVABLE SINGLE SOLID WHITE LINE 8" | |
| RSSL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

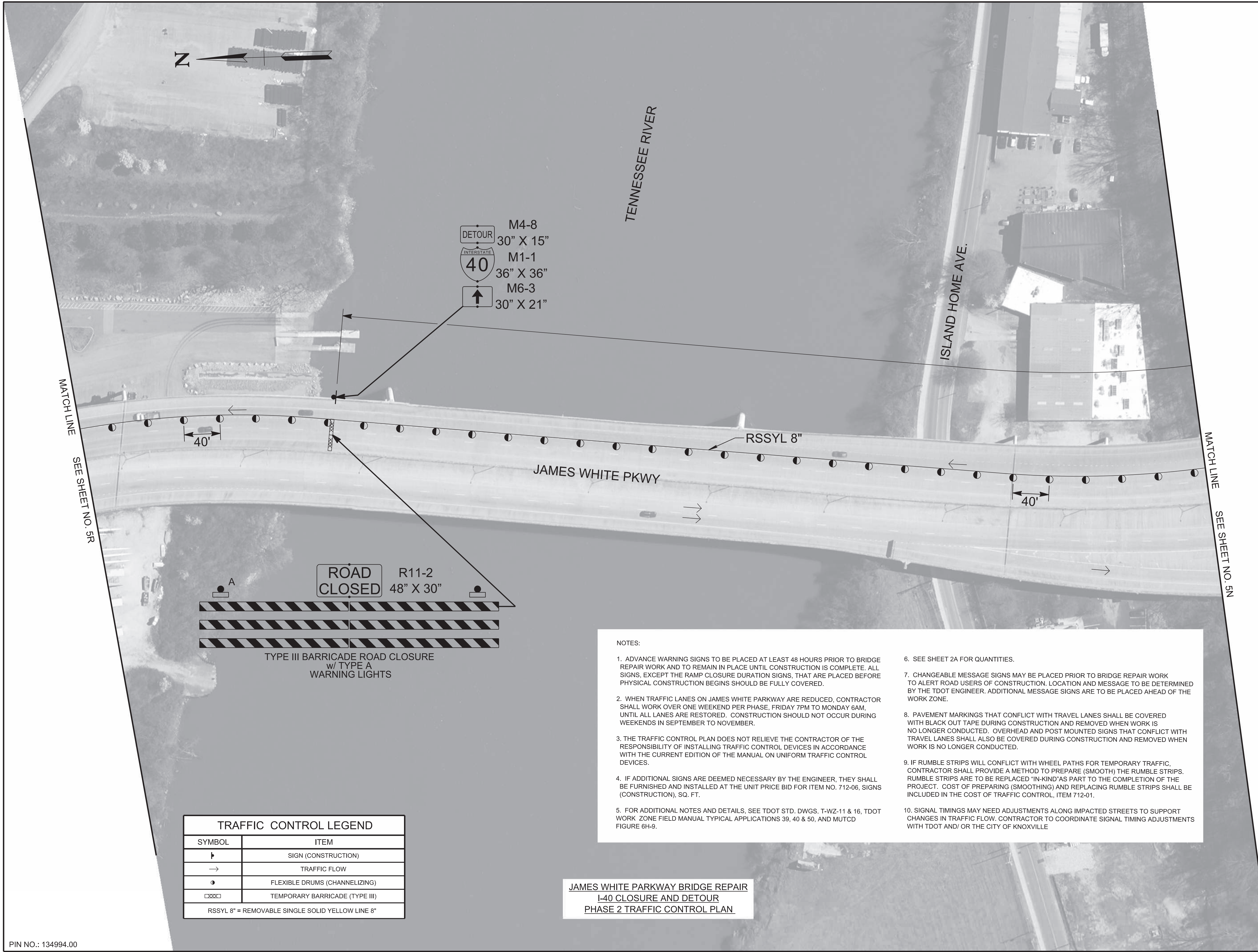
TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5P |
| PS&E | 2026 | 471040-M3-012 | 5P |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)



| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| SYMBOL | ITEM |
| ↑ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ○ | FLEXIBLE DRUMS (CHANNELIZING) |
| ▢ | TEMPORARY BARRICADE (TYPE III) |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

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**JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN**

SEALED BY

02-06-2026

**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'**

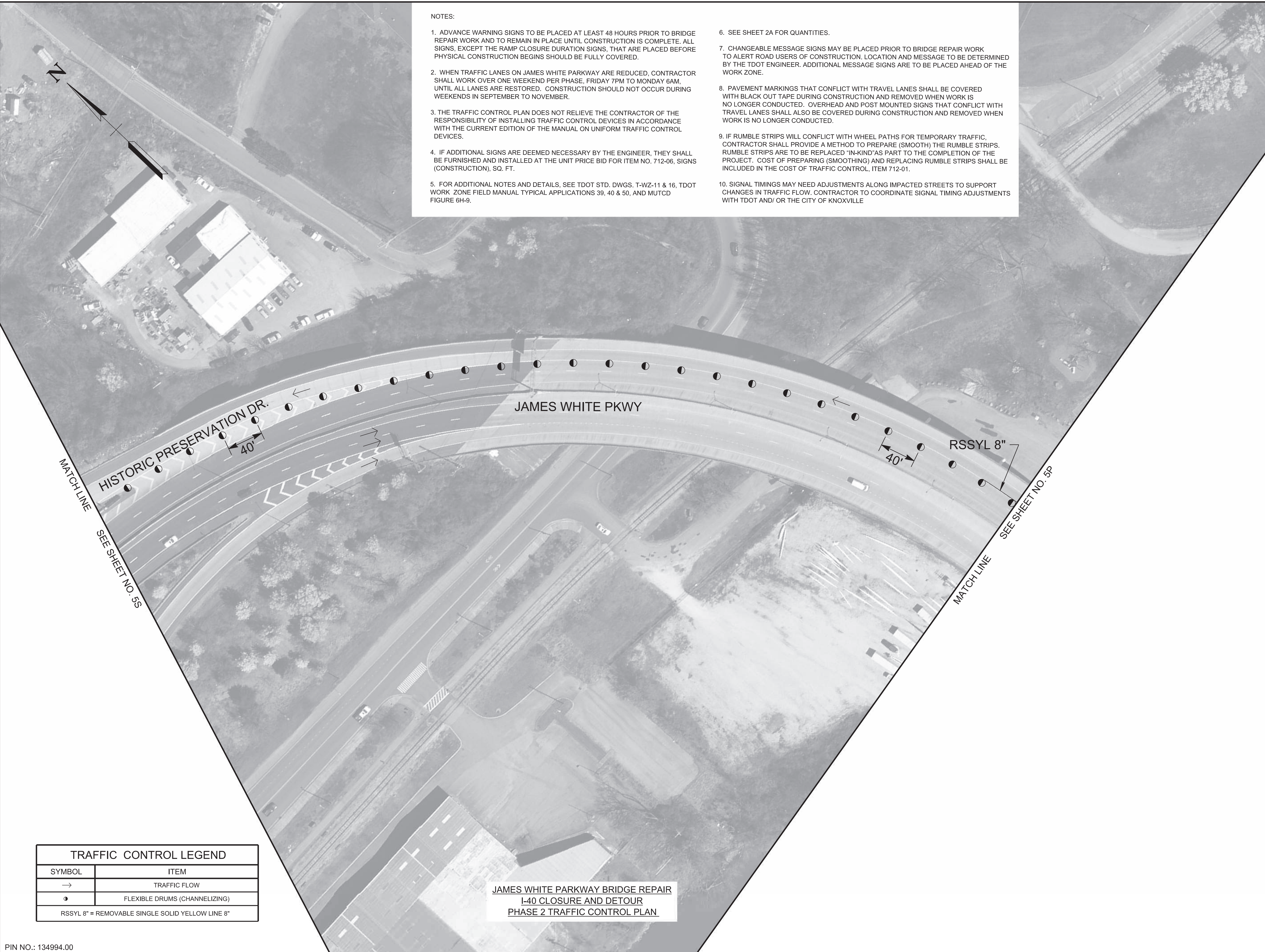
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PIN NO.: 134994.00

| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
| L&G | 2025 | 471040-M3-012 | 5R |
| PS&E | 2026 | 471040-M3-012 | 5R |
| | | | |

KNOX COUNTY I-40 (JAMES WHITE PKWY)

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| TRAFFIC CONTROL LEGEND | |
|--|-------------------------------|
| SYMBOL | ITEM |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| RSSYL 8" = REMOVABLE SINGLE SOLID YELLOW LINE 8" | |

JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN

SEALED BY

02-06-2026

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

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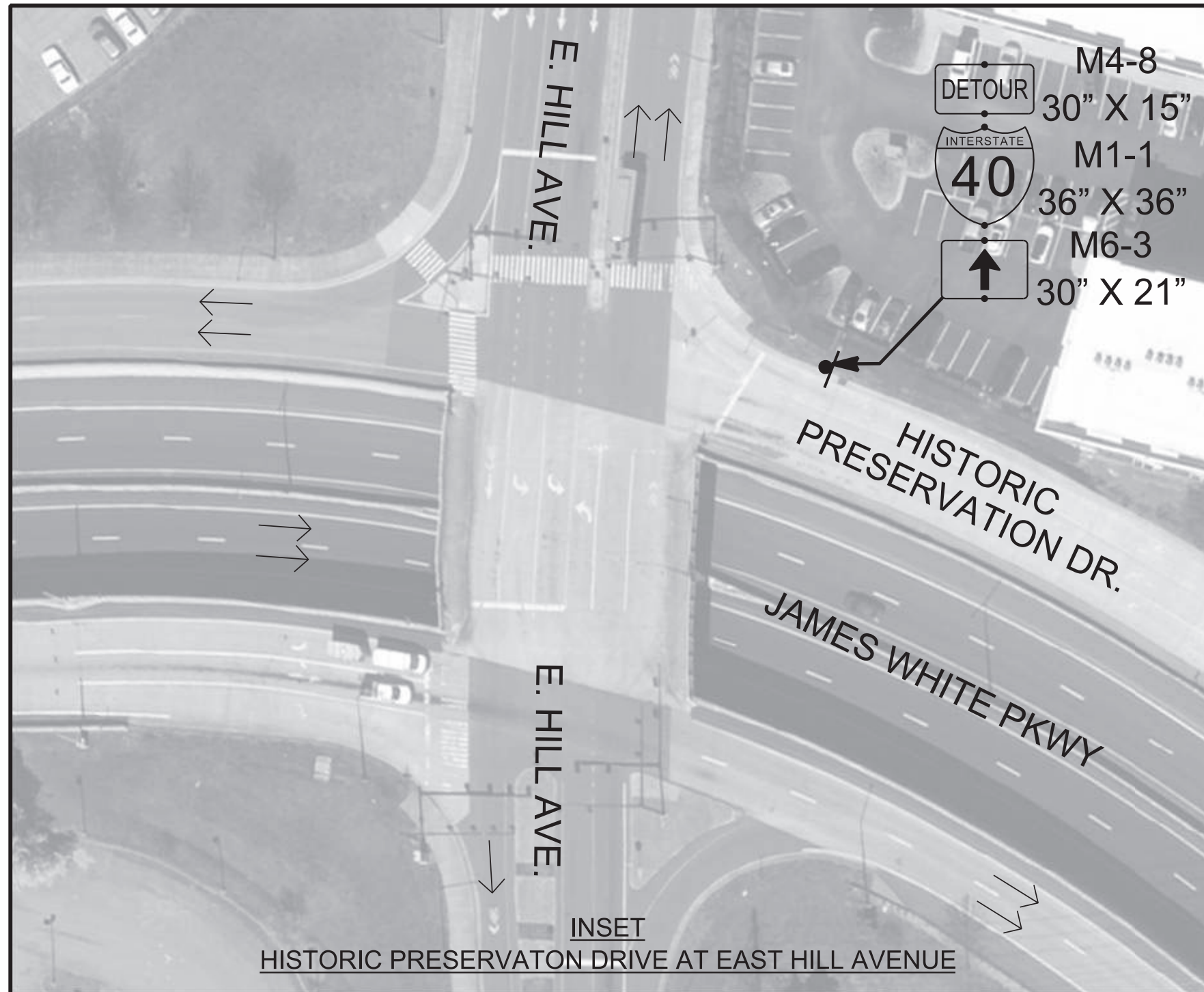
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| TYPE | YEAR | PROJECT NO. | SHEET NO. |
|------|------|---------------|-----------|
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| PS&E | 2026 | 471040-M3-012 | 5S |

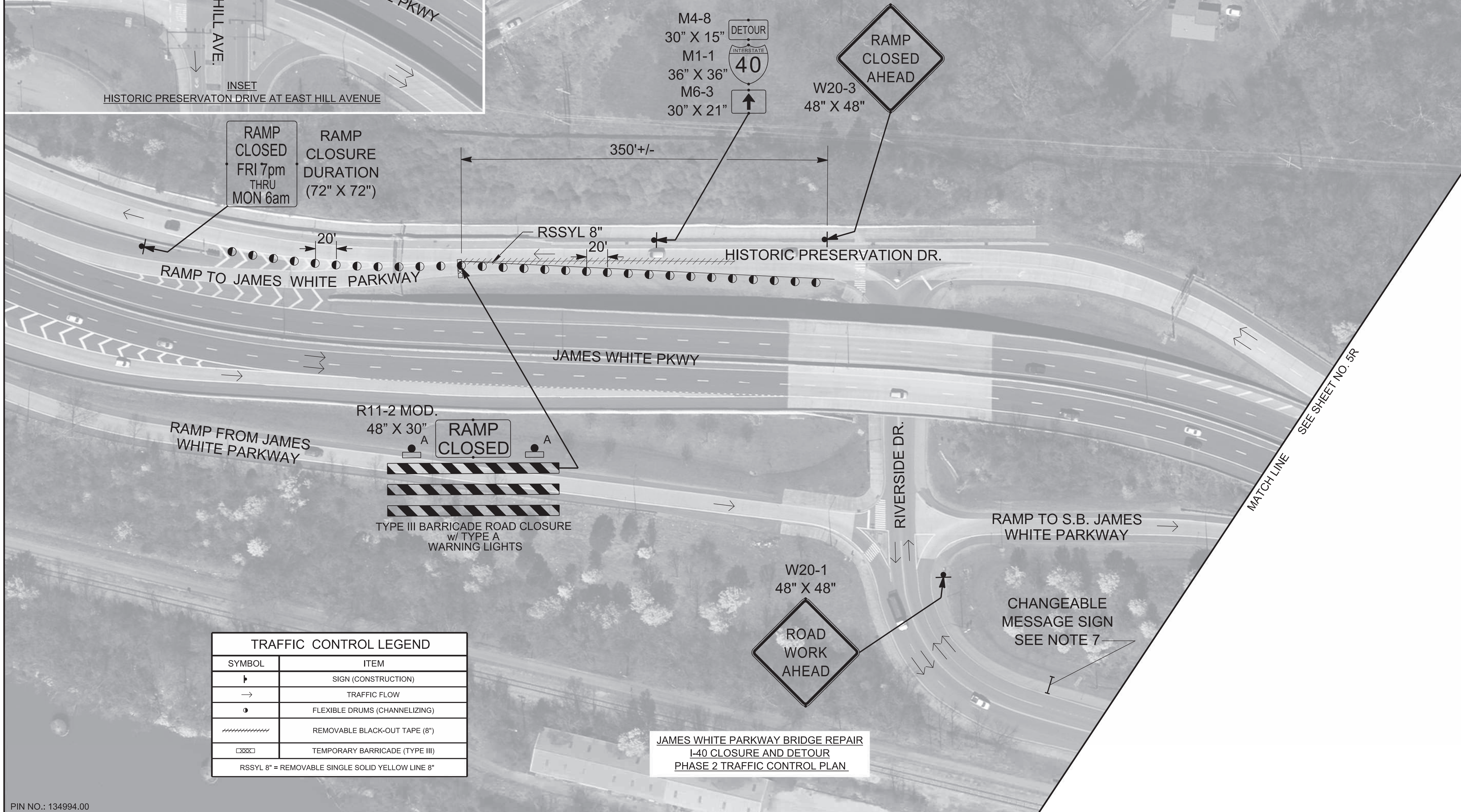
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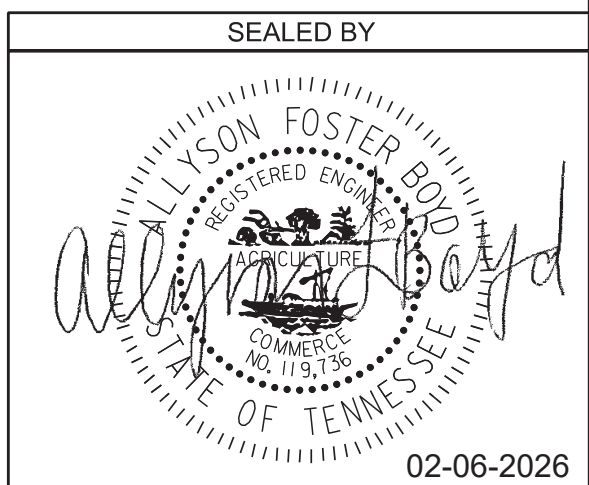


INSET
HISTORIC PRESERVATION DRIVE AT EAST HILL AVENUE



| TRAFFIC CONTROL LEGEND | |
|--|--------------------------------|
| ▬ | SIGN (CONSTRUCTION) |
| → | TRAFFIC FLOW |
| ● | FLEXIBLE DRUMS (CHANNELIZING) |
| //// | REMOVABLE BLACK-OUT TAPE (8") |
| □ | TEMPORARY BARRICADE (TYPE III) |
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JAMES WHITE PARKWAY BRIDGE REPAIR
I-40 CLOSURE AND DETOUR
PHASE 2 TRAFFIC CONTROL PLAN



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLAN
BR. NO. 47-10040-19.30
PHASE 2
1"=50'

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PIN NO.: 134994.00

| PROJECT NO. | YEAR | SHEET NO. | |
|---------------|------|------------|-------------------|
| 471040-M3-012 | 2026 | BR-133-304 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| | | | |
| | | | |
| | | | |
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LIST OF BRIDGE DRAWINGS

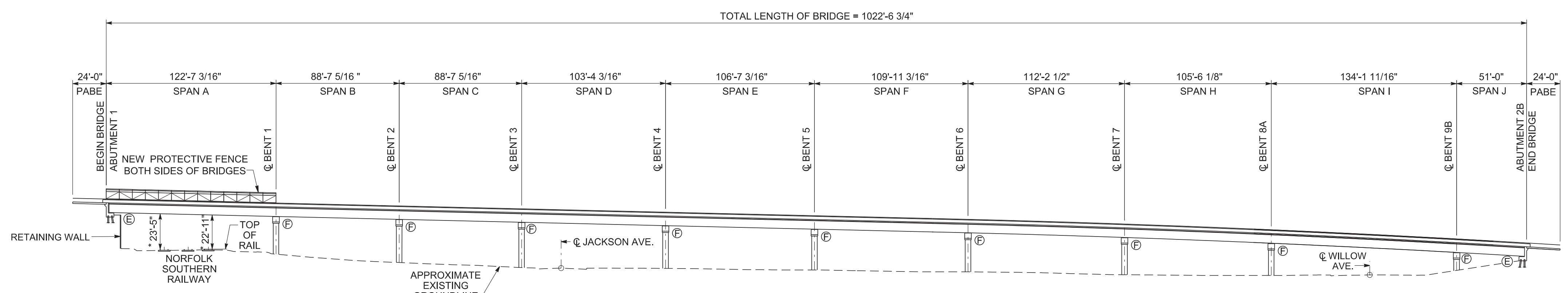
| DRAWING | DWG. NO. | REV. DATE |
|---|------------|-----------|
| LAYOUT OF BRIDGE TO BE REPAIRED | BR-133-304 | |
| GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES | BR-133-305 | |
| PHASE 1 CONSTRUCTION | BR-133-306 | |
| PHASE 2 CONSTRUCTION | BR-133-307 | |
| SUPERSTRUCTURE REPAIRS - SPAN A | BR-133-308 | |
| SUPERSTRUCTURE REPAIRS - SPAN J | BR-133-309 | |
| BENT 1 - DIAPHRAGM REPAIRS | BR-133-310 | |
| BENT 3 - DIAPHRAGM REPAIRS | BR-133-311 | |
| BENT 4 - DIAPHRAGM REPAIRS | BR-133-312 | |
| BENT 5 - DIAPHRAGM REPAIRS | BR-133-313 | |
| BENT 6 - DIAPHRAGM REPAIRS | BR-133-314 | |
| BENT 8A AND 8B - DIAPHRAGM REPAIRS | BR-133-315 | |
| BENT 9A, 9B, AND 9C - DIAPHRAGM REPAIRS | BR-133-316 | |
| JOINT REPAIR DETAILS | BR-133-317 | |
| CONCRETE REPAIR DETAILS | BR-133-318 | |

LIST OF BRIDGE REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)
M-472-123 THRU M-472-125, M-472-142 THRU M-472-158,
M-472-160A, M-472-166, M-472-171 THRU M-472-173, M-472-186, M-472-187,
M-472-190, M-472-191, M-472-193, M-472-194, M-472-200, M-472-201A,
M-472-201B, M-472-201C AND STANDARD DWG. STD-1-5 (REV. 07-31-00)

STANDARD BRIDGE DRAWINGS

| DRAWING | REV. DATE | DESCRIPTION |
|---------|-----------|-----------------------------------|
| STD-8-5 | 10-01-25 | STANDARD PROTECTIVE FENCE DETAILS |



ELEVATION

(SCALE: 1" = 50')

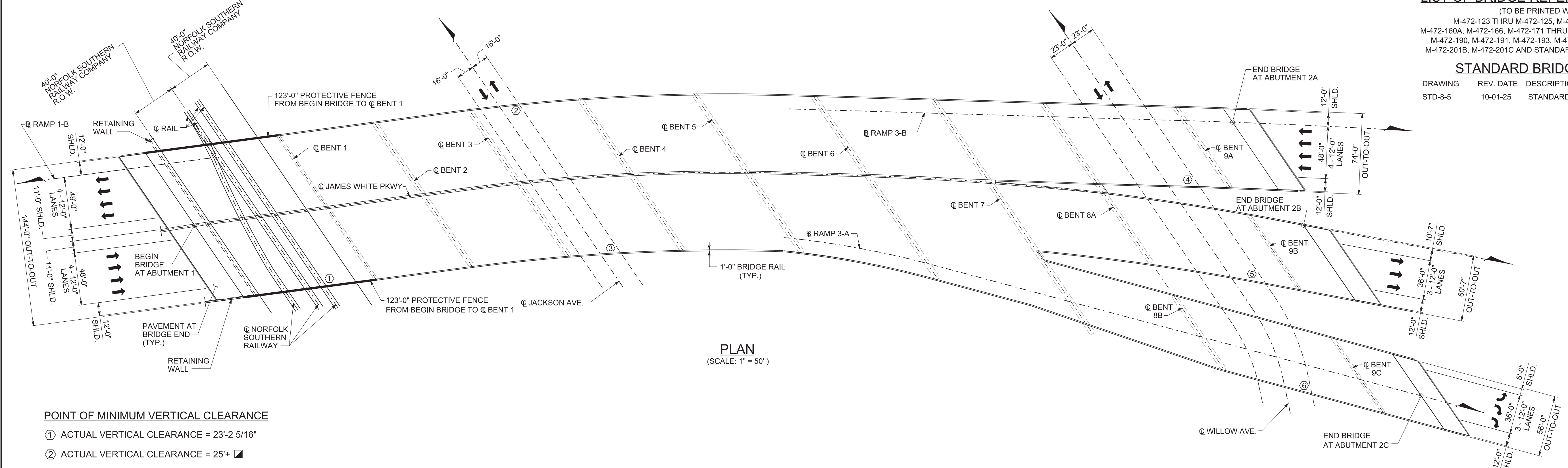
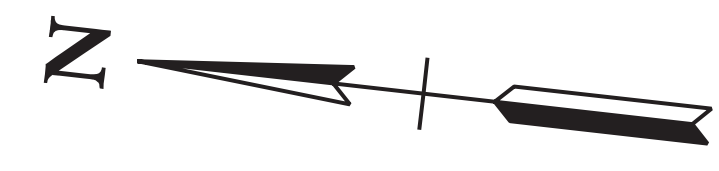
ALL DIMENSIONS ARE ALONG C SURVEY JAMES WHITE PKWY. UNLESS OTHERWISE NOTED ELEVATION VIEW PROJECTED FROM PLAN VIEW

ⓔ - DENOTES EXPANSION
ⓕ - DENOTES FIXED

*EXISTING MINIMUM VERTICAL CLEARANCE (MINIMUM ALLOWABLE TEMPORARY VERTICAL CLEARANCE IS 22'-0")

NOTES:

- EXISTING SUBSTANDARD CLEARANCES SHALL NOT BE FURTHER REDUCED FOR THE TEMPORARY CONSTRUCTION CONDITION WITHOUT WRITTEN PERMISSION FROM NORFOLK SOUTHERN.
- THE MINIMUM TEMPORARY CLEARANCES FOR USE DURING CONSTRUCTION ARE 14'-0" (HORIZONTAL) AND 22'-0" VERTICAL IN ACCORDANCE WITH H.1.2 OF THE NS PUBLIC PROJECTS MANUAL.



PLAN

(SCALE: 1" = 50')

POINT OF MINIMUM VERTICAL CLEARANCE

- ① ACTUAL VERTICAL CLEARANCE = 23'-2 5/16"
- ② ACTUAL VERTICAL CLEARANCE = 25'+
- ③ ACTUAL VERTICAL CLEARANCE = 25'+
- ④ ACTUAL VERTICAL CLEARANCE = 19'-5"
- ⑤ ACTUAL VERTICAL CLEARANCE = 14'-5"
- ⑥ ACTUAL VERTICAL CLEARANCE = 14'-10"

☑ PER TDOT INSPECTION REPORT 11-04-2022

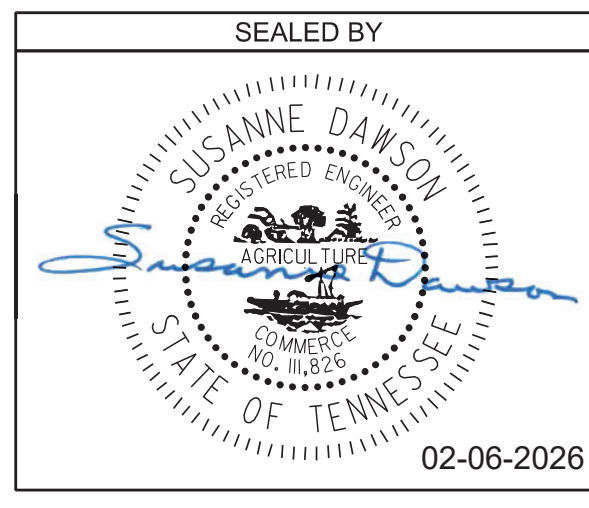
MIN. VERTICAL CLEARANCE FOR NORFOLK SOUTHERN RAILWAY = 23'-0"

MIN. VERTICAL CLEARANCE FOR JACKSON AVE. AND WILLOW AVE. = 14'-6"

SCOPE OF WORK

- PROVIDE REQUIRED TRAFFIC CONTROL AND PHASE CONSTRUCTION FOR WEEKEND CLOSURES OF SOUTH BOUND JAMES WHITE PARKWAY AND RAMP 3-A TO SUMMIT HILL DRIVE IN PHASE 1 AND WEEKEND CLOSURES OF NORTH BOUND JAMES WHITE PARKWAY IN PHASE 2.
- RECONSTRUCT THE CONCRETE BRIDGE DECKS IN AREAS OF PARTIAL DEPTH DECK REPAIR UTILIZING 18 HOUR HIGH EARLY STRENGTH CONCRETE.
- REMOVE AND REPLACE EXPANSION JOINTS AND HEADERS AT ABUTMENT 2A, 2B, AND 2C PABES WITH PREFORMED ELASTOMERIC COMPRESSION JOINT WITH ELASTOMERIC CONCRETE HEADERS.
- REMOVE AND REPLACE EXPANSION JOINT AT ABUTMENT 1 PABE WITH PREFORMED ELASTOMERIC COMPRESSION JOINT.
- INSTALL NEW PROTECTIVE FENCING ON BRIDGE RAIL OVER THE RAILWAY.
- SHOTBLAST BRIDGE DECKS TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPPING, OR ANY OTHER DETRIMENTAL MATERIAL.
- PLACE TYPE 1 THIN EPOXY OVERLAY SYSTEM OVER ENTIRE DECKS INCLUDING PABES.
- REPAIR DETERIORATED AREAS OF CONCRETE ON PRESTRESSED PRECAST CONCRETE GIRDERS AND DIAPHRAGMS, AND APPLY CONCRETE STAIN TO MATCH EXISTING.
- TEXTURE COAT CONCRETE SPALLS ON SUBSTRUCTURE UNITS.
- CLEAN AND MAINTAIN END OF BRIDGE DRAINS (COST TO BE INCLUDED IN ITEMS BID ON).
- HIGH PRESSURE WATER WASH AND TEXTURE COAT ALL EXPOSED CONCRETE SURFACES OF PARAPETS AND MEDIAN BARRIERS. TOP AND INSIDES FACE OF PARAPET AND MEDIAN BARRIERS SHALL BE WHITE. ALL OTHER SURFACES SHALL BE MOUNTAIN GREY.
- PLACE NEW RUMBLE STRIPS, SNOW PLOWABLE PAVEMENT MARKINGS, STRIPING AND SIGNING AS APPROPRIATE.

| TRAFFIC DATA | |
|--------------|--|
| S.R. 158 | POSTED SPEED LIMIT = 55 M.P.H. AADT (2024) = 40,798 AADT (2024) NB = 20,956 AADT (2024) SB = 19,843 |



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF BRIDGE TO BE REPAIRED
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK SOUTHERN RAILWAY, WILLOW AVENUE, AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-304

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GENERAL NOTES:

SPECIFICATIONS & LOADING

CONSTRUCTION SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1, 2021 EDITION), THE 4TH EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS, AND NORFOLK SOUTHERN PUBLIC PROJECTS MANUAL.

STEEL, CONCRETE, REINFORCING, AND FORMING

HIGH EARLY STRENGTH CONCRETE: THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS X. THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH ±2% ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3±1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. TRAFFIC SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF 18 HOURS.

CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

MISCELLANEOUS GENERAL NOTES

DEMOLITION: THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. FOR FULL DEPTH SLAB REMOVAL, EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH SLAB REMOVAL AND ANY WORK OVER THE BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS; CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. EXPANSION JOINT REMOVAL SHALL FOLLOW THE SAME RESTRICTIONS AS FULL DEPTH SLAB REMOVAL. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.

THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.

SPECIAL NOTE FOR RAILROAD CROSSINGS: THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY THE NORFOLK SOUTHERN RAILWAY SO AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAFFIC.

PROTECTIVE FENCE: RAILROAD PROTECTIVE FENCE IS REQUIRED TO BE BUILT IN ACCORDANCE WITH STANDARD DRAWING STD-8-5. DIMENSION "H" AS SHOWN ON STANDARD DRAWING STD-8-5 SHALL BE 10'.

QUICK-SET PATCHING MATERIAL: QUICK-SET PATCHING MATERIAL SHALL BE A POLYMER MODIFIED CEMENTITIOUS PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13.009 POLY MOD CEMENT STRUCT PATCH VERT & OVER FOR ACCEPTABLE PATCHING MATERIALS.

RAILROAD NOTES

THE CONTRACTOR SHALL CONDUCT HIS WORK SO AS TO PROTECT THE RAILROAD'S TRACKS AND PROPERTIES FROM ANY DAMAGE. THE WORK SHALL BE DONE IN ACCORDANCE WITH REGULATIONS STIPULATED BY NORFOLK SOUTHERN RAILWAY AS TO MAINTAIN CLEARANCE AND NOT INTERRUPT TRAFFIC IN ANY MANNER.

FOR A GENERAL LIST OF ANTICIPATED CONSTRUCTION SUBMISSIONS, PLEASE REVIEW SECTION 6.A.2 OF THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR PROTECTION OF RAILWAYS INTERESTS. A LIST OF REQUIRED SUBMISSIONS WILL BE PROVIDED AT THE PRECONSTRUCTION MEETING.

THE CONTRACTOR SHALL USE EXTREME CARE AND TAKE ANY MEASURES NECESSARY TO PREVENT DEBRIS FROM FALLING ONTO THE RAILROAD'S RIGHT-OF-WAY. THE METHOD PROPOSED TO ACCOMPLISH THIS MUST BE SUBMITTED TO NORFOLK SOUTHERN FOR REVIEW AND APPROVAL PRIOR TO USE AND MUST NOT INFRINGE ON THE VERTICAL AND/OR HORIZONTAL CLEARANCES IN THESE PLANS. THE TEMPORARY MINIMUM VERTICAL AND HORIZONTAL CONSTRUCTION CLEARANCES THAT THE CONTRACTOR MUST MEET AT ALL TIMES ARE LISTED IN SECTION 5.A OF THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR PROTECTION OF RAILWAYS INTERESTS, WHICH CRITERIA ARE INCLUDED IN THE SPECIAL PROVISION 105C. NORFOLK SOUTHERN CORPORATION WILL NOT ALLOW TRACK OR TRACKS TO BE OBSTRUCTED WITH AT GRADE CRANE MAT PROTECTION. COMPLETE AND FULL ENCLOSURE OF STRUCTURE DEMOLITION WITHIN ALLOWABLE CLEARANCES WILL BE REQUIRED. THE COST OF REMOVING AND DISPOSING OF DEBRIS, AND THE COMPLETE AND FULL PROTECTION OF THE CONTRACTOR'S WORK AREA FOR DEMOLITION SHALL BE INCLUDED IN UNIT ITEMS BID ON.

CONTRACTOR PROTECTIVE SERVICES SHALL BE ONSITE ANYTIME CONSTRUCTION ACTIVITIES ARE TAKING PLACE ON OR ADJACENT TO THE RAILROAD PROPERTY AND/OR HAVE THE POTENTIAL TO FOUL THE RAILROAD'S TRACK OR OPERATIONS. SEE SECTION 8 OF THE NORFOLK SOUTHERN SPECIAL PROVISIONS FOR PROTECTION OF RAILWAYS INTERESTS - DIRECT HIRE FOR ADDITIONAL CLARIFICATIONS AND REQUIREMENTS REGARDING CONTRACTOR PROTECTION SERVICES.

NO DRAINAGE, TEMPORARY OR PERMANENT, MAY BE DIVERTED ON TO NORFOLK SOUTHERN PROPERTY, HOWEVER, EXISTING DRAINS MAY REMAIN WITHOUT MODIFICATIONS.

PER SECTION H.1.5.J OF THE NORFOLK SOUTHERN PUBLIC PROJECTS MANUAL, NO DRAINAGE IS PERMITTED TO DRAIN ON RAILROAD PROPERTY. THIS INCLUDES ANY WASH WATER FROM THE CLEANING OF EXPOSED CONCRETE SURFACES.

ALL WORK ON, OVER, UNDER, OR ADJACENT TO NORFOLK SOUTHERN (NS) RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH THE NORFOLK SOUTHERN "SPECIAL PROVISIONS FOR THE PROTECTION OF RAILWAY INTERESTS" (NS SPECIAL PROVISIONS).

THE CONTRACTOR WILL NOT BE PERMITTED TO STORE ANY EQUIPMENT ON NORFOLK SOUTHERN PROPERTY WITHOUT PERMISSION FROM THE NORFOLK SOUTHERN RAILROAD ENGINEER IN ACCORDANCE WITH SECTION E.5.K.1 OF THE NORFOLK SOUTHERN PUBLIC PROJECTS MANUAL.

"ONE CALL" SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTRACT THE RAILROAD'S REPRESENTATIVE TWO (2) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY, UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.

RAILROAD NOTES (CONT.)

ALL UTILITY INSTALLATIONS OR RELOCATIONS THAT ARE REQUIRED IN CONJUNCTION WITH THIS PROJECT CAN BE INSTALLED OR RELOCATED AS PART OF THE PROJECT PROVIDED THE CONSTRUCTION IS PERFORMED BY THE PROJECT CONTRACTOR OR PROJECT CONTRACTOR'S SUB-CONTRACTOR. HOWEVER, THE UTILITY MUST SUBMIT AN APPLICATION FOR THE INSTALLATION OR RELOCATION TO NORFOLK SOUTHERN PIPE AND WIRE FOR APPROPRIATE HANDLING FOR LICENSE AGREEMENT AND APPLICABLE FEES. FOR UTILITY APPLICATIONS GO TO: [HTTP://WWW.NSCORP.COM/CONTENT/NSCORP/EN/REAL-ESTATE/NORFOLK-SOUTHERN-SERVICES/WIRE-PIPELINE-FIBER-OPTIC-PROJECTS.HTML](http://www.nscorp.com/content/nscorp/en/real-estate/norfolk-southern-services/wire-pipeline-fiber-optic-projects.html). NOTE: LICENSE AGREEMENT MUST BE EXECUTED PRIOR TO UTILITY BEING INSTALLED OR RELOCATED.

NORFOLK SOUTHERN CORPORATION CONTACT:

SHAWN STARLING, P.E.
SENIOR ENGINEER PUBLIC IMPROVEMENTS
NORFOLK SOUTHERN RAILWAY COMPANY
ENGINEERING - DESIGN & CONSTRUCTION
650 WEST PEACHTREE STREET NW - BOX 45
ATLANTA, GA 30308
PHONE: (470) 463-6721
EMAIL: DOUGLAS.STARLING@NSCORP.COM

CHRIS DELGALLO, DIVISION ENGINEER
1400 NORFOLK SOUTHERN DRIVE
BIRMINGHAM, AL 35210
PHONE: (330) 663-6644
EMAIL: CHRISTOPHER.DELGALLO@NSCORP.COM

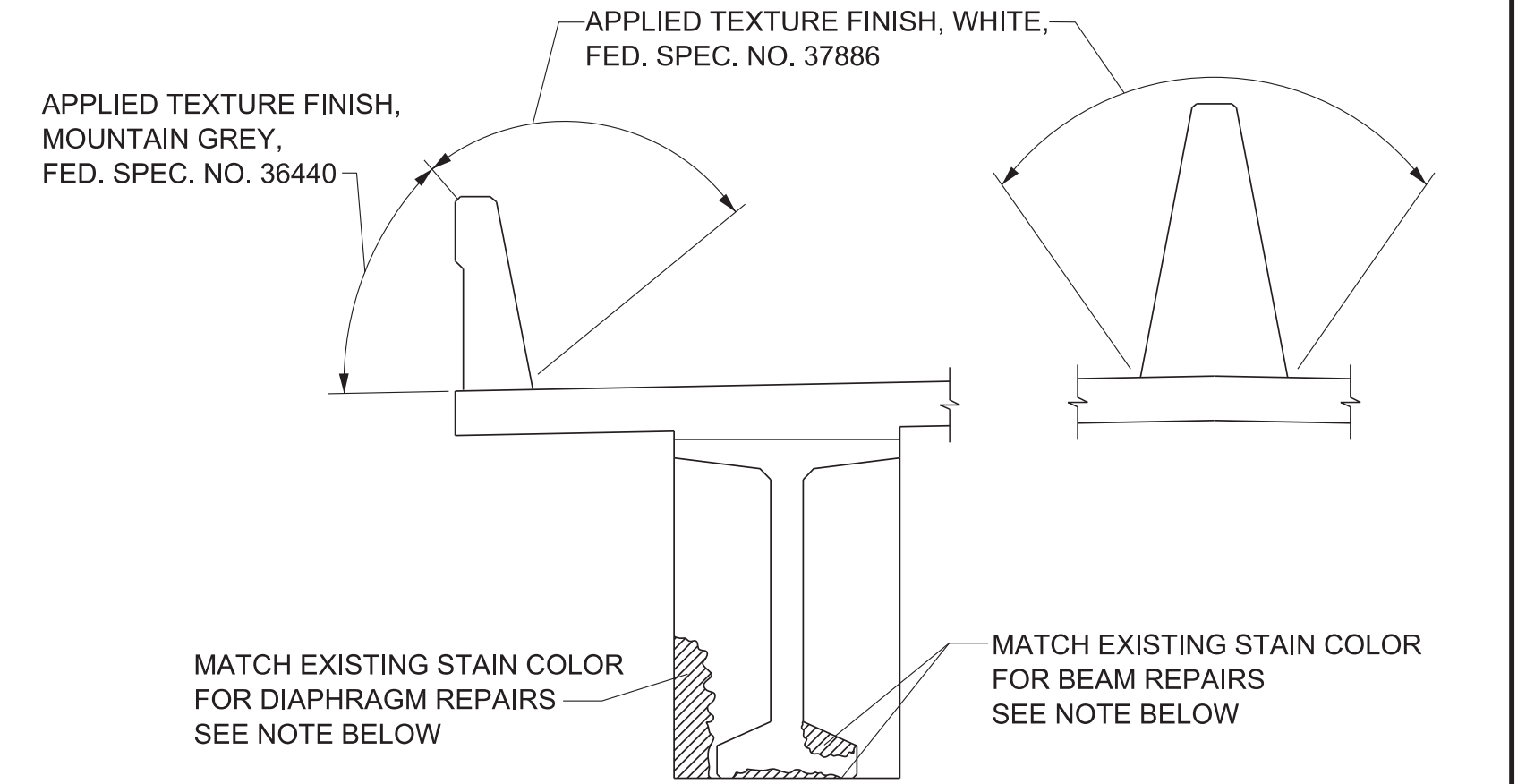
| ESTIMATED BRIDGE QUANTITIES | | | | |
|-----------------------------|--|------|----------------|-------|
| ITEM NO. | DESCRIPTION | UNIT | 47-10040-19.30 | TOTAL |
| 1 | 604-04.02 APPLIED TEXTURE FINISH (EXISTING STRUCTURES) | S.Y. | 3265 | 3265 |
| 2 | 604-10.05 CONCRETE | S.F. | 71 | 71 |
| 3 | 604-10.42 CONCRETE REPAIRS | C.F. | 3 | 3 |
| 4 | 604-10.44 EXPANSION JOINT REPAIRS | L.F. | 402 | 402 |
| | 604-10.50 BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB) | S.Y. | 10 | 10 |
| 5 | 604-10.54 CONCRETE REPAIRS | S.F. | 71 | 71 |
| | 617-04.01 TYPE 1 THIN OVERLAY (EPOXY URETHANE) | S.Y. | 18841 | 18841 |
| | 707-07.01 CHAIN-LINK FENCE (BRIDGES) | S.F. | 1845 | 1845 |

* DENOTES ITEM CAN BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

FOOTNOTES

- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE APPLICATION OF TEXTURE COATING FOR PARAPETS, MEDIAN, AND ALL SPALL REPAIR AREAS AS SHOWN IN THE TEXTURE COATING DETAIL ON THIS SHEET. ALSO INCLUDES SURFACE PREPARATION USING A HIGH PRESSURE WASH TO REMOVE ALL LOOSE COATINGS, FLAKING, AND OTHER FOREIGN SUBSTANCES AND CONTAINMENT SCREENS OR OTHER MEASURES TO THE FULL SATISFACTION OF THE ENGINEER.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY TO PLACE HIGH EARLY STRENGTH CONCRETE FOR REPAIR OF INDICATED AREAS.
- INCLUDES COST OF ALL LABOR, MATERIALS, AND INCIDENTALS TO PERFORM BEAM REPAIRS AS SHOWN ON DWG. NOS. BR-133-308 AND BR-133-309 UTILIZING HIGH EARLY STRENGTH CONCRETE.
- INCLUDES THE COST OF ALL LABOR AND MATERIALS NECESSARY TO REPLACE PABE EXPANSION JOINT SYSTEM AT ABUTMENT NO. 1, ABUTMENT NO. 2A, ABUTMENT NO. 2B, AND ABUTMENT NO. 2C AS SHOWN ON DWG. NO. BR-133-317.
- INCLUDES THE COST OF ALL LABOR AND MATERIALS NECESSARY TO PLACE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL FOR REPAIR OF INDICATED AREAS.

| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-305 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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APPLIED TEXTURE FINISH SKETCH

LEFT SIDE ONLY SHOWN. SKETCH IS SYMMETRIC ABOUT C/ BRIDGE.

HIGH PRESSURE WATER WASH ALL EXPOSED EXISTING CONCRETE SURFACES AND TEXTURE COAT ALL EXPOSED CONCRETE SURFACES OF PARAPETS AND MEDIAN BARRIER. TOP AND INSIDES FACE OF PARAPETS AND MEDIAN BARRIER SHALL BE WHITE (AMS-STD-595A, COLOR NO. 37886). ALL OTHER SURFACES SHALL BE MOUNTAIN GREY (AMS-STD-595A, COLOR NO. 36440).

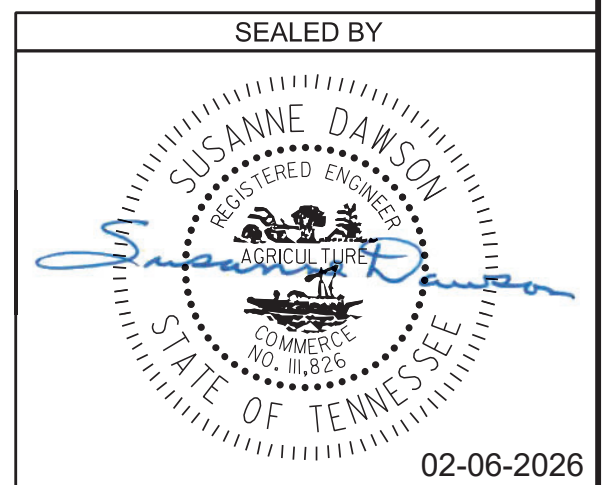
THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.

THE EXISTING SURFACES THAT ARE TO RECEIVE A TEXTURE FINISH SHALL BE FREE OF ALL EFFLORESCENCE, FLAKING TEXTURE COATING, RUST, DIRT, OIL, AND OTHER FOREIGN SUBSTANCES PRIOR TO THE APPLICATION OF THE TEXTURE FINISH. THE SURFACE SHALL BE CLEANED TO THE COMPLETE SATISFACTION OF THE ENGINEER USING A HIGH PRESSURE WATER WASH. COST TO BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.

THE WASH WATER IS TO BE FILTERED AND PAINT CHIPS AND DEBRIS COLLECTED PRIOR TO RELEASE OF WATER.

CONCRETE STAIN COLOR SHALL MATCH THE EXISTING BEAM AND DIAPHRAGM. FINAL COLOR NUMBER TO BE APPROVED BY DIVISION OF STRUCTURES. COST TO BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.

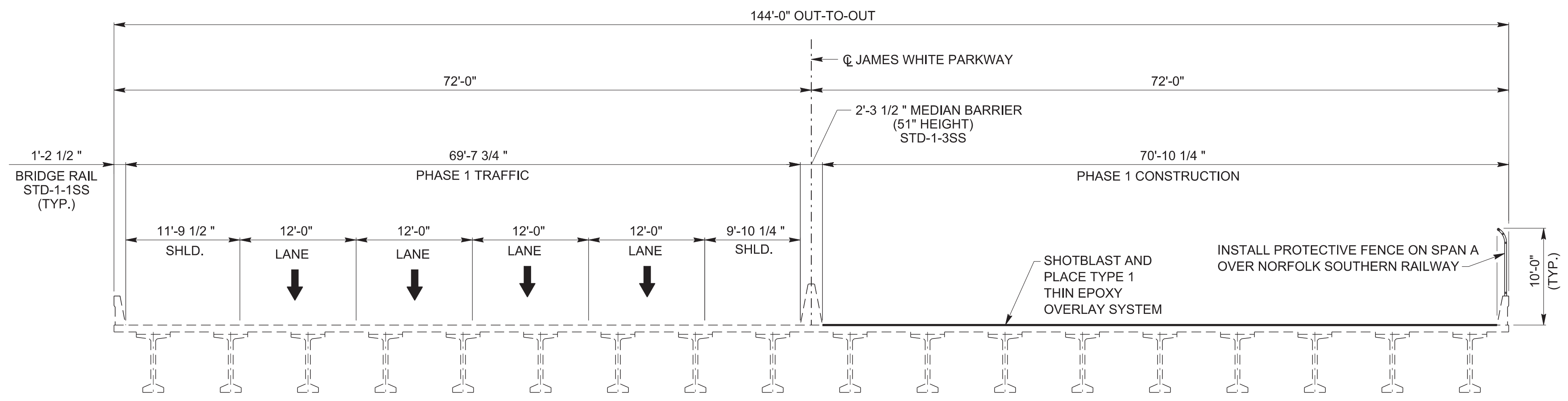
COST OF TEXTURE COATING AND STAIN FOR DIAPHRAGM AND BEAM REPAIRS SHALL BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.



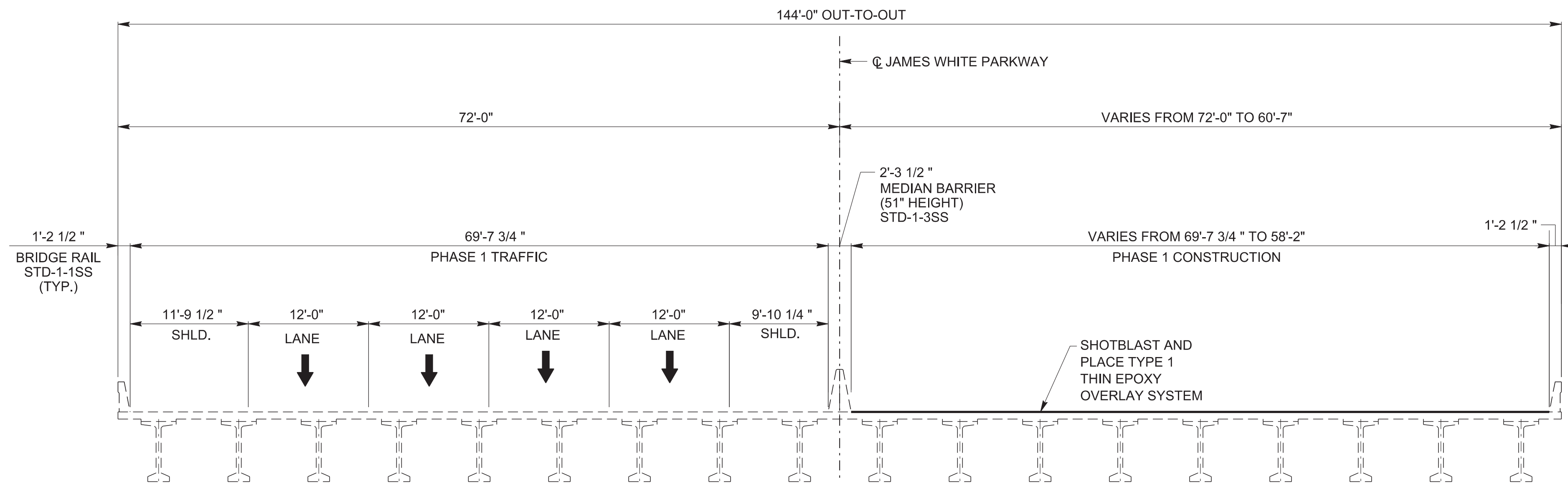
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
GENERAL NOTES AND
ESTIMATED BRIDGE QUANTITIES
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-305

| | | | |
|----------------|--------------------|-------|------------|
| PIN NO.: | 134994.00 | DATE: | MARCH 2025 |
| DESIGN BY: | BRAD WARREN | DATE: | MARCH 2025 |
| DRAWN BY: | T. PELOW/D. PICKEL | DATE: | MARCH 2025 |
| SUPERVISED BY: | SUSANNE DAWSON | DATE: | MARCH 2025 |
| CHECKED BY: | FRANK BALE | DATE: | MARCH 2025 |

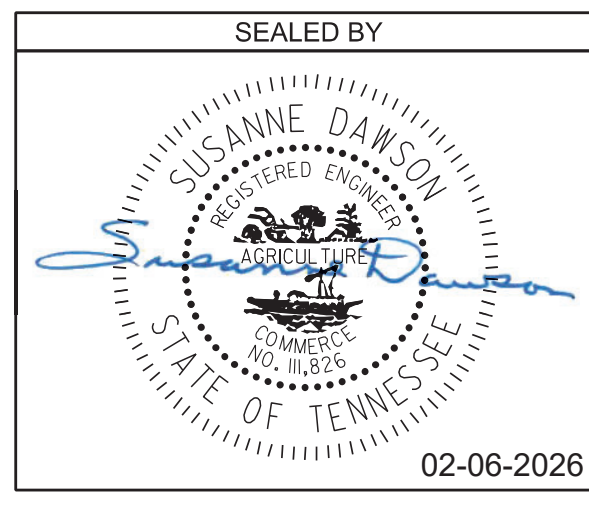
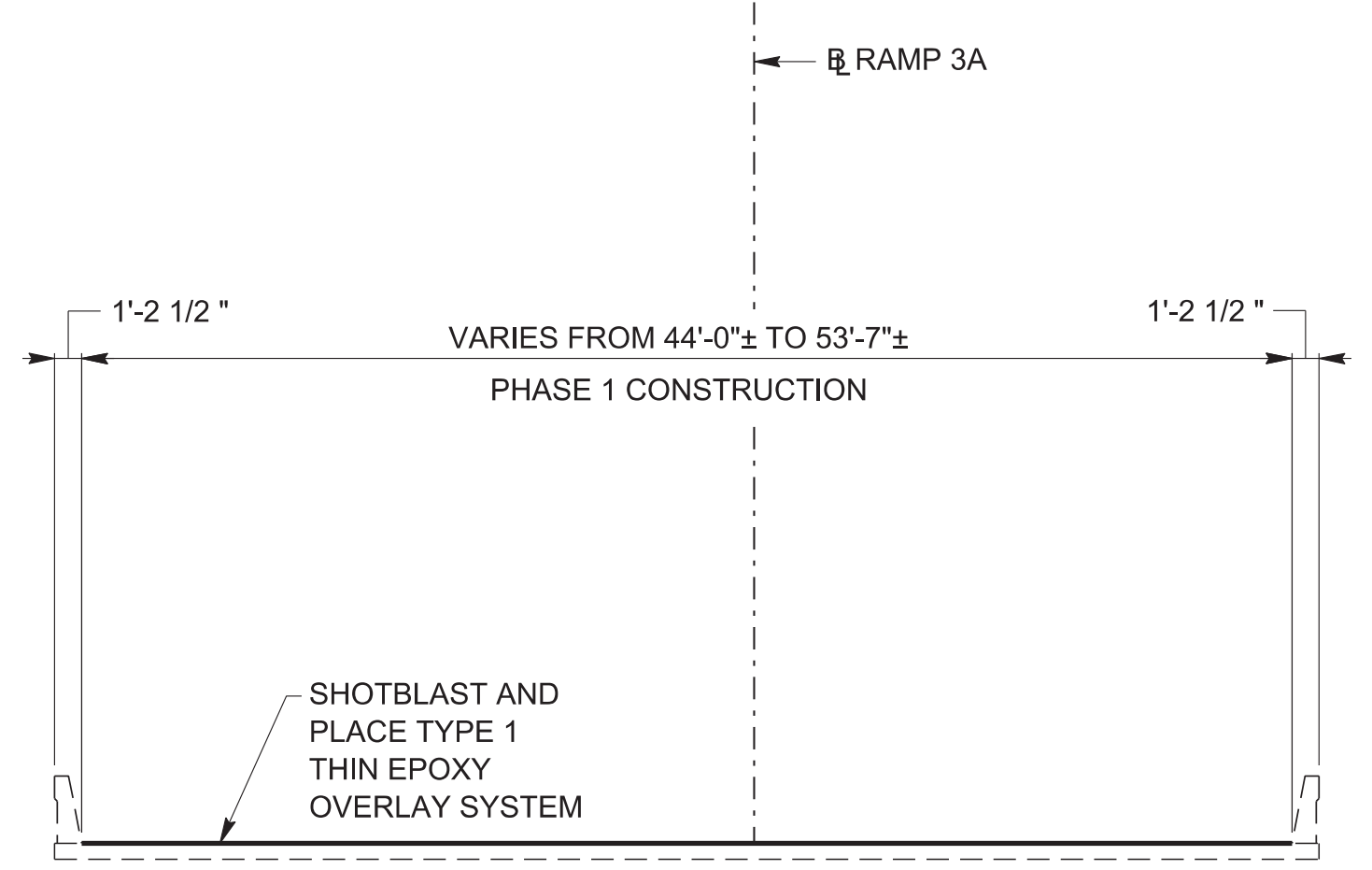
| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-306 | |
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| NO. | DATE | BY | BRIEF DESCRIPTION |
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PHASE 1 CONSTRUCTION
(LOOKING AHEAD ON SURVEY)
FROM BEGIN BRIDGE TO APPROXIMATELY BENT 5



TYPICAL SECTION
(LOOKING AHEAD ON SURVEY)
FROM APPROXIMATELY BENT 5 TO END BRIDGE



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE 1 CONSTRUCTION

BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026

BR-133-306

PIN NO.: 134994.00

DESIGN BY: BRAD WARREN DATE: MARCH 2025

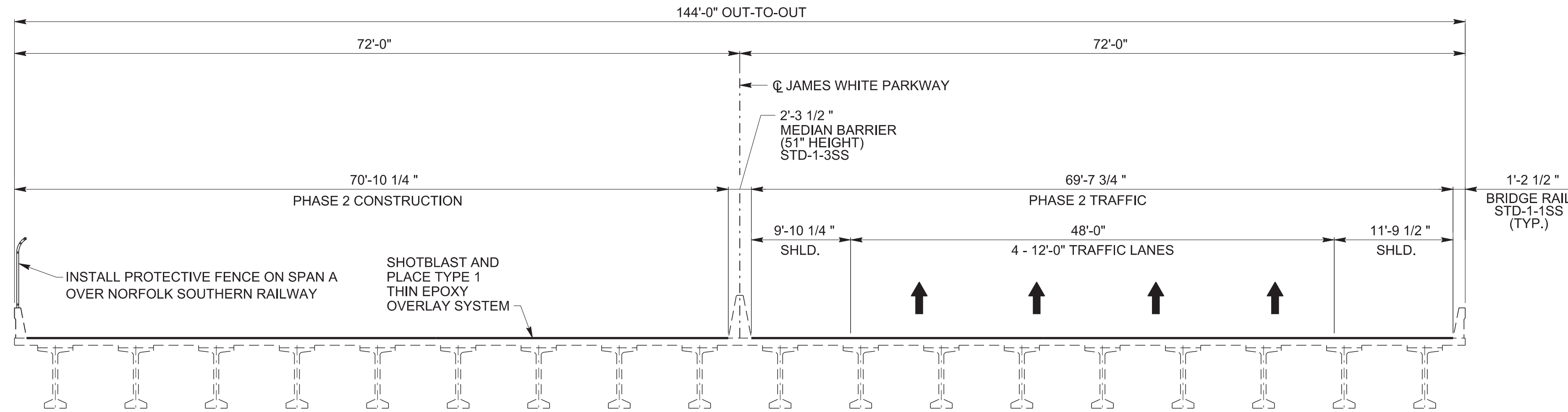
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025

SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025

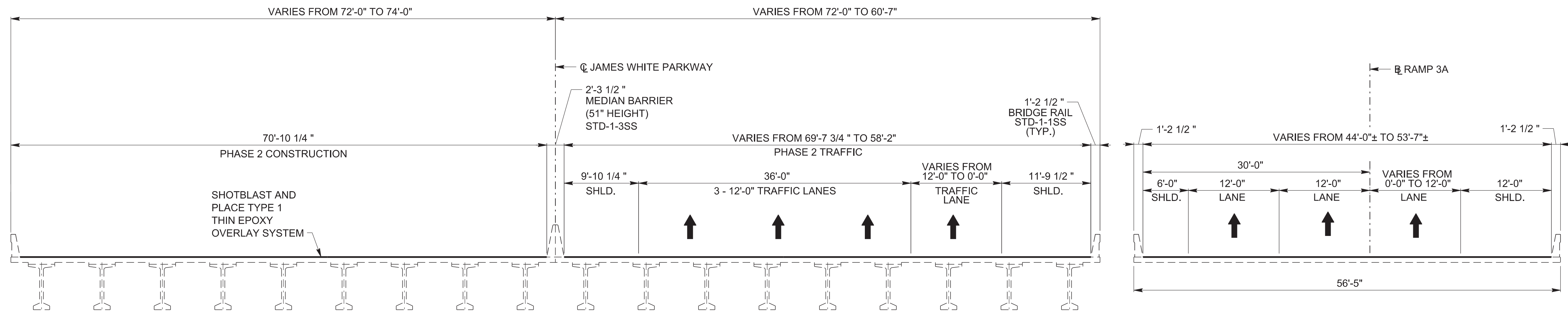
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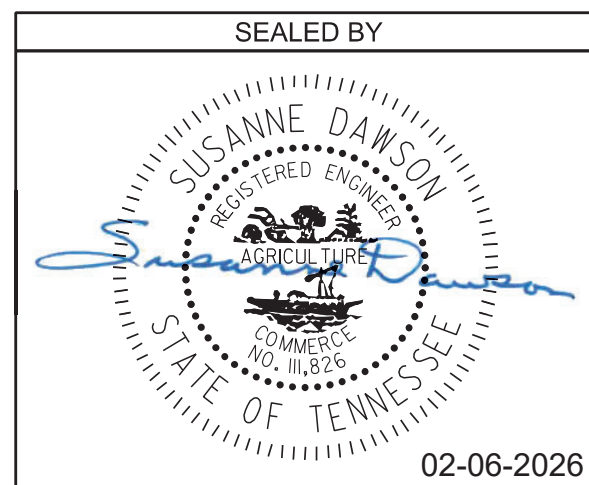
| PROJECT NO. | YEAR | SHEET NO. | |
|---------------|------|------------|-------------------|
| 471040-M3-012 | 2026 | BR-133-307 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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PHASE 2 CONSTRUCTION
(LOOKING AHEAD ON SURVEY)
FROM BEGIN BRIDGE TO APPROXIMATELY BENT 5



PHASE 2 CONSTRUCTION
(LOOKING AHEAD ON SURVEY)
FROM APPROXIMATELY BENT 5 TO END BRIDGE

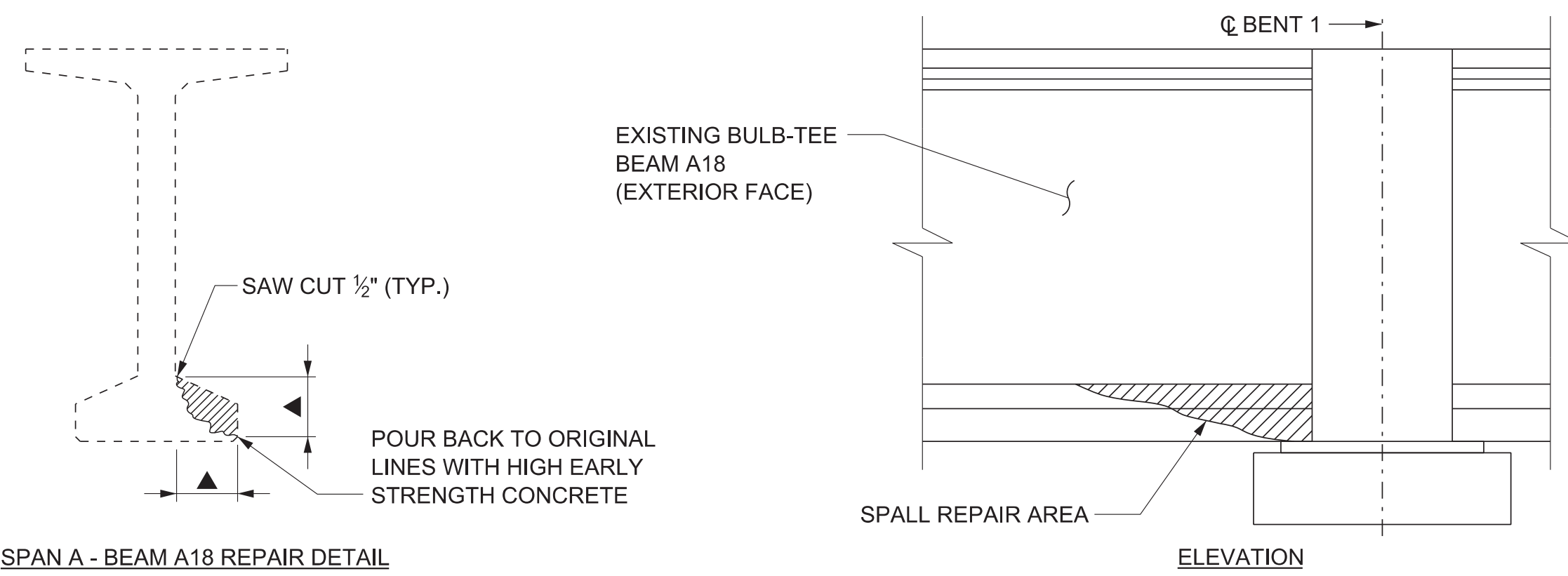


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE 2 CONSTRUCTION
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-307

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

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SPAN A - BEAM A18 REPAIR DETAIL

NOTES:

▲ REMOVE CONCRETE TO A DEPTH OF 3/4" BEHIND ANY EXPOSED REINFORCING STEEL TO ENSURE THAT SOUND CONCRETE IS REACHED IN ALL AREAS. SEE PLAN AND DETAILS THIS SHEET FOR APPROXIMATE LIMITS AND LOCATIONS. FINAL LIMITS AND LOCATIONS OF REPAIRS TO BE DESIGNATED BY THE ENGINEER.


FORM AND POUR REPAIR AREA TO ORIGINAL BEAM LINES USING $F'_c = 7,000$ PSI CONCRETE. AGGREGATE USED SHALL BE SIZE NO. 6 MAXIMUM PER TDOT QUALIFIED PRODUCTS LIST. THE FORMS MAY BE STRIPPED ONLY AFTER CONCRETE HAS OBTAINED A COMPRESSIVE STRENGTH OF NOT LESS THAN 3,000 PSI.

APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.

CONTRACTOR TO TAKE EXTREME CARE WHEN REMOVING DELAMINATED CONCRETE SO AS NOT TO DAMAGE EXISTING REINFORCING STEEL OR PRESTRESSING STRANDS. ALL EXISTING REINFORCING STEEL AND PRESTRESSING STRANDS SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING. IF DAMAGE OCCURS, THE ENGINEER SHALL INSPECT THE DAMAGE AND, IF DEEMED NECESSARY, THE CONTRACTOR SHALL REPLACE OR REPAIR THE DAMAGED MEMBER AT NO ADDITIONAL COST.

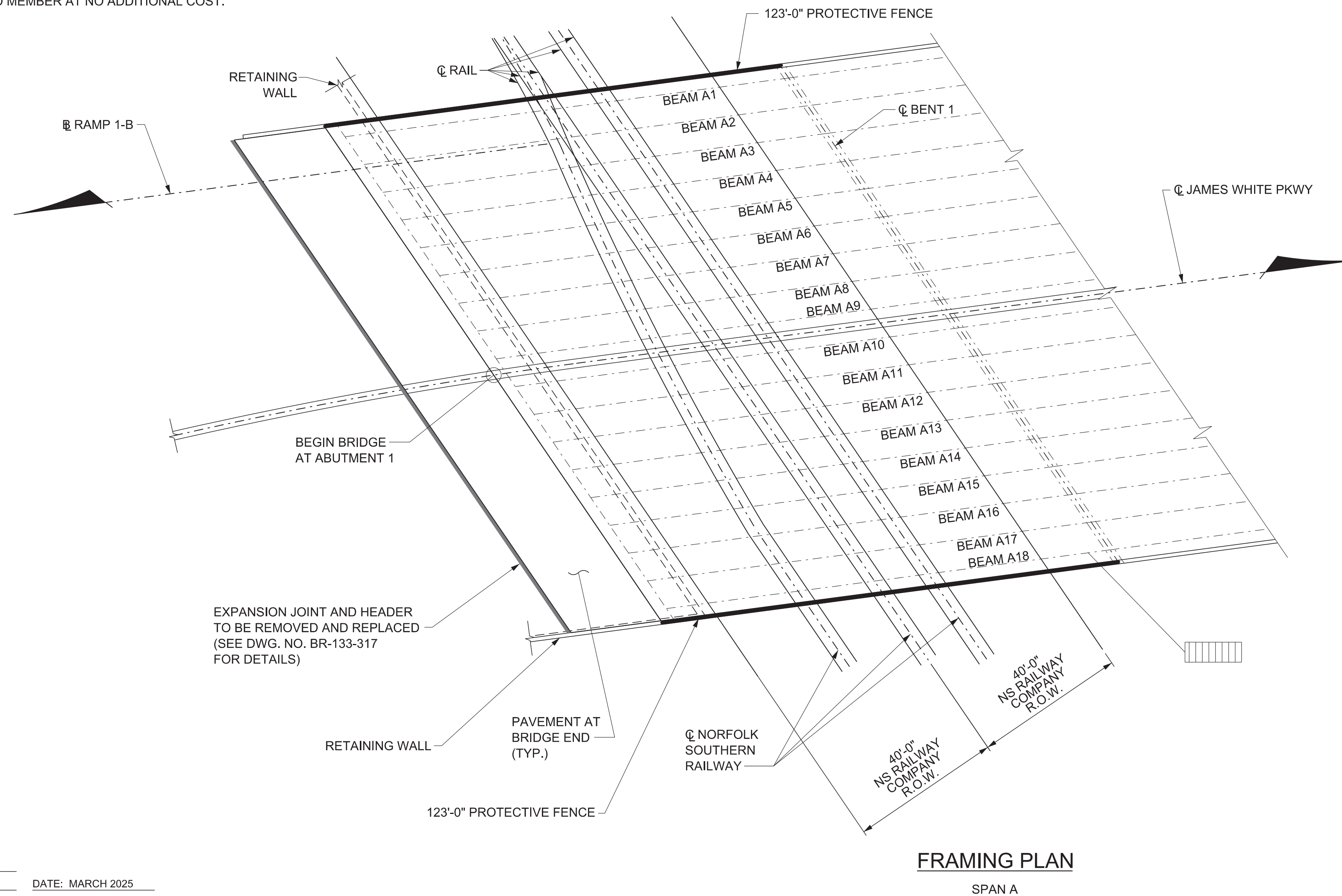
THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS TO THE BEAMS. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE THE BEGINNING OF ANY BEAM REPAIR. COST TO BE INCLUDED IN ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.

THE COST OF REMOVING PORTION OF EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL MISCELLANEOUS MATERIAL NECESSARY TO COMPLETE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.

 DENOTES AREA OF BEAM REPAIR TO BE PAID FOR UNDER ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F. SEE REPAIR DETAILS ON THIS SHEET.

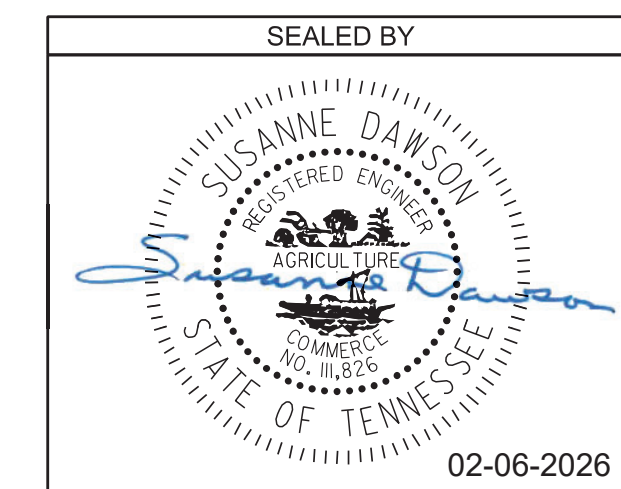
SEE BRIDGE REFERENCE DRAWINGS FOR BEAM FRAMING PLAN AND BEAM NO. NOMENCLATURE

| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-308 | |
| REVISIONS | | | |
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FRAMING PLAN
SPAN A

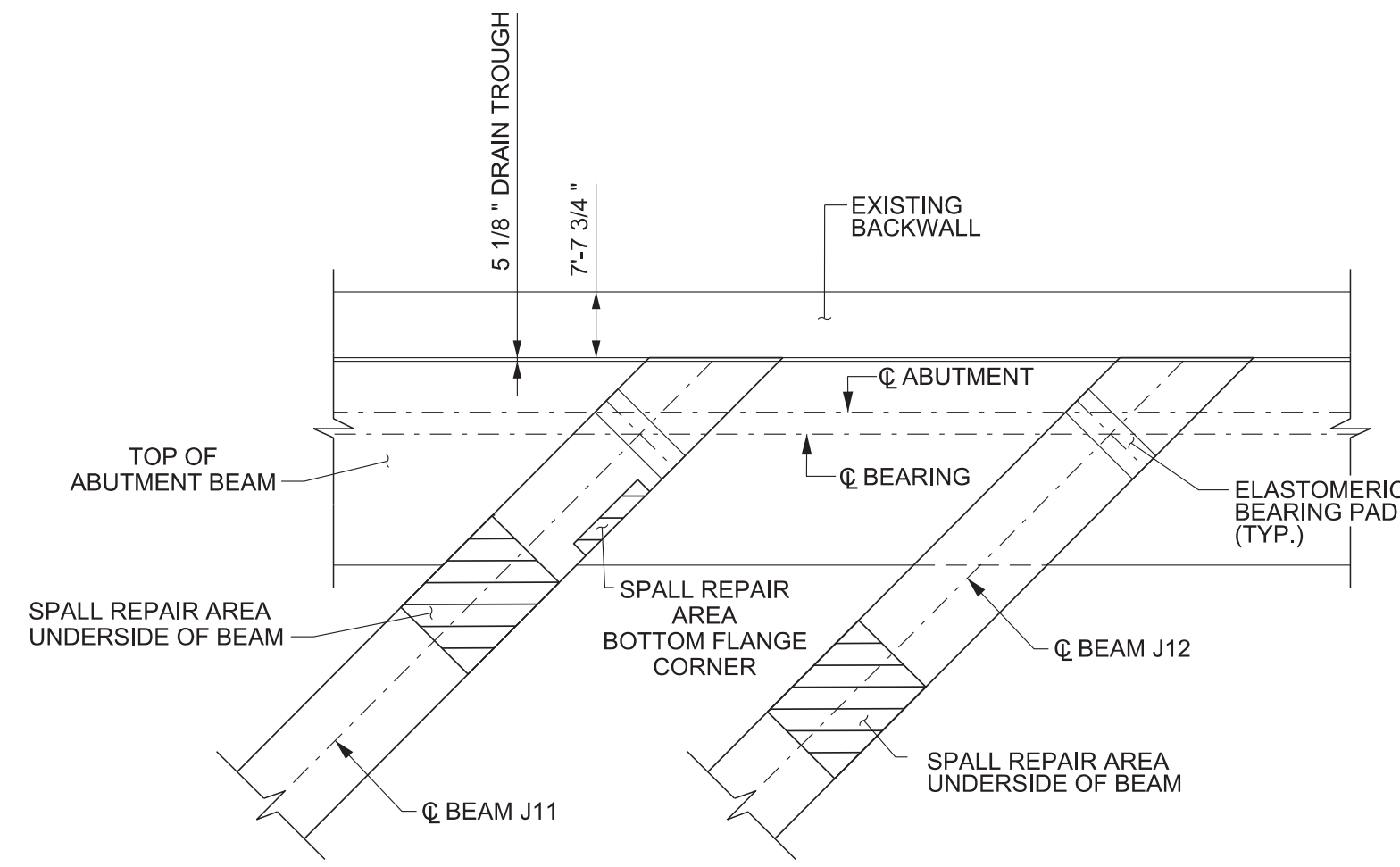
| ESTIMATED QUANTITIES | |
|----------------------|---|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.42 CONCRETE REPAIRS (C.F.) |
| BEAM 1 | 1 |



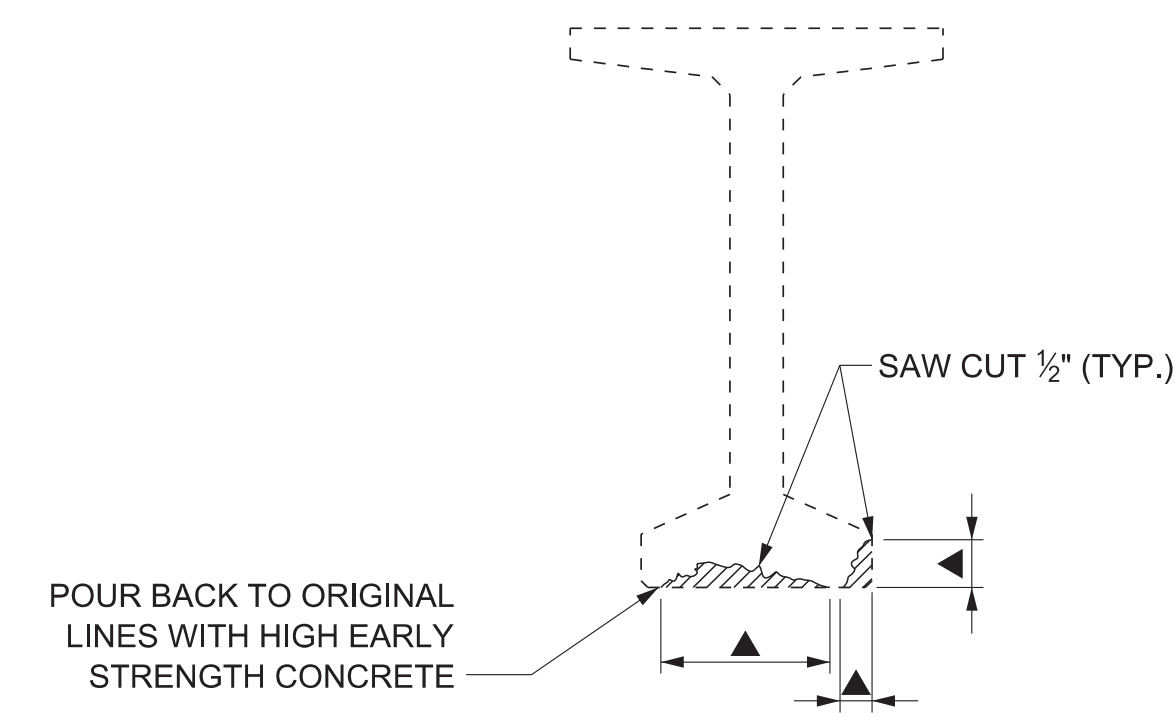
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE REPAIRS
SPAN A
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-308

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-309 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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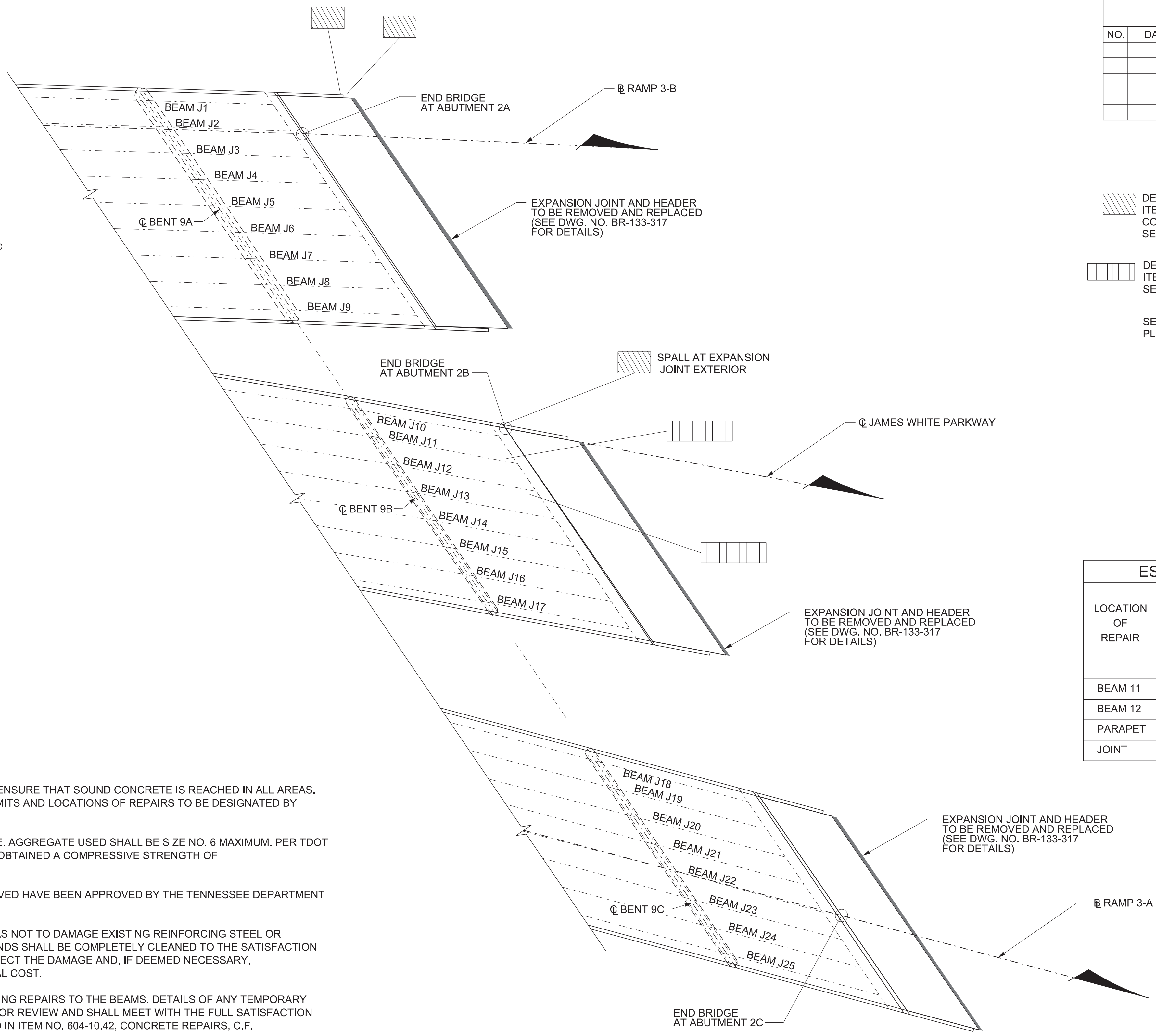
BEAM REPAIR AT ABUTMENT 2B
SPAN J, BEAM J11 AND BEAM J12 - UNDERSIDE OF BEAM PROJECTED



SPAN J - BEAM J11 AND J12 REPAIR DETAIL

NOTES:

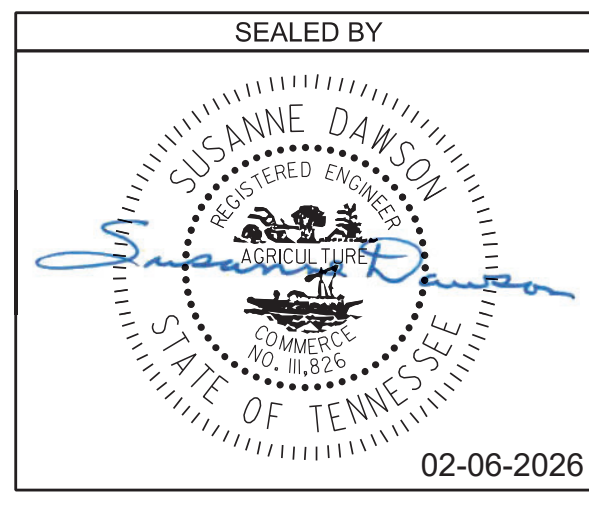
- ▲ REMOVE CONCRETE TO A DEPTH OF 3/4" BEHIND ANY EXPOSED REINFORCING STEEL TO ENSURE THAT SOUND CONCRETE IS REACHED IN ALL AREAS. SEE PLAN AND DETAILS THIS SHEET FOR APPROXIMATE LIMITS AND LOCATIONS. FINAL LIMITS AND LOCATIONS OF REPAIRS TO BE DESIGNATED BY THE ENGINEER.
- FORM AND POUR REPAIR AREA TO ORIGINAL BEAM LINES USING F_c = 5,000 PSI CONCRETE. AGGREGATE USED SHALL BE SIZE NO. 6 MAXIMUM. PER TDOT QUALIFIED PRODUCTS LIST. THE FORMS MAY BE STRIPPED ONLY AFTER CONCRETE HAS OBTAINED A COMPRESSIVE STRENGTH OF NOT LESS THAN 3,000 PSI.
- APPROVAL OF MATERIALS: NO FABRICATION SHALL BE STARTED UNTIL MATERIALS INVOLVED HAVE BEEN APPROVED BY THE TENNESSEE DEPARTMENT OF TRANSPORTATION, DIVISION OF MATERIALS AND TESTS.
- CONTRACTOR TO TAKE EXTREME CARE WHEN REMOVING DELAMINATED CONCRETE SO AS NOT TO DAMAGE EXISTING REINFORCING STEEL OR PRESTRESSING STRANDS. ALL EXISTING REINFORCING STEEL AND PRESTRESSING STRANDS SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING. IF DAMAGE OCCURS, THE ENGINEER SHALL INSPECT THE DAMAGE AND, IF DEEMED NECESSARY, THE CONTRACTOR SHALL REPLACE OR REPAIR THE DAMAGED MEMBER AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS TO THE BEAMS. DETAILS OF ANY TEMPORARY SUPPORT SYSTEM OR FALSEWORK REQUIRED SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND SHALL MEET WITH THE FULL SATISFACTION OF THE ENGINEER BEFORE THE BEGINNING OF ANY BEAM REPAIR. COST TO BE INCLUDED IN ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.
- THE COST OF REMOVING PORTION OF EXISTING CONCRETE WITHIN THE LIMITS SHOWN, SAW CUTTING, COMPLETELY CLEANING EXISTING REINFORCING STEEL, HIGH EARLY STRENGTH CONCRETE, FORMING, LABOR, AND ALL MISCELLANEOUS MATERIAL NECESSARY TO COMPLETE REPAIRS AS SHOWN SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F.



FRAMING PLAN
SPAN J

- DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS, S.F. SEE REPAIR DETAILS ON DWG. NO. BR-133-318.
- DENOTES AREA OF BEAM REPAIR TO BE PAID FOR UNDER ITEM NO. 604-10.42, CONCRETE REPAIRS, C.F. SEE REPAIR DETAILS ON THIS SHEET.
- SEE BRIDGE REFERENCE DRAWINGS FOR BEAM FRAMING PLAN AND BEAM NO. NOMENCLATURE

| ESTIMATED QUANTITIES | | |
|----------------------|---|---|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) | APPROX. REPAIR AREAS ITEM NO. 604-10.42 CONCRETE REPAIRS (C.F.) |
| BEAM 11 | - | 1 |
| BEAM 12 | - | 1 |
| PARAPET | 4 | - |
| JOINT | 1 | - |

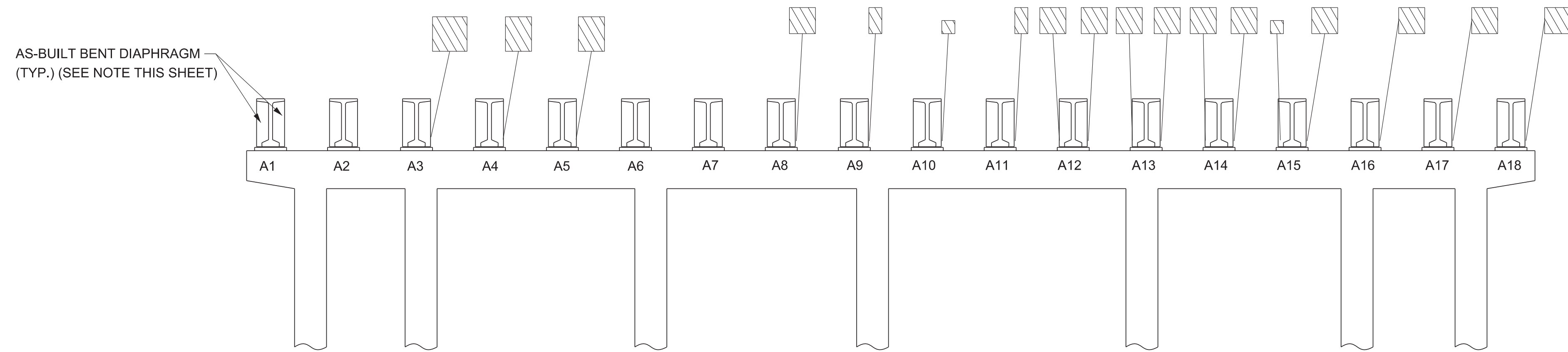


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
SUPERSTRUCTURE REPAIRS
SPAN J
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-309

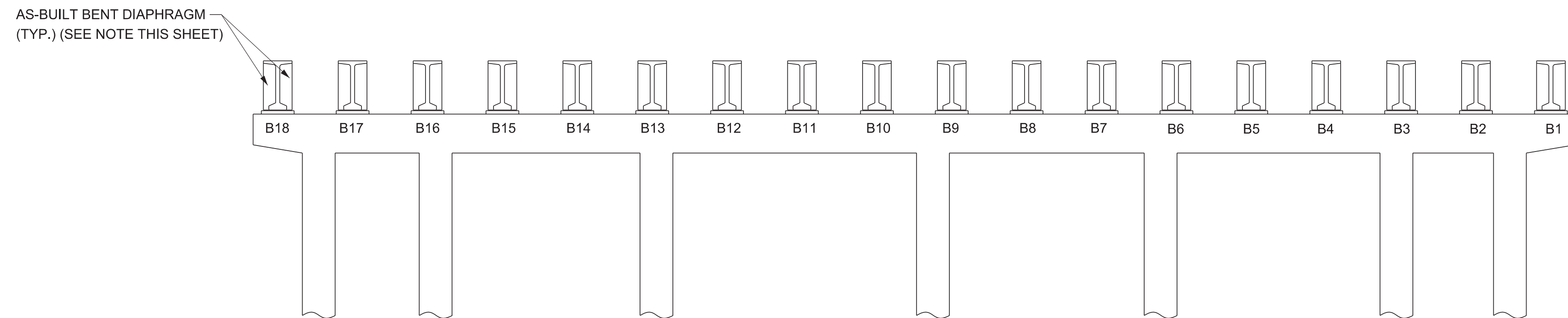
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| PIN NO.: | 134994.00 | DATE: | MARCH 2025 |
| DESIGN BY: | BRAD WARREN | DATE: | MARCH 2025 |
| DRAWN BY: | T. PELOW/D. PICKEL | DATE: | MARCH 2025 |
| SUPERVISED BY: | SUSANNE DAWSON | DATE: | MARCH 2025 |
| CHECKED BY: | FRANK BALE | DATE: | MARCH 2025 |


| PROJECT NO. | YEAR | SHEET NO. | |
|---------------|------|------------|-------------------|
| 471040-M3-012 | 2026 | BR-133-310 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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BENT 1
LOOKING AHEAD



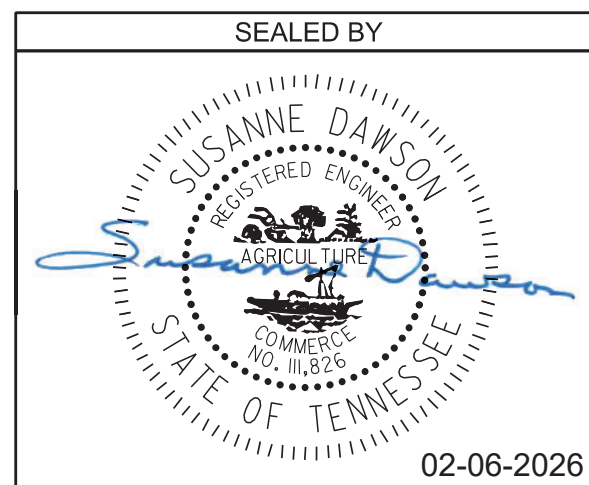
BENT 1
LOOKING BACK

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS, S.F. SEE REPAIR DETAILS ON DWG. NO. BR-133-318.

A1 - A18 DENOTES BEAM NO. IN SPAN A
B1 - B18 DENOTES BEAM NO. IN SPAN B

NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.

| ESTIMATED QUANTITIES | |
|----------------------|--|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 1 | 68 |



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BENT 1 - DIAPHRAGM REPAIRS

BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026

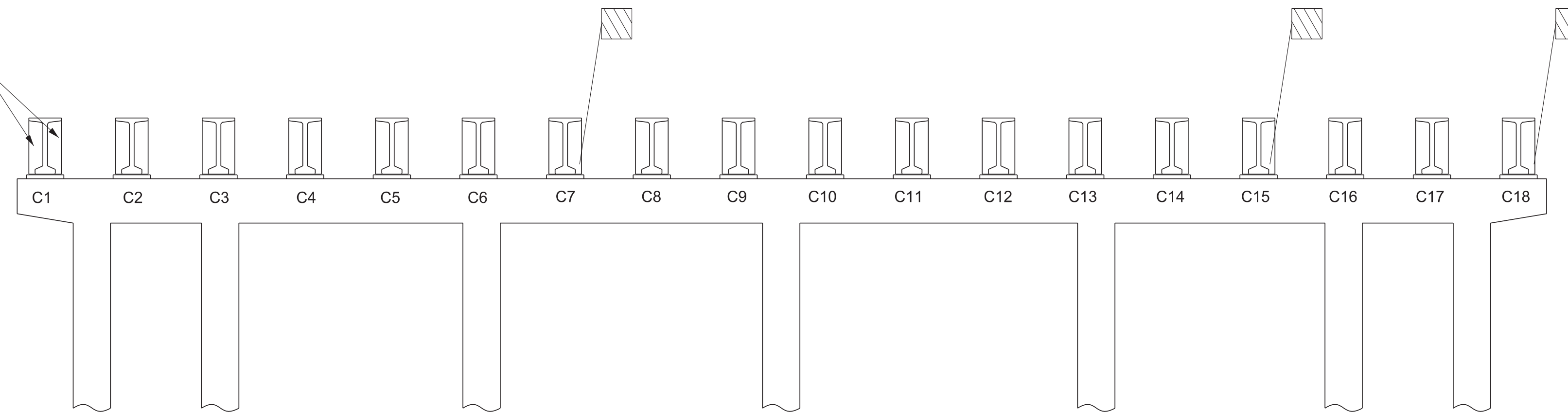
BR-133-310

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

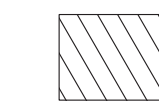
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| PROJECT NO. | YEAR | SHEET NO. | |
|---------------|------|------------|-------------------|
| 471040-M3-012 | 2026 | BR-133-311 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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AS-BUILT BENT DIAPHRAGM
(TYP.)(SEE NOTE THIS SHEET)



BENT 3
LOOKING AHEAD



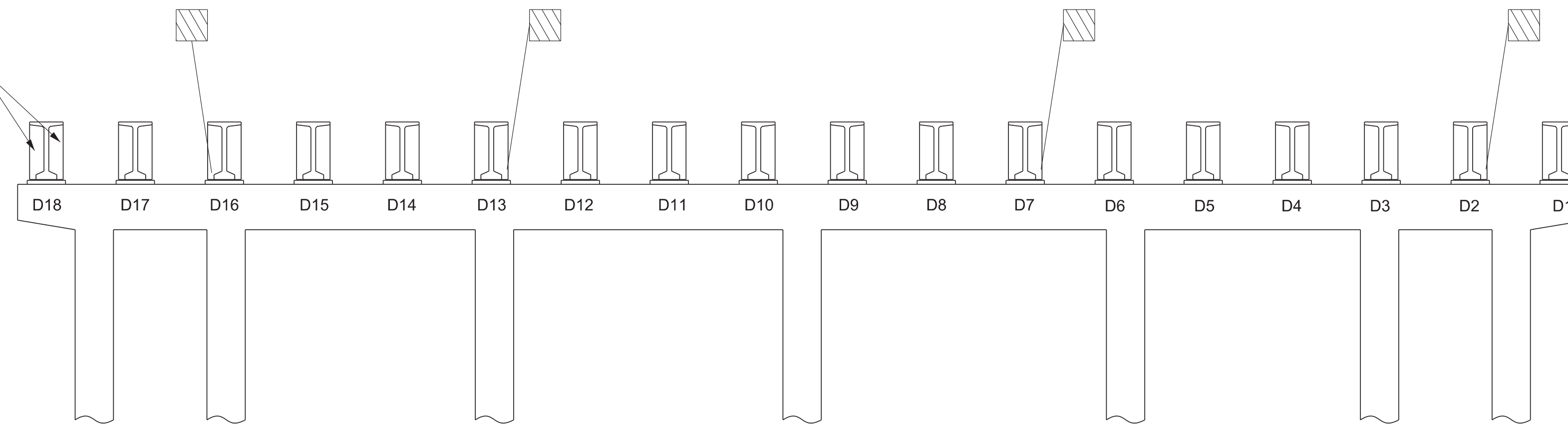
DENOTES AREA TO BE REPAIRED UNDER
ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS, S.F.
SEE REPAIR DETAILS ON DWG. NO. BR-133-318.

C1 - C18 DENOTES BEAM NO. IN SPAN C

D1 - D18 DENOTES BEAM NO. IN SPAN D

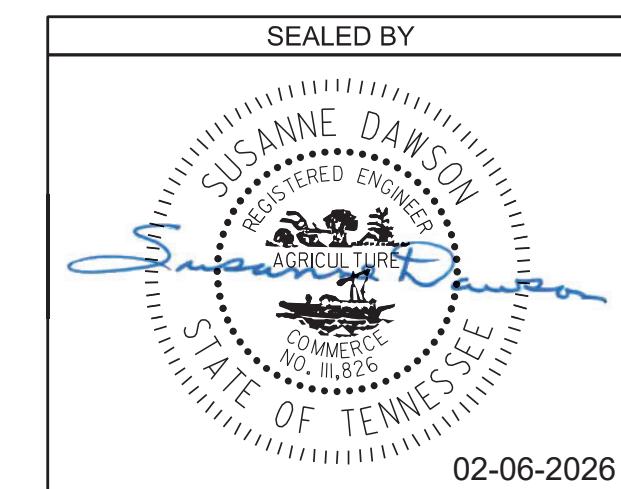
NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE
TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN
DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.

AS-BUILT BENT DIAPHRAGM
(TYP.)(SEE NOTE THIS SHEET)



BENT 3
LOOKING BACK

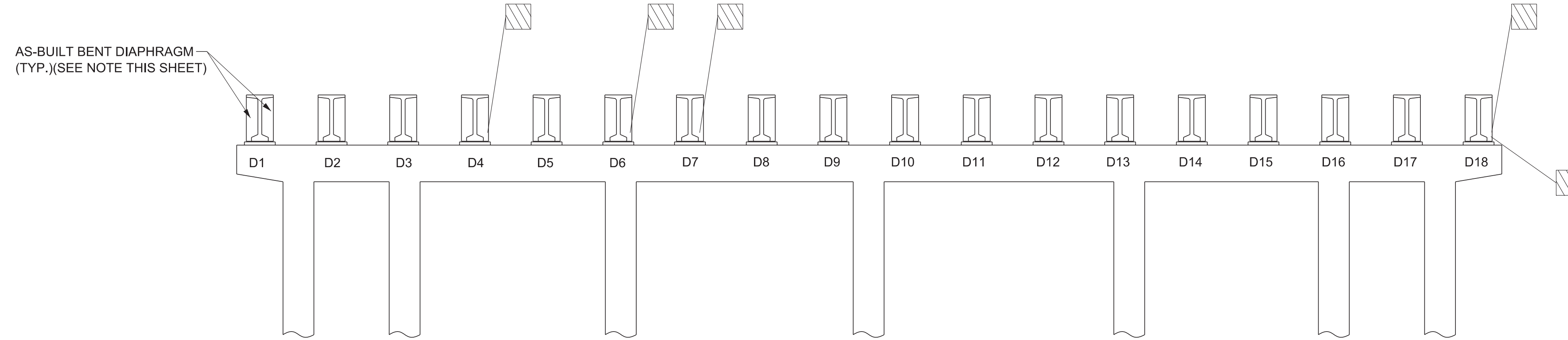
| ESTIMATED QUANTITIES | |
|----------------------|--|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 3 | 9 |




STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT 3 - DIAPHRAGM REPAIRS
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-311

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

| PROJECT NO. | YEAR | SHEET NO. | |
|---------------|------|------------|-------------------|
| 471040-M3-012 | 2026 | BR-133-312 | |
| REVISIONS | | | |
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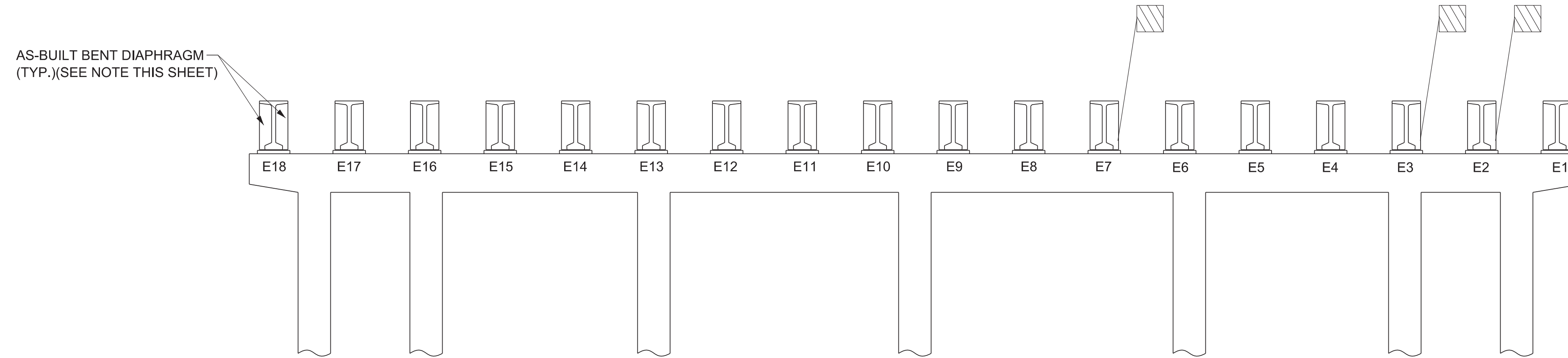
BENT 4
LOOKING AHEAD

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS, S.F. SEE REPAIR DETAILS ON DWG. NO. BR-133-318.

D1 - D18 DENOTES BEAM NO. IN SPAN D

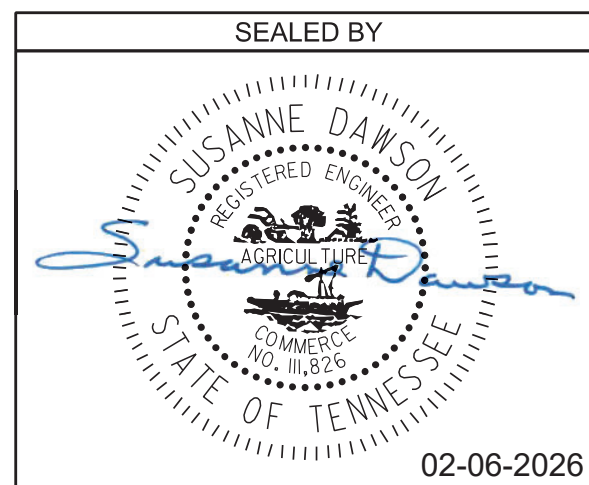
E1 - E18 DENOTES BEAM NO. IN SPAN E

NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.



BENT 4
LOOKING BACK

| ESTIMATED QUANTITIES | |
|----------------------|--|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 4 | 12 |

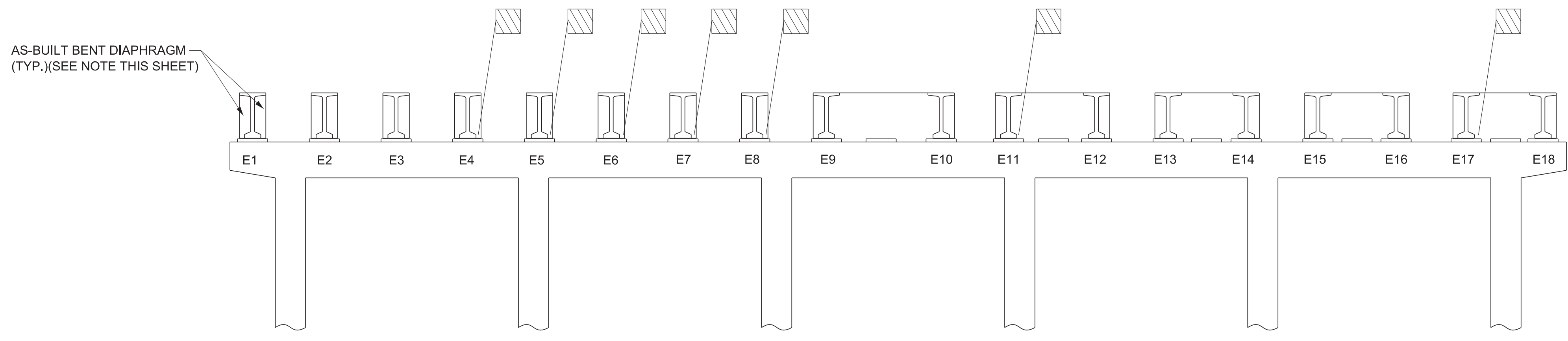



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT 4 - DIAPHRAGM REPAIRS
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-312

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

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| PROJECT NO. | YEAR | SHEET NO. | |
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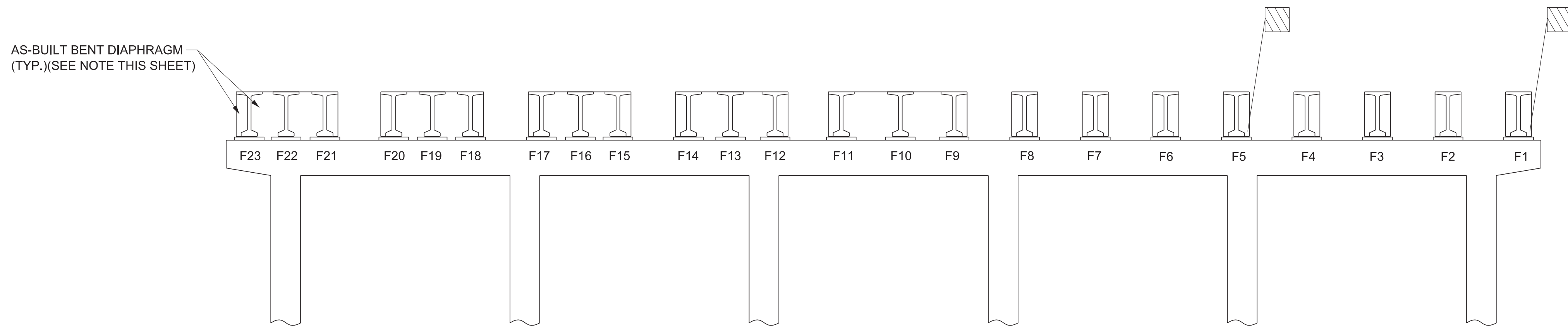
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E1 - E18 DENOTES BEAM NO. IN SPAN E

F1 - F23 DENOTES BEAM NO. IN SPAN F

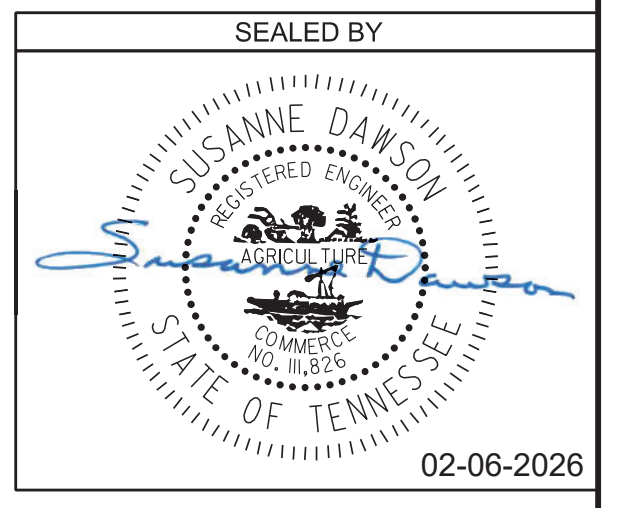
NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.

BENT 5
LOOKING AHEAD



| ESTIMATED QUANTITIES | |
|----------------------|--|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 5 | 10 |

BENT 5
LOOKING BACK



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BENT 5 - DIAPHRAGM REPAIRS

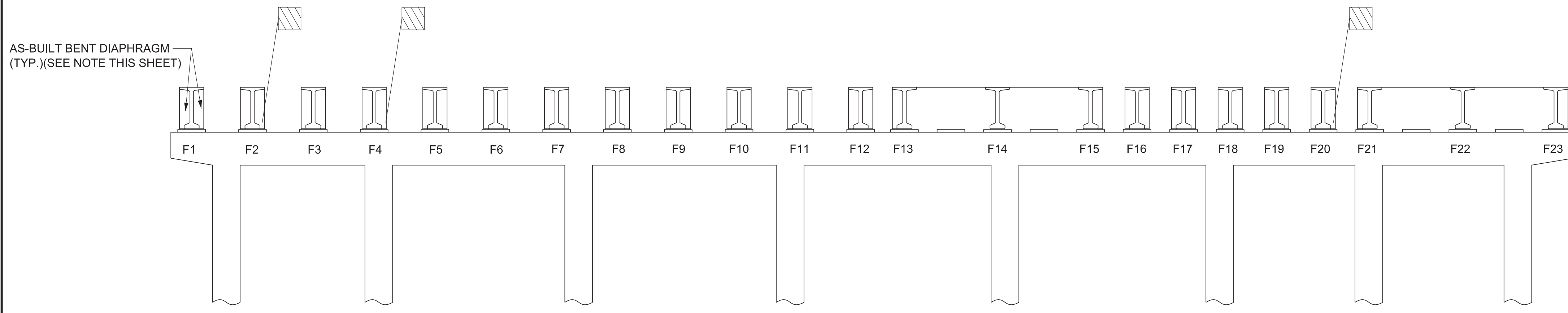
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026

BR-133-313

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

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| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-314 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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BENT 6
LOOKING AHEAD

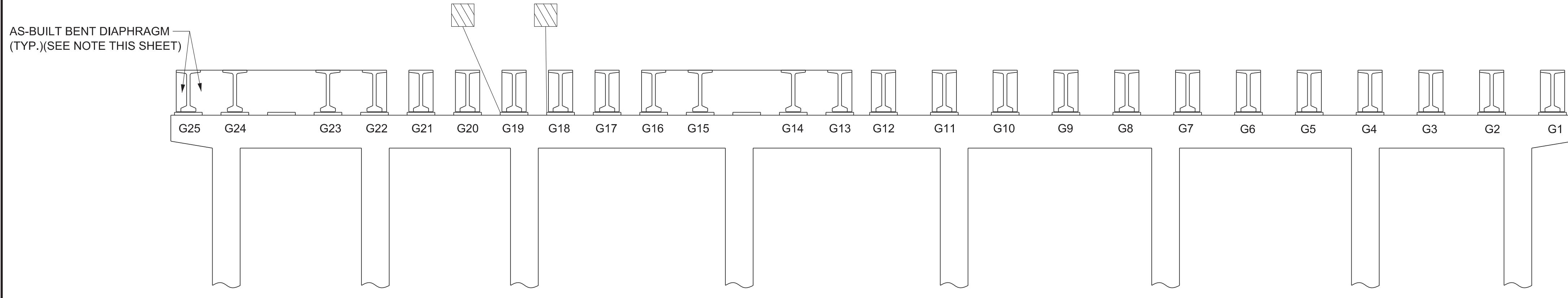
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F1 - F23 DENOTES BEAM NO. IN SPAN F

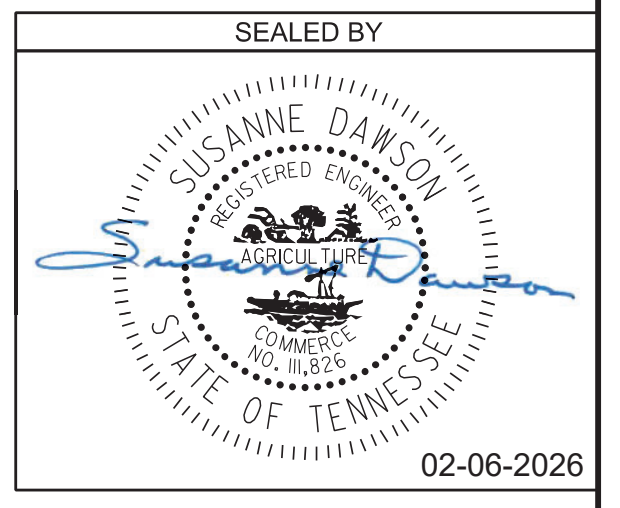
G1 - G25 DENOTES BEAM NO. IN SPAN G

NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.

| ESTIMATED QUANTITIES | |
|----------------------|--|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 6 | 5 |



BENT 6
LOOKING BACK



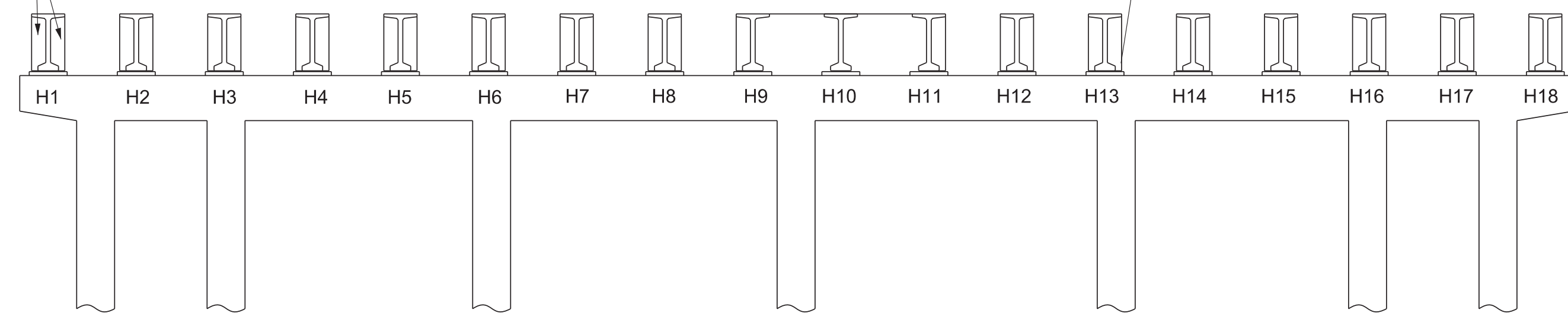
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT 6 - DIAPHRAGM REPAIRS
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-314

PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

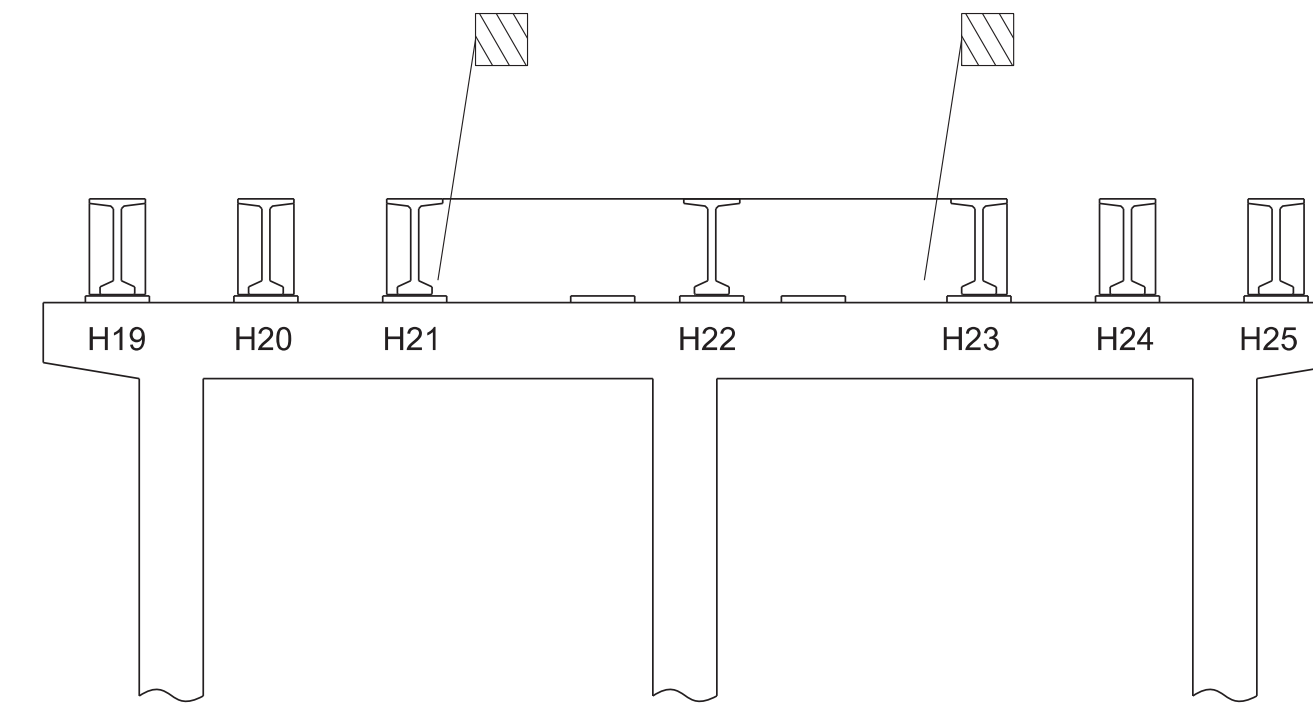
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| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-315 | |
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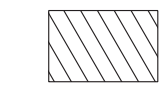
AS-BUILT BENT DIAPHRAGM (TYP.)
(SEE NOTE THIS SHEET)



BENT 8A
LOOKING AHEAD



BENT 8B
LOOKING AHEAD

 DENOTES AREA TO BE REPAIRED UNDER
ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS, S.F.
SEE REPAIR DETAILS ON DWG. NO. BR-133-318.

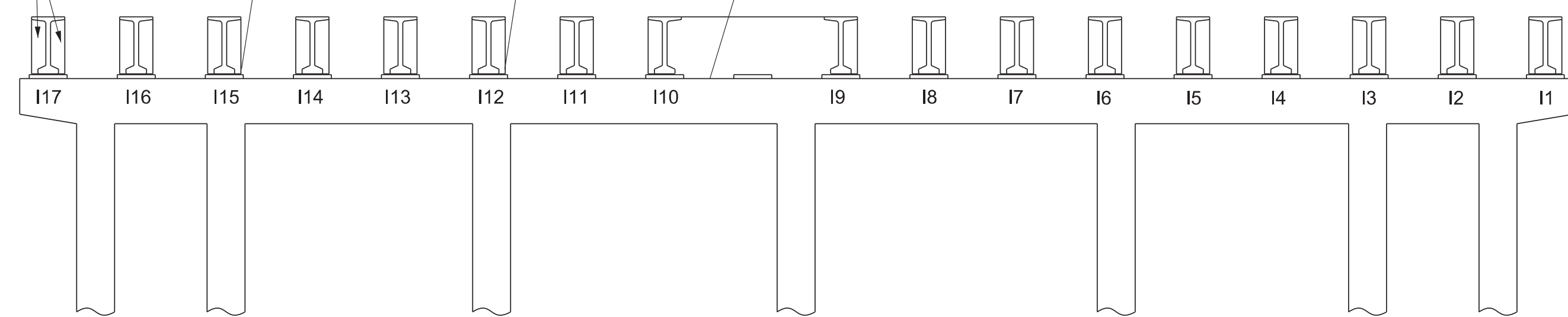
H1 - H25 DENOTES BEAM NO. IN SPAN H

I1 - I17 DENOTES BEAM NO. IN SPAN I

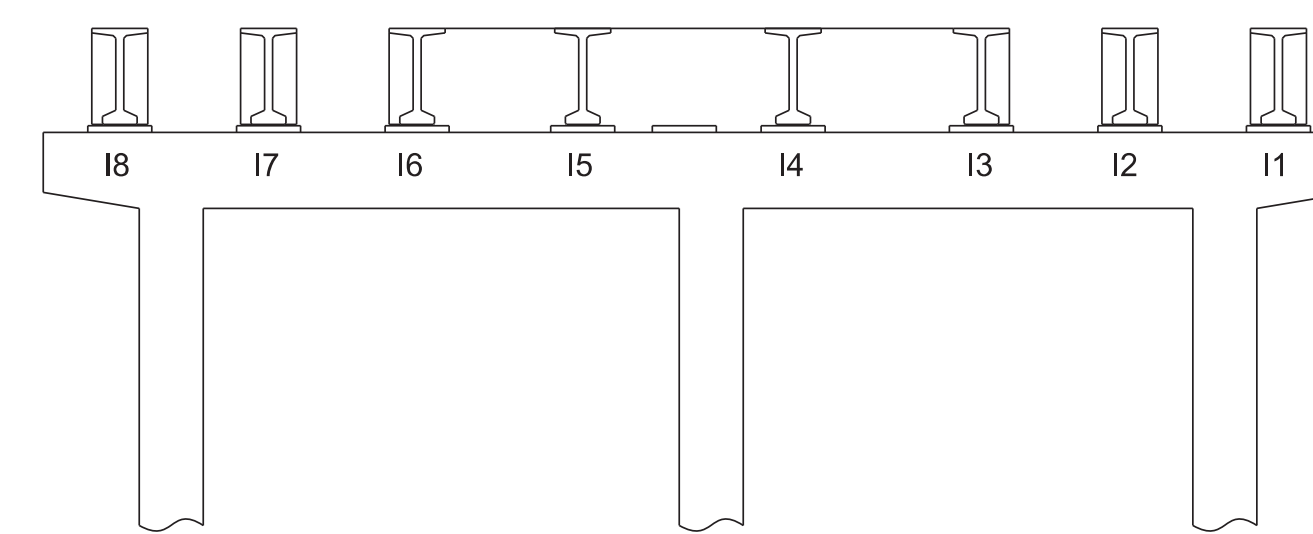
NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE
TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN
DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.

| ESTIMATED QUANTITIES | |
|----------------------|--|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 8A | 13 |
| BENT 8B | 4 |

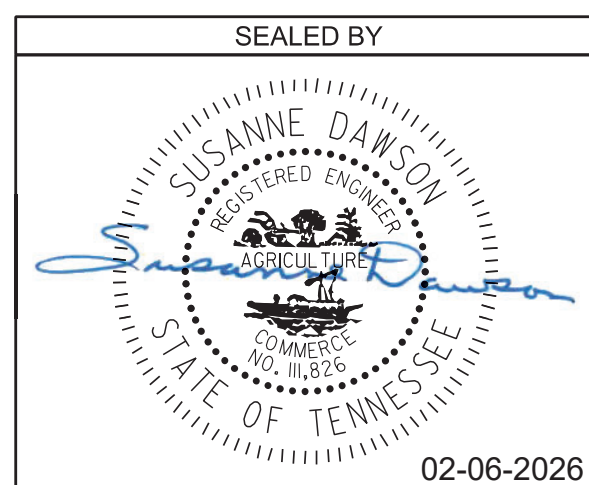
AS-BUILT BENT DIAPHRAGM (TYP.)
(SEE NOTE THIS SHEET)



BENT 8A
LOOKING BACK




BENT 8B
LOOKING BACK



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BENT 8A AND 8B -
DIAPHRAGM REPAIRS
BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY
2026 BR-133-315

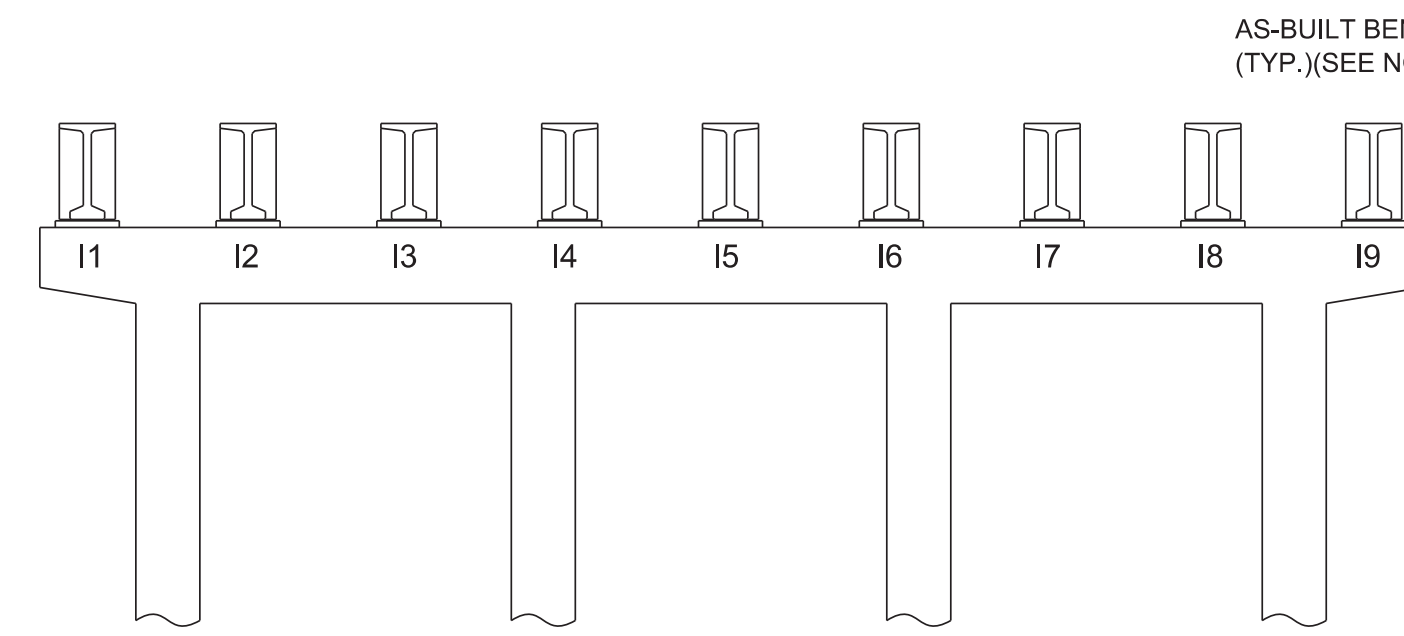
PIN NO.: 134994.00
DESIGN BY: BRAD WARREN DATE: MARCH 2025
DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
CHECKED BY: FRANK BALE DATE: MARCH 2025

| PROJECT NO. | YEAR | SHEET NO. | |
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| 471040-M3-012 | 2026 | BR-133-316 | |
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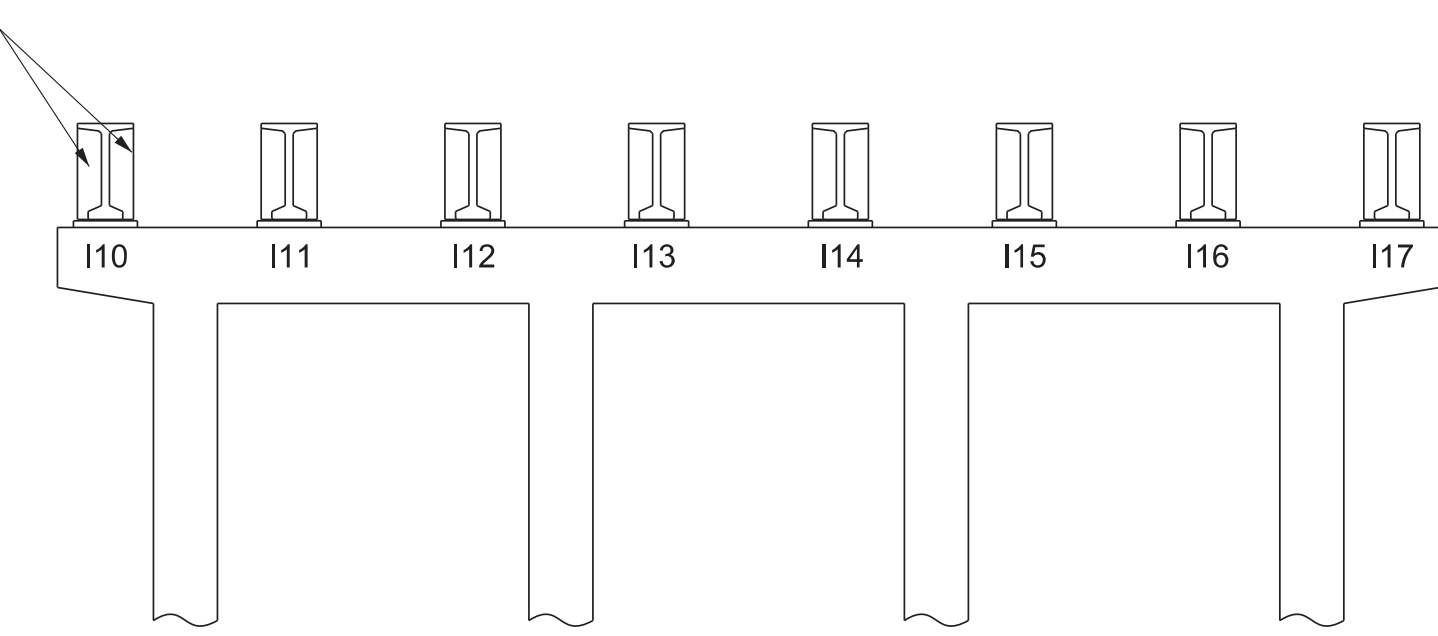
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I1 - I25 DENOTES BEAM NO. IN SPAN I
 J1 - J25 DENOTES BEAM NO. IN SPAN J

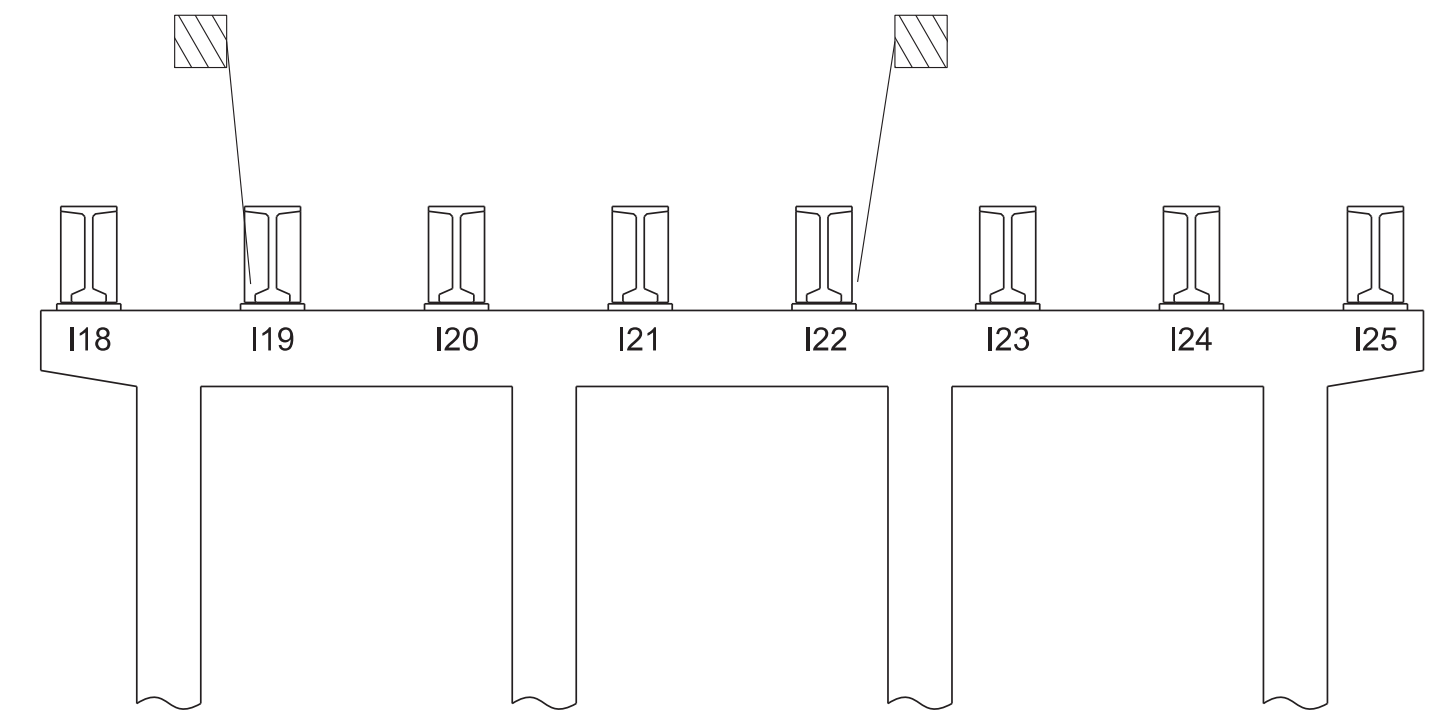
NOTE: FIELD CHANGES DURING CONSTRUCTION WERE MADE TO THE BENT DIAPHRAGMS. AS-BUILT BENT DIAPHRAGMS AS SHOWN DO NOT MATCH THE BRIDGE REFERENCE DRAWINGS.



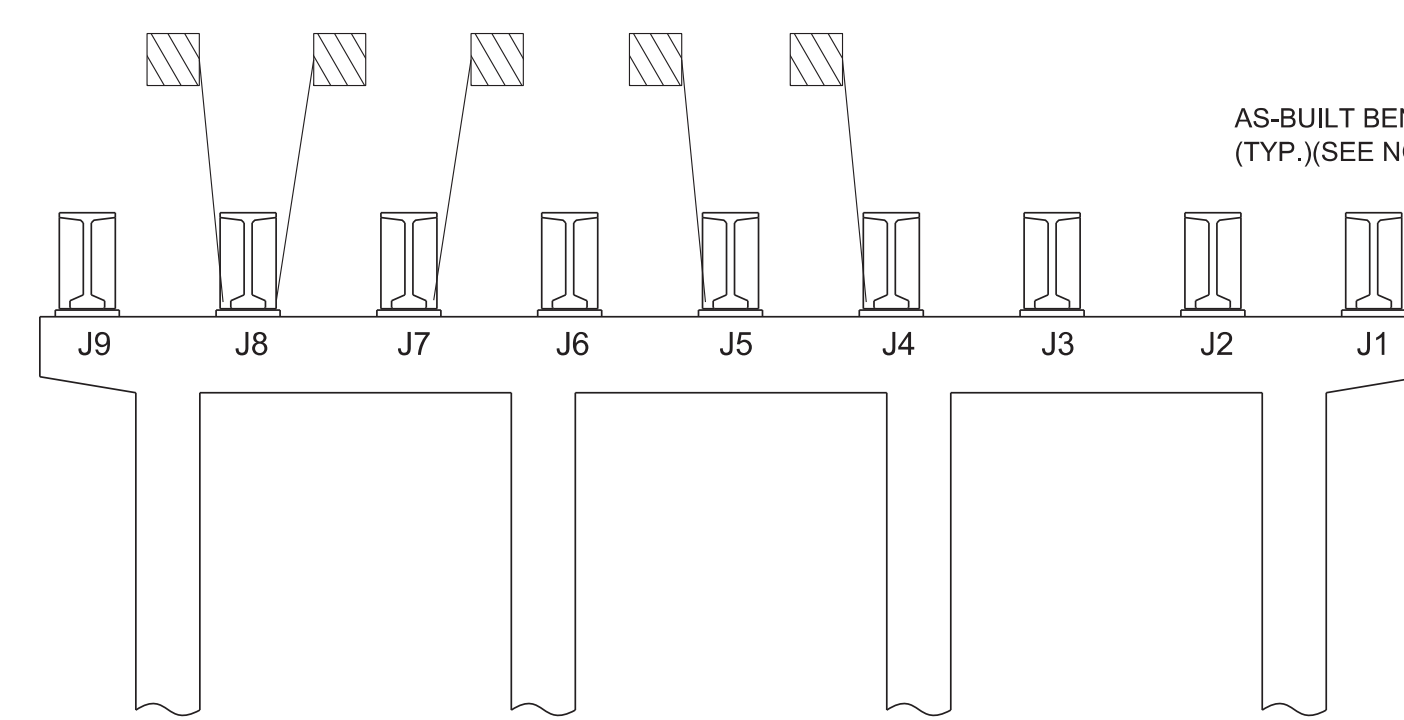
BENT 9A
LOOKING AHEAD



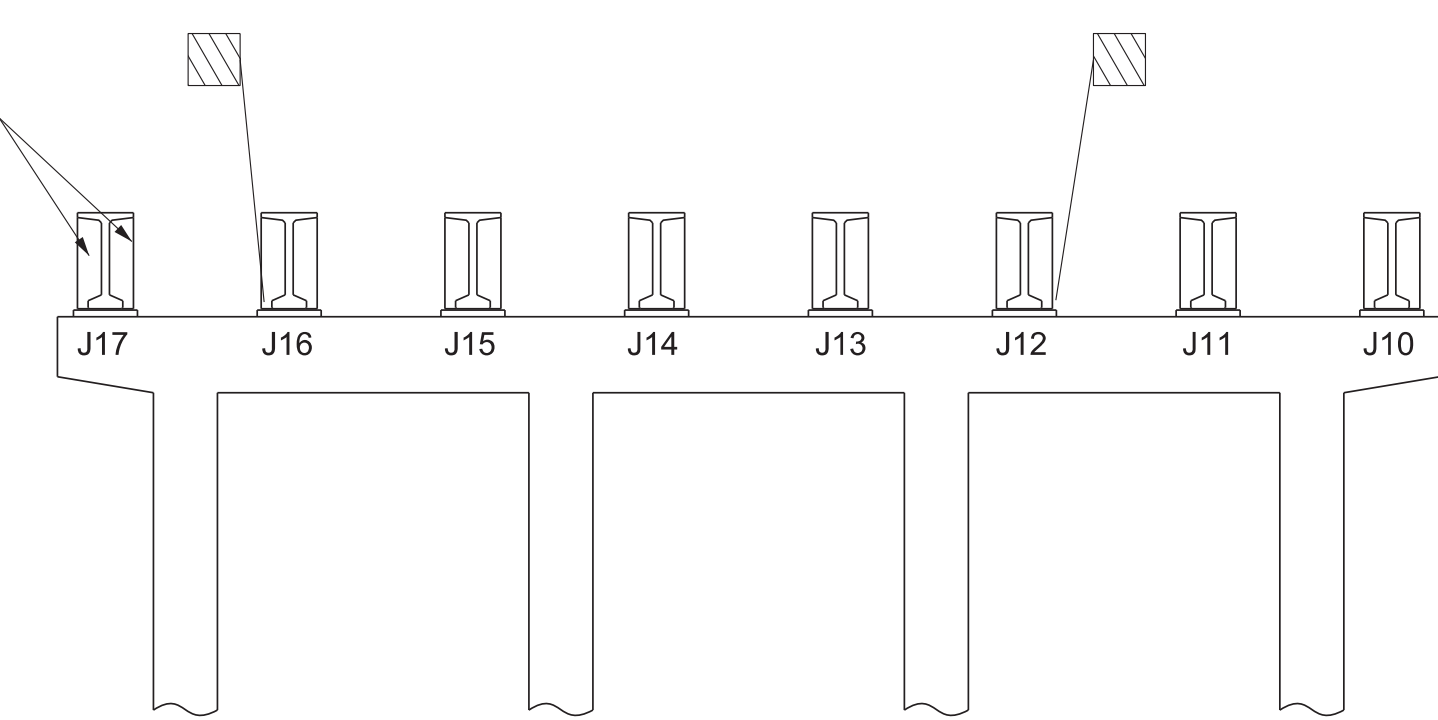
BENT 9B
LOOKING AHEAD



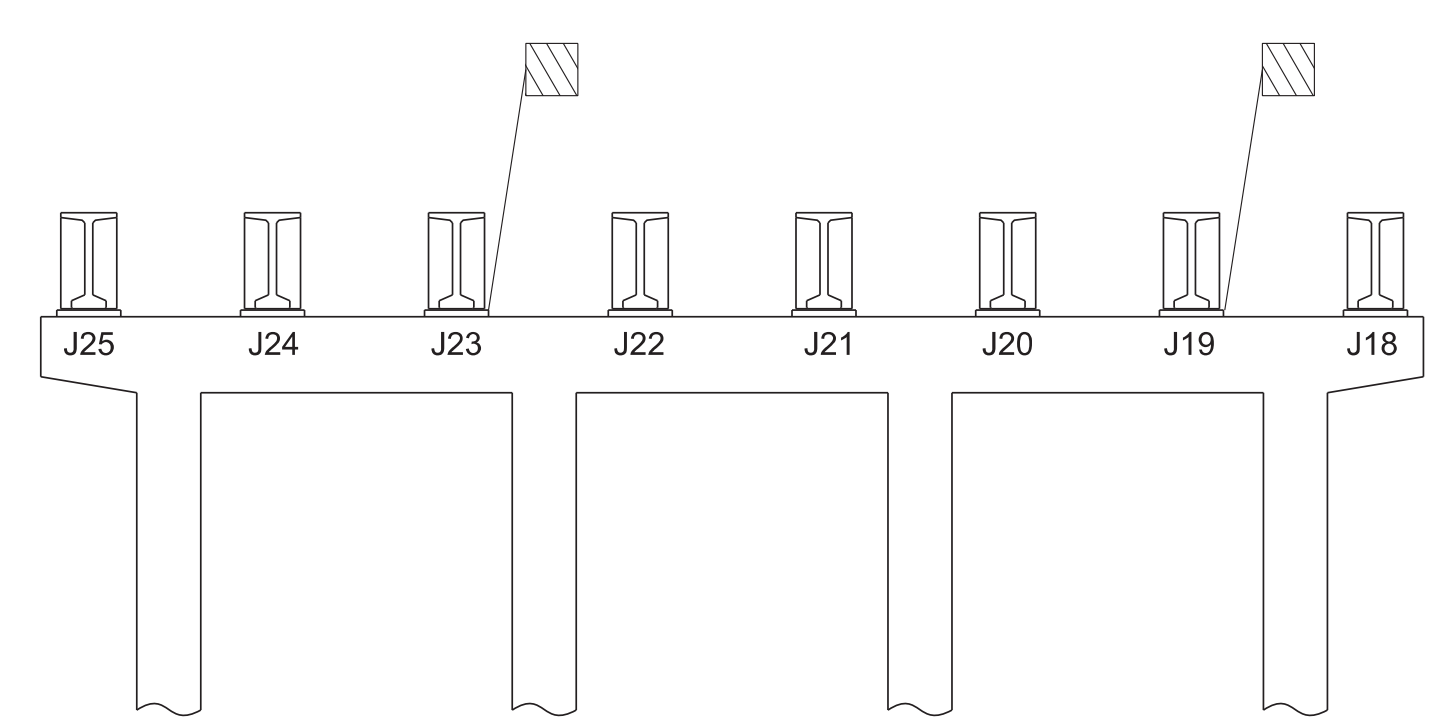
BENT 9C
LOOKING AHEAD



BENT 9A
LOOKING BACK

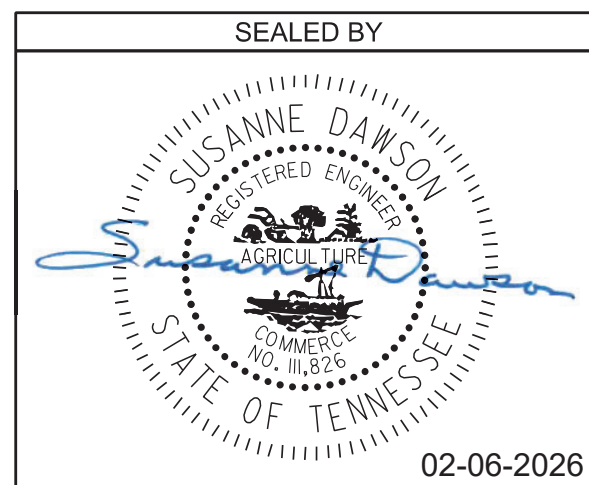


BENT 9B
LOOKING BACK



BENT 9C
LOOKING BACK

| ESTIMATED QUANTITIES | |
|----------------------|---|
| LOCATION OF REPAIR | APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54 CONCRETE/CONCRETE REPAIRS (S.F.) |
| BENT 9A | 6 |
| BENT 9B | 2 |
| BENT 9C | 8 |

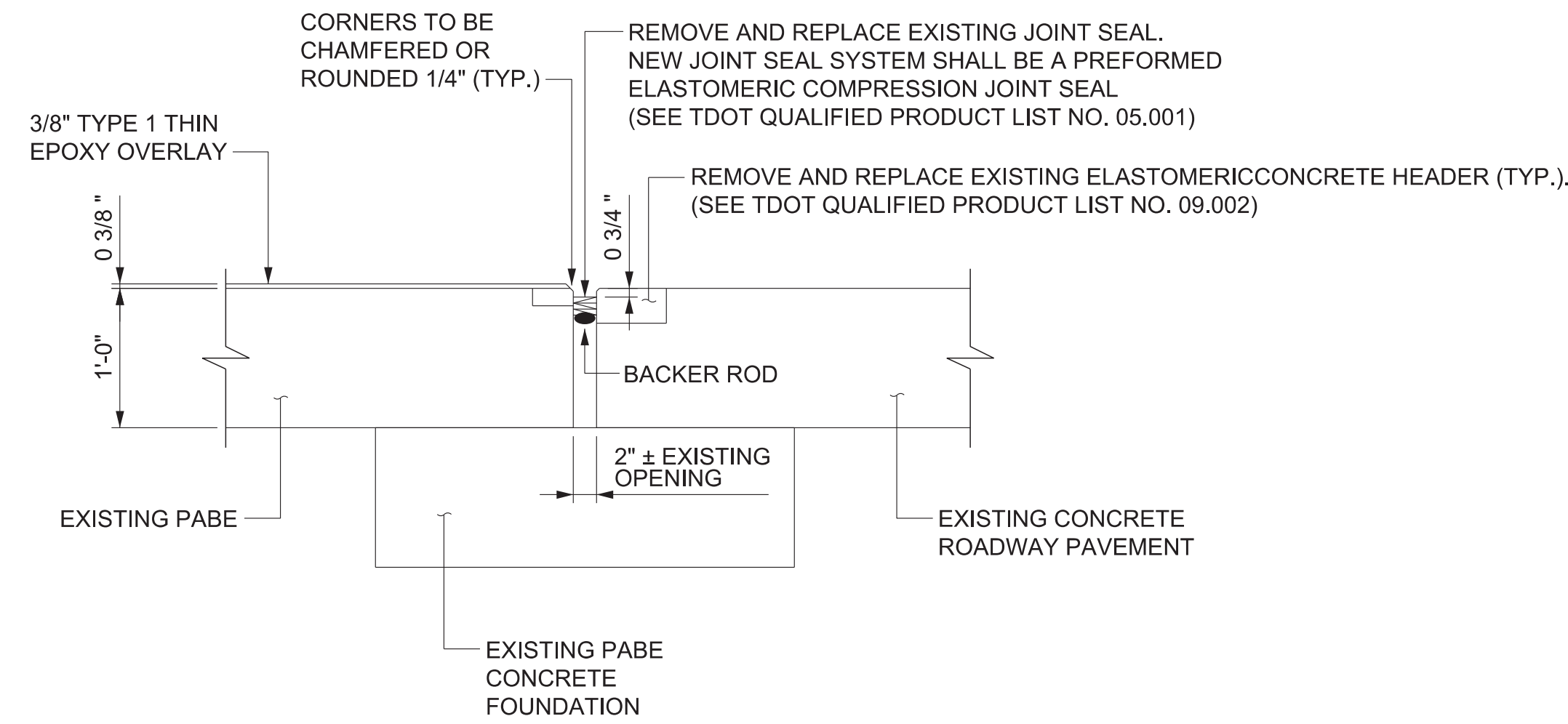


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
**BENT 9A, 9B, AND 9C -
 DIAPHRAGM REPAIRS**
 BRIDGE NO. 47-10040-19.30
 FEDERAL BRIDGE ID NO. 47100400205
 JAMES WHITE PARKWAY OVER NORFOLK
 SOUTHERN RAILWAY, WILLOW AVENUE,
 AND JACKSON AVENUE
 KNOX COUNTY
 2026 BR-133-316

1/6/2026 12:33:15 PM c:\pw_pl1\peplow\td4278401\BR-133-316_WO13_Bent_9.dgn

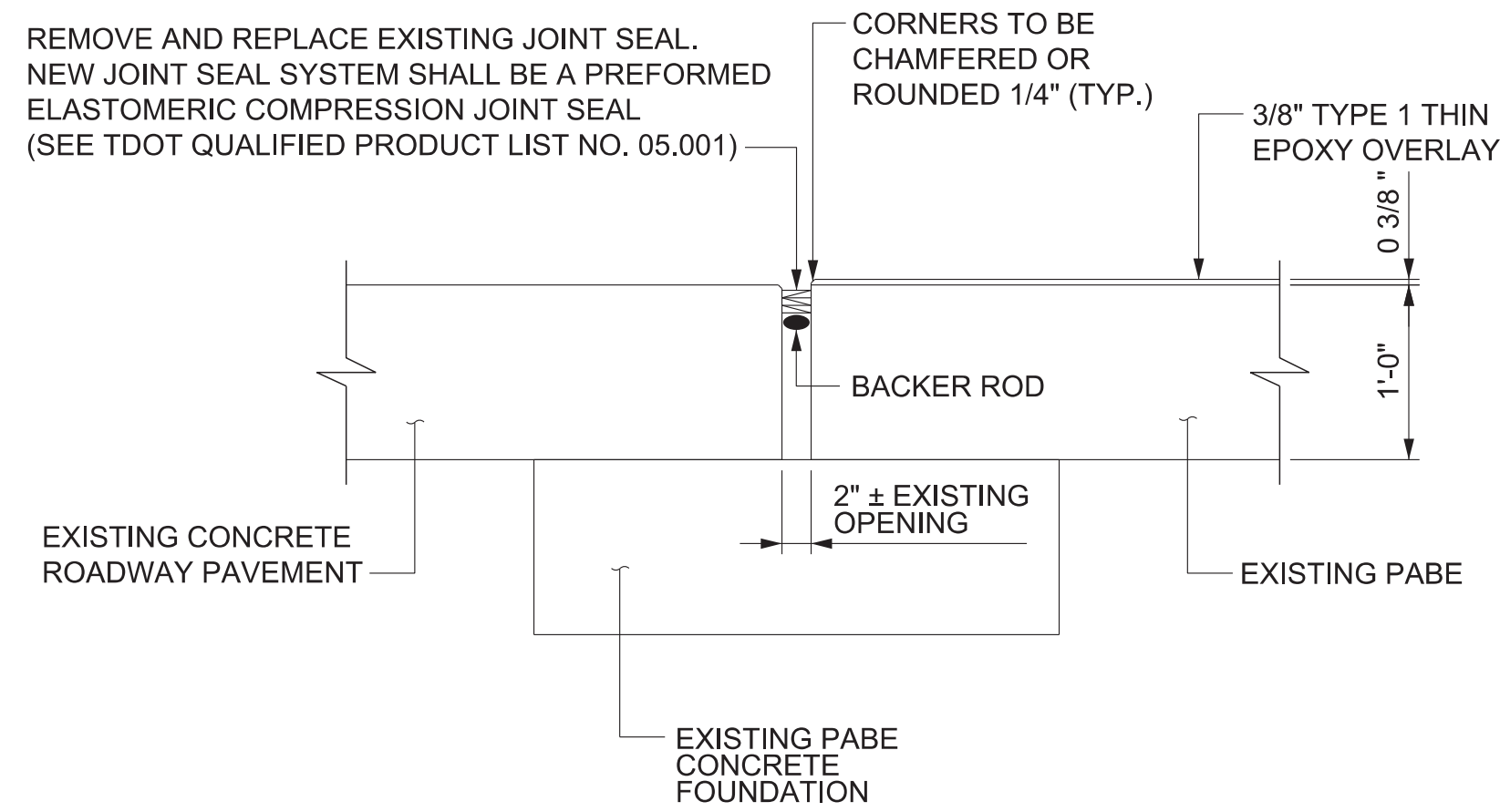
PIN NO.: 134994.00
 DESIGN BY: BRAD WARREN DATE: MARCH 2025
 DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
 SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
 CHECKED BY: FRANK BALE DATE: MARCH 2025

| | | | |
|---------------|------|------------|-------------------|
| PROJECT NO. | YEAR | SHEET NO. | |
| 471040-M3-012 | 2026 | BR-133-317 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| | | | |
| | | | |
| | | | |
| | | | |



PABE EXPANSION JOINT REPAIR AT END BRIDGE

END PABE AT ABUTMENT 2A, 2B AND 2C SHOWN



PABE EXPANSION JOINT REPAIR AT BEGIN BRIDGE

BEGIN PABE AT ABUTMENT 1 SHOWN

EXPANSION JOINT REPAIR NOTES:

THE EXISTING JOINT OPENING SHALL BE FILLED WITH A BACKER ROD OF SUITABLE DIAMETER, THE ROD SHALL BE PLACED AT A DEPTH TO INSURE THE CORRECT WIDTH/DEPTH RATIO OF THE NEW JOINT SEALANT MATERIAL. BACKER ROD SHALL BE AS PER JOINT MANUFACTURE'S RECOMMENDATION.

FULL DEPTH OF ALL EXISTING JOINT SHALL BE RESEALED WITH NEW JOINT SEALER. THE NEW JOINT SEALER SHALL BE A COLD POUR TWO PART SILICONE SEALER FROM QPL 05.001.

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 09.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT.

THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 07.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM QPL 09.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS.

PRODUCTS FROM QPL 07.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED.

THE TOP OF THE QPL 07.001 JOINT FILLER SHALL BE A MINIMUM OF 3 / 4 OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REQUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK.

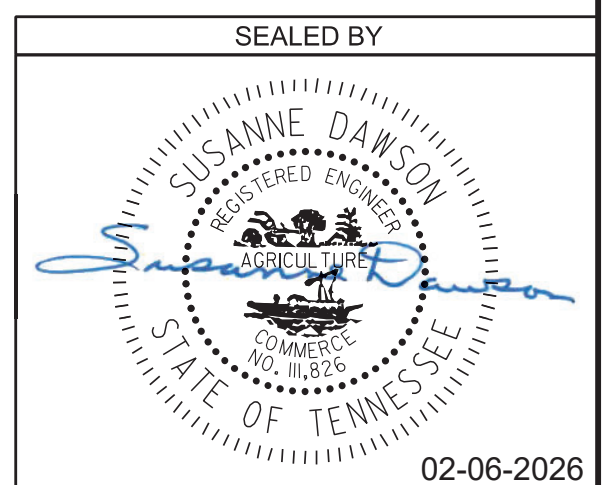
THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL WITHIN THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

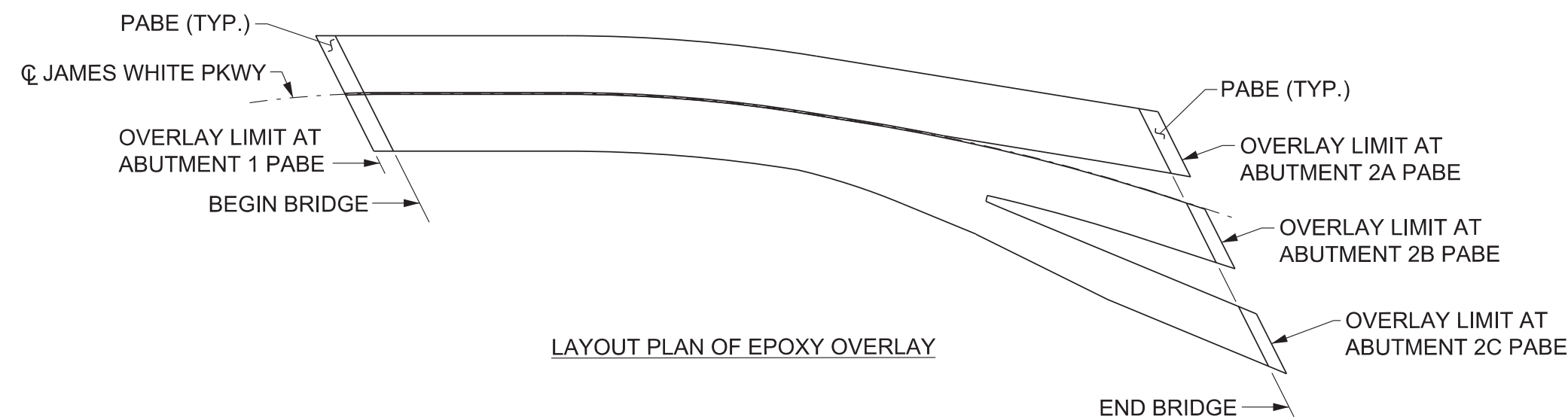
THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT IS TO BE INCLUDED IN ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

| ESTIMATED QUANTITIES | |
|--------------------------------|--|
| LOCATION OF REPAIR | ITEM NO. 604-10.44 EXPANSION JOINT REPAIR (L.F.) |
| BEGIN BRIDGE PABE (ABUTMENT 1) | 156 |
| END BRIDGE PABE (ABUTMENT 2A) | 82 |
| END BRIDGE PABE (ABUTMENT 2B) | 82 |
| END BRIDGE PABE (ABUTMENT 2C) | 82 |

PIN NO.: 134994.00
 DESIGN BY: BRAD WARREN DATE: MARCH 2025
 DRAWN BY: T. PELOW/D. PICKEL DATE: MARCH 2025
 SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2025
 CHECKED BY: FRANK BALE DATE: MARCH 2025



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 JOINT REPAIR DETAILS
 BRIDGE NO. 47-10040-19.30
 FEDERAL BRIDGE ID NO. 47100400205
 JAMES WHITE PARKWAY OVER NORFOLK
 SOUTHERN RAILWAY, WILLOW AVENUE,
 AND JACKSON AVENUE
 KNOX COUNTY
 2026 BR-133-317



SPECIAL NOTES FOR TYPE 1 THIN LIFT EPOXY OVERLAY

TYPE 1 THIN EPOXY OVERLAY SYSTEM USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED. THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

| SIEVE SIZE | % PASSING |
|------------|-----------|
| NO. 6 | 95-100 |
| NO. 10 | 10-35 |
| NO. 20 | 0-3 |

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

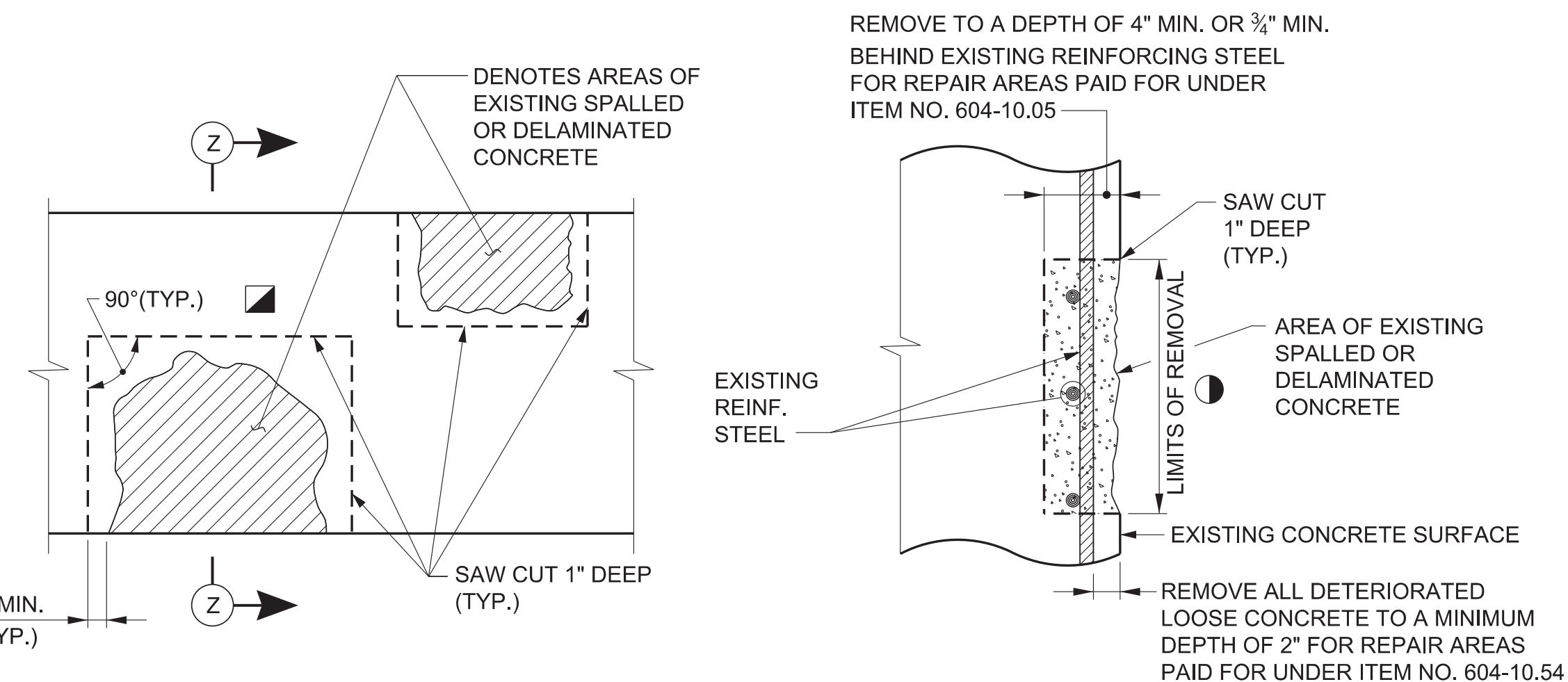
SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

| | | | |
|----------------|--------------------|-------|------------|
| PIN NO.: | 134994.00 | DATE: | MARCH 2025 |
| DESIGN BY: | BRAD WARREN | DATE: | MARCH 2025 |
| DRAWN BY: | T. PELOW/D. PICKEL | DATE: | MARCH 2025 |
| SUPERVISED BY: | SUSANNE DAWSON | DATE: | MARCH 2025 |
| CHECKED BY: | FRANK BALE | DATE: | MARCH 2025 |



SECTION Z-Z

DETAILS SHOWING AREAS OF EXISTING SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED

- - DENOTES LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE ENGINEER
- - DENOTES SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS

NOTES

EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED OR DELAMINATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE AND INCORPORATED INTO THE NEW CONSTRUCTION. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALLED OR DELAMINATED AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE HIGH EARLY STRENGTH CONCRETE PER TDOT QUALIFIED PRODUCTS LIST. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL PER TDOT QUALIFIED PRODUCTS LIST QPL 13.009.

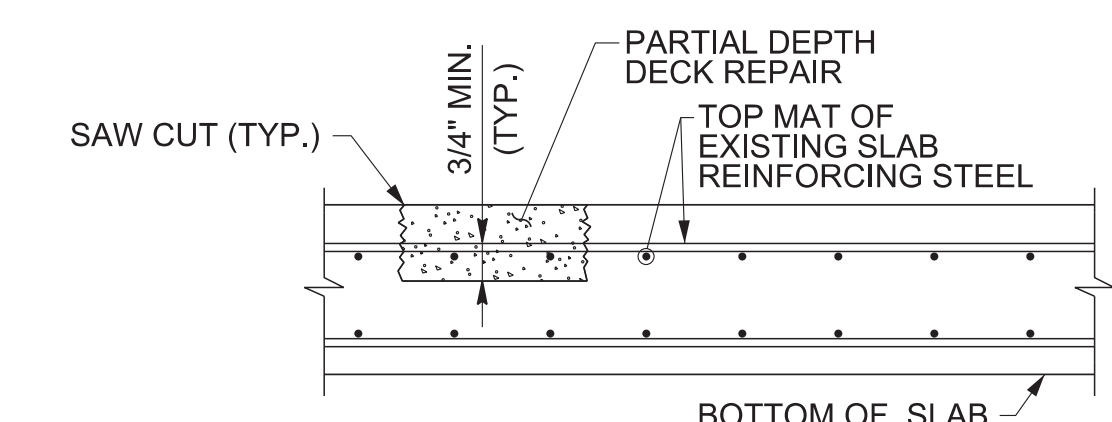
PNEUMATICALLY PLACED CONCRETE IS NOT ALLOWED.

THE ENGINEER SHALL DESIGNATE ALL SPALLED OR DELAMINATED CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND 604-10.54 MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER HAND DRIVEN TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

1. PNEUMATIC HAMMERS HEAVIER THAN THE 35 LB. CLASS SHALL NOT BE USED.
2. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL.

COST OF SAW CUTTING, REMOVING SPALLED OR DELAMINATED CONCRETE, CLEANING, PATCHING MATERIAL, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN TO BE INCLUDED IN ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F. OR ITEM NO. 604-10.05, CONCRETE, S.F.



DETAIL SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

CONCRETE FOR DECK REPAIR SHALL BE HIGH EARLY STRENGTH CONCRETE $f_c = 4,000$ p.s.i. @ 28 DAY STRENGTH, TRAFFIC WILL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A COMPRESSIVE STRENGTH OF 3,000 p.s.i. MINIMUM AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF EIGHTEEN (18) HOURS.

REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE.

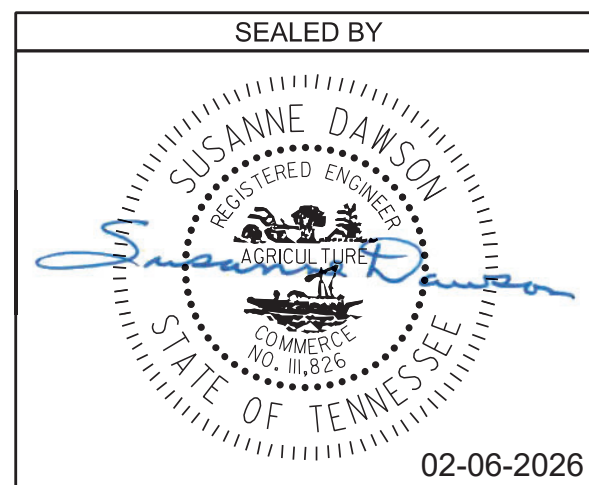
DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.50, BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB). POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

- 1) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED.
- 2) CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.
- 3) TRAFFIC CONTROL SHALL BE PROVIDED FOR TRAFFIC BELOW BRIDGE DURING PARTIAL DEPTH DECK REPAIR.

ITEM NO. 604-10.50 MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

EXTREME CARE SHALL BE TAKEN WHEN REMOVING DETERIORATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.

| PROJECT NO. | YEAR | SHEET NO. | |
|---------------|------|------------|-------------------|
| 471040-M3-012 | 2026 | BR-133-318 | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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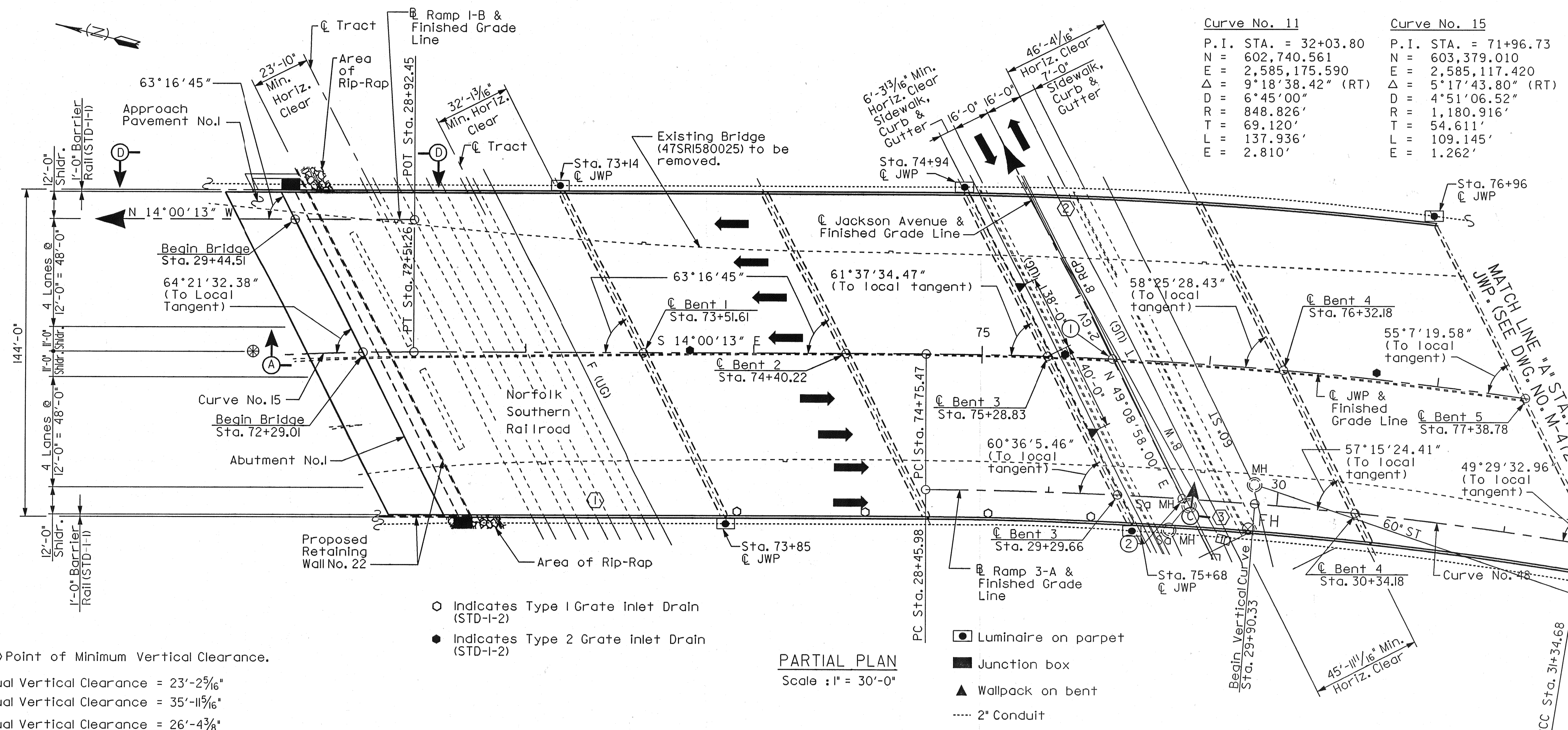
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIR DETAILS

BRIDGE NO. 47-10040-19.30
FEDERAL BRIDGE ID NO. 47100400205
JAMES WHITE PARKWAY OVER NORFOLK
SOUTHERN RAILWAY, WILLOW AVENUE,
AND JACKSON AVENUE
KNOX COUNTY

2026 BR-133-318

\$USERS
 \$SUNES
 \$DATES
 \$FILES



| DRAIN LOCATIONS JWP | |
|--------------------------------|--|
| 73+72.00 LT. (Median) | |
| 73+93.00 RT. (Barrier Parapet) | |
| 74+49.00 RT. (Barrier Parapet) | |
| 75+04.00 RT. (Barrier Parapet) | |
| 75+36.50 LT. (Median) | |
| 75+51.00 RT. (Barrier Parapet) | |
| 76+73.00 LT. (Median) | |

| DRAIN LOCATIONS RAMP 3A | |
|--------------------------------|--|
| 31+42.00 RT. (Barrier Parapet) | |

| CONSTR. NO. 47003-3154-44 | | |
|---------------------------|------|-----------|
| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
|-----------|----------|-----|---------------------------------|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 5-31-05 | CMD | REVISED LIMITS OF STRUCTURE |
| 2 | 05-25-07 | VD | Revised Profiles and Elevations |

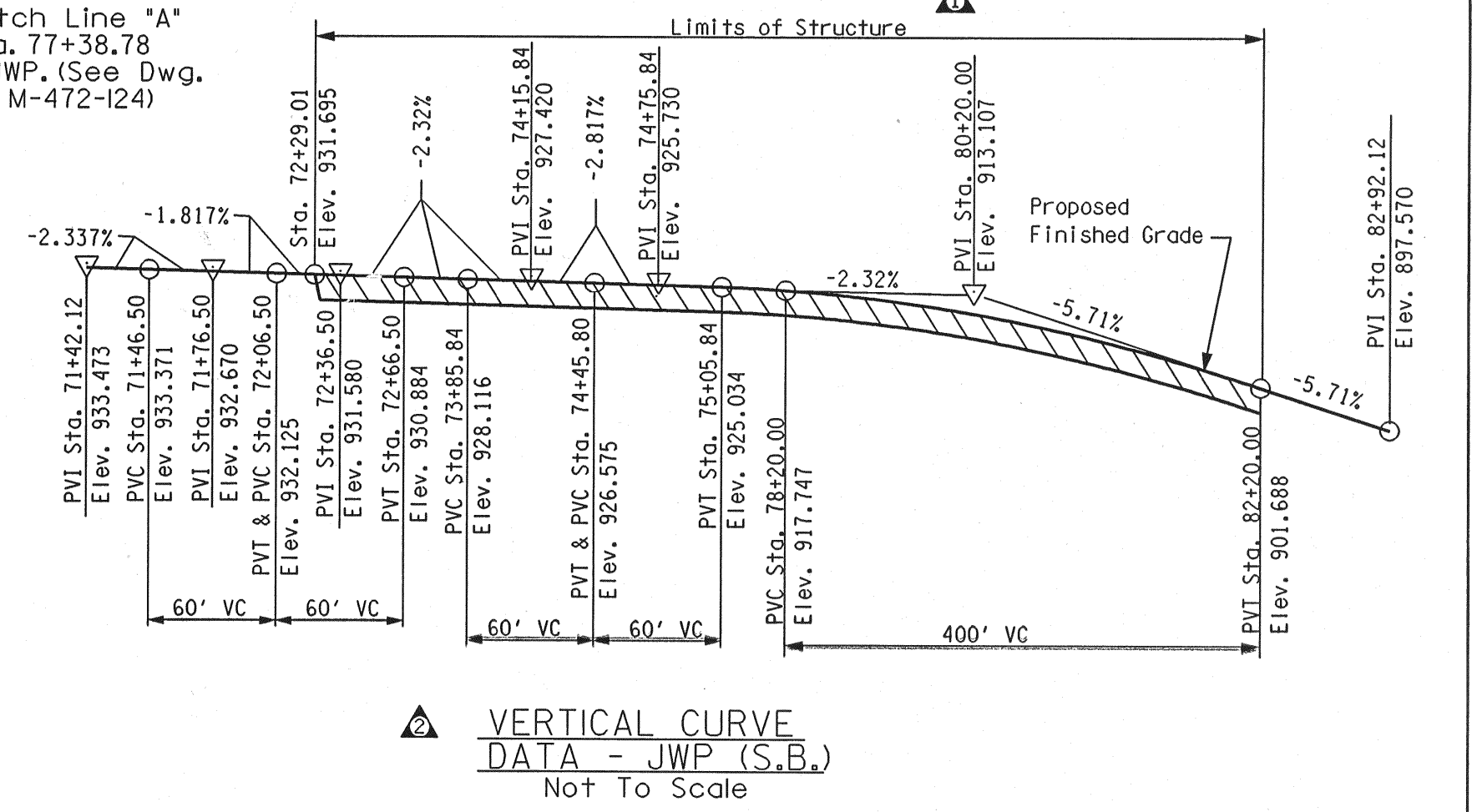
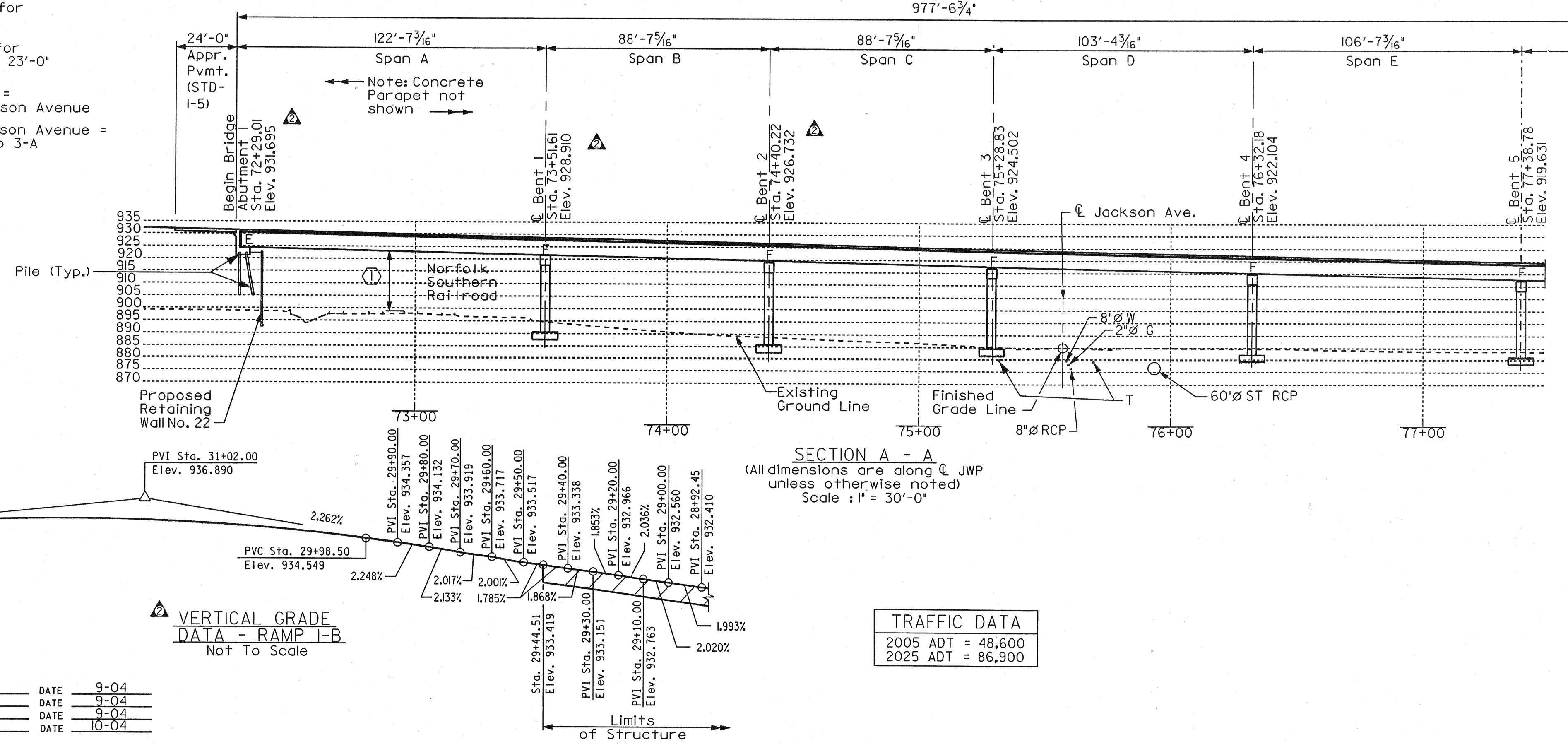
NOTES:
 F - Denotes Fixed.
 E - Denotes Expansion.
 JWP - Denotes James White Pkwy. (S.B.)
 → - Indicates direction of one lane of traffic.
 ⊕ Indicates Guardrail attachment

For Roadway Data not shown, see Roadway Plans.
 Design Velocity = 45 mph on Ramp 3-A, 3-B, & JWP.
 Design Velocity = 50 mph on Ramp I-B.
 For Superelevation Information, please refer to DWG. M-472-132.

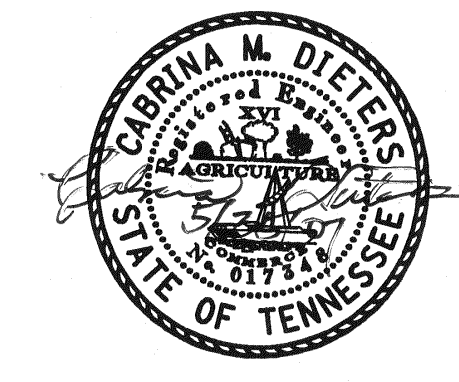
Where underground utilities may be encountered during construction, it shall be the Contractor's responsibility to protect these lines from any damage. Where relocation or readjustment of a utility is required within the scope of this contract, it shall be done by the Contractor. Where relocation or readjustment of a utility is required outside the scope of this contract, it shall be done by the owner of the utility.

- ①②③ Point of Minimum Vertical Clearance.
- ① Actual Vertical Clearance = 23'-2⁵/₁₆"
- ② Actual Vertical Clearance = 35'-11⁵/₁₆"
- ③ Actual Vertical Clearance = 26'-4³/₁₆"

Minimum Vertical Clearance for Jackson Avenue = 14'-6"
 Minimum Vertical Clearance for Norfolk-Southern Railroad = 23'-0"
 ① P.O.C. Sta. 75+57.24 @ JWP = P.O.T. Sta. 13+68.66 @ Jackson Avenue
 ② P.O.T. Sta. 13+00.53 @ Jackson Avenue = P.O.C. Sta. 29+58.54 @ Ramp 3-A



| | | | |
|---------------|-----|------|-------|
| DESIGNED BY | AAM | DATE | 9-04 |
| DRAWN BY | LEL | DATE | 9-04 |
| SUPERVISED BY | FCB | DATE | 9-04 |
| CHECKED BY | CCS | DATE | 10-04 |

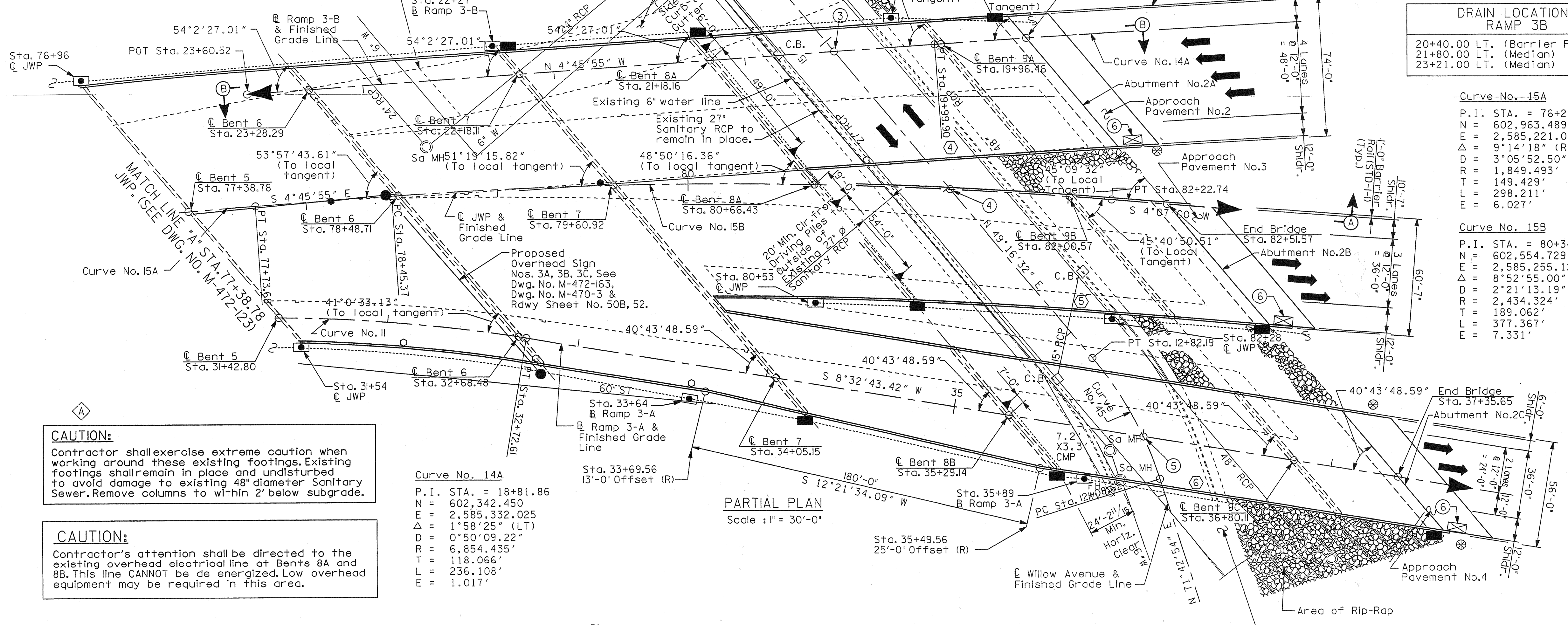


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS

**LAYOUT OF BRIDGE NO. 21(1)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 BRIDGE ID. NO. 17SRI580025
 KNOX COUNTY
 2005**

CORRECT
 ENGINEER OF STRUCTURES
 APPROVED
 DIRECTOR OF HIGHWAYS
 M-472-123

| Curve No. 11 | Curve No. 45 |
|----------------------|----------------------|
| P.I. STA. = 32+03.80 | P.I. STA. = 12+45.88 |
| N = 602,740.561 | N = 602,350.859 |
| E = 2,585,175.590 | E = 2,585,129.678 |
| Δ = 9°18'38.42" (RT) | Δ = 22°26'22" (LT) |
| D = 6°45'00" | D = 30°30'00" |
| R = 848.826' | R = 187.855' |
| T = 69.120' | T = 37.264' |
| L = 137.936' | L = 73.572' |
| E = 2.810' | E = 3.660' |

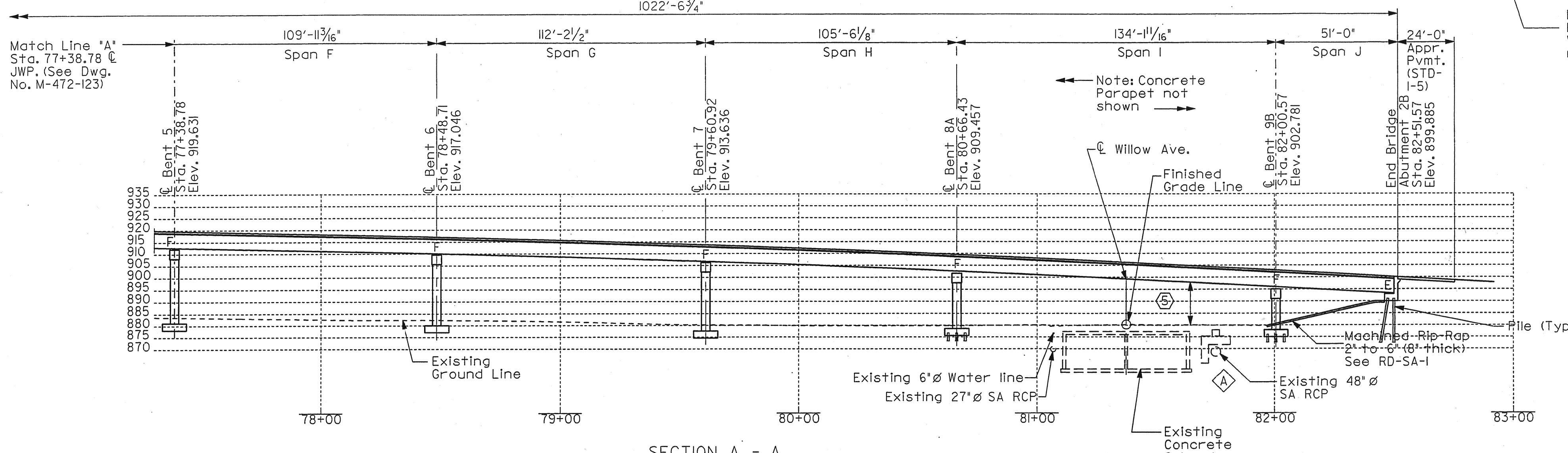


CAUTION:
Contractor shall exercise extreme caution when working around these existing footings. Existing footings shall remain in place and undisturbed to avoid damage to existing 48" diameter Sanitary Sewer. Remove columns to within 2' below subgrade.

CAUTION:
Contractor's attention shall be directed to the existing overhead electrical line at Bents 8A and 8B. This line CANNOT be de-energized. Low overhead equipment may be required in this area.

| Curve No. 14A |
|----------------------|
| P.I. STA. = 18+81.86 |
| N = 602,342.450 |
| E = 2,585,332.025 |
| Δ = 1°58'25" (LT) |
| D = 0°50'09.22" |
| R = 6,854.435' |
| T = 118.066' |
| L = 236.108' |
| E = 1.017' |

PARTIAL PLAN
Scale: 1" = 30'-0"



| Sta. | Elev. | |
|---------------|---------------|--------------|
| Sta. 12+74.00 | Elev. 886.71 | |
| -1.724% | Sta. 13+00.00 | Elev. 886.26 |
| -1.900% | Sta. 13+25.00 | Elev. 885.78 |
| -1.868% | Sta. 13+50.00 | Elev. 885.32 |
| -1.470% | Sta. 13+75.00 | Elev. 884.95 |
| -1.463% | Sta. 14+00.00 | Elev. 884.58 |
| -1.399% | Sta. 14+25.00 | Elev. 884.23 |
| -1.293% | Sta. 14+50.00 | Elev. 883.91 |
| -0.763% | Sta. 14+75.00 | Elev. 883.72 |

VERTICAL GRADE
DATA - JACKSON AVE.
Not To Scale

○ Indicates Type I Grate Inlet Drain (STD-I-2)
● Indicates Type 2 Grate Inlet Drain (STD-I-2)

| DRAIN LOCATIONS RAMP 3A | |
|--------------------------------|--|
| 32+09.00 RT. (Barrier Parapet) | |
| 32+80.00 RT. (Barrier Parapet) | |
| 33+62.00 RT. (Barrier Parapet) | |

| DRAIN LOCATIONS RAMP 3B | |
|--------------------------------|--|
| 20+40.00 LT. (Barrier Parapet) | |
| 21+80.00 LT. (Median) | |
| 23+21.00 LT. (Median) | |

- NOTES:**
- F - Denotes Fixed.
 - E - Denotes Expansion.
 - JWP - Denotes James White Pkwy. (S.B.)
 - ➔ - Indicates direction of one lane of traffic.
 - ⊗ Indicates Guardrail attachment
 - ⊠ Indicates end of bridge drain
 - For Roadway Data not shown, see Roadway Plans. Design Velocity = 45 mph on Ramp 3-A, 3-B, & JWP. Design Velocity = 50 mph on Ramp 1-B.
 - ③ P.O.T. Sta. 20+52.81 @ Ramp 3-B = P.O.T. Sta. 14+93.18 @ Willow Avenue
 - ④ P.O.C. Sta. 81+37.61 @ JWP. = P.O.T. Sta. 13+98.65 @ Willow Avenue
 - ⑤ P.O.C. Sta. 12+32.72 @ Willow Avenue = P.O.T. Sta. 36+00.29 @ Ramp 3-A
 - ⑥ End of Bridge Drain (4'-0"x8'-7", see STD-I-6, 7 & 9)
 - ④⑤⑥ Point of Minimum Vertical Clearance.
 - ④ Actual Vertical Clearance = 19'-7³/₁₆"
 - ⑤ Actual Vertical Clearance = 14'-7¹/₁₆"
 - ⑥ Actual Vertical Clearance = 14'-9³/₁₆"
 - Minimum Vertical Clearance for Willow Avenue = 14'-6"
 - ⊠ Luminaire on parapet
 - Junction box
 - ▲ Wallpack on bent
 - 2" Conduit

| CONSTR. NO. 47003-3154-44 | | |
|---------------------------|------|-----------|
| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
|-----------|---------|-----|-------------------|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 5-31-05 | CMD | GENERAL REVISIONS |



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS

LAYOUT OF BRIDGE NO. 21(2) Δ
JAMES WHITE PKWY OVER
NORFOLK SOUTHERN R.WY.,
WILLOW AVE. AND JACKSON AVE.
STA. 81+37.61 @ JWP (S.B.)
BRIDGE ID. NO. 47SRI580025
KNOX COUNTY
2005

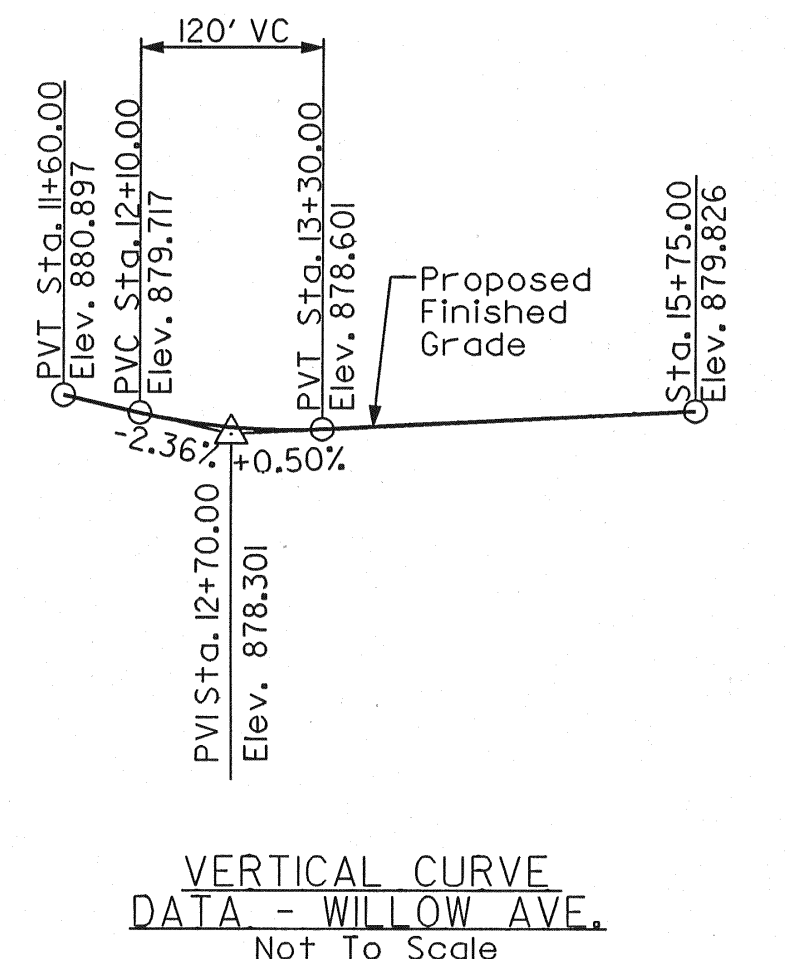
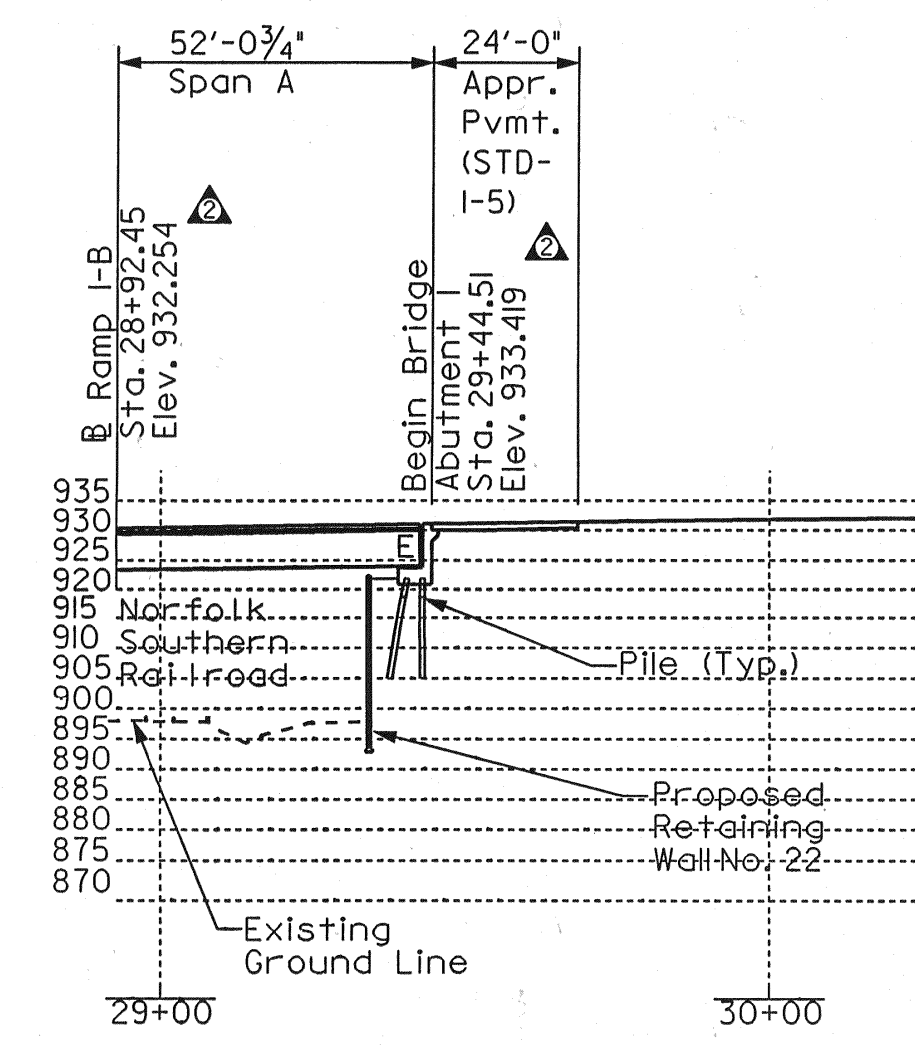
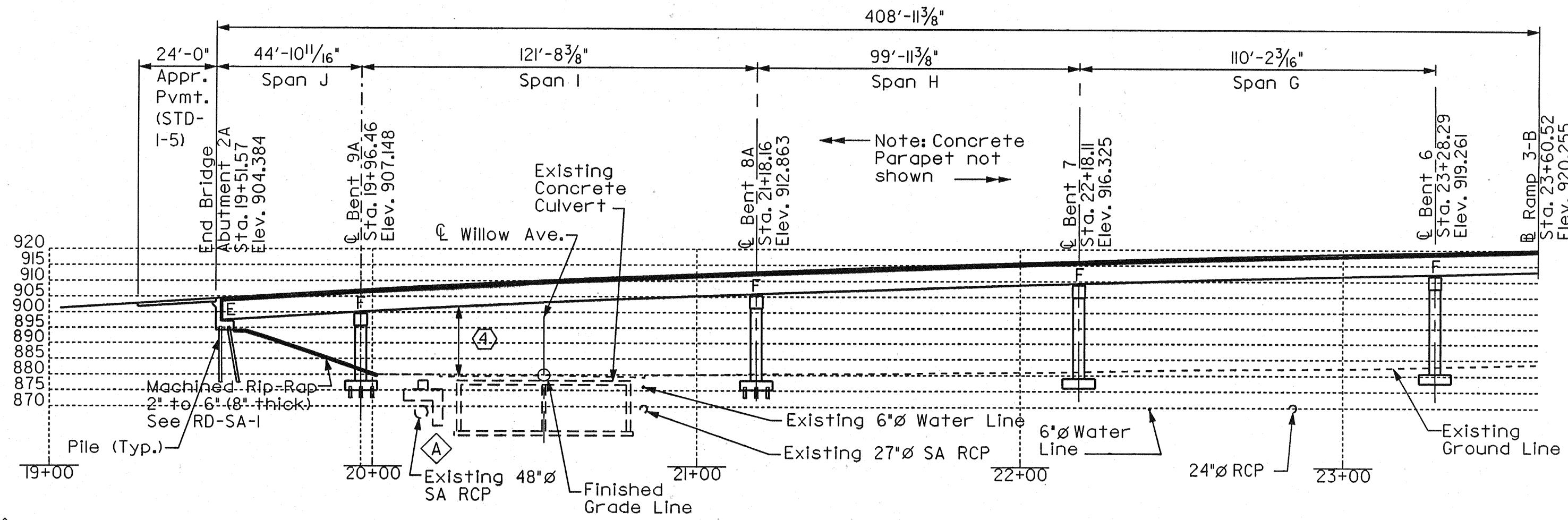
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|---------------|-----|------|-------|
| DESIGNED BY | AAM | DATE | 9-04 |
| DRAWN BY | VD | DATE | 9-04 |
| SUPERVISED BY | FGB | DATE | 9-04 |
| CHECKED BY | CCS | DATE | 10-04 |

CORRECT _____
ENGINEER OF STRUCTURES

APPROVED _____
DIRECTOR OF HIGHWAYS

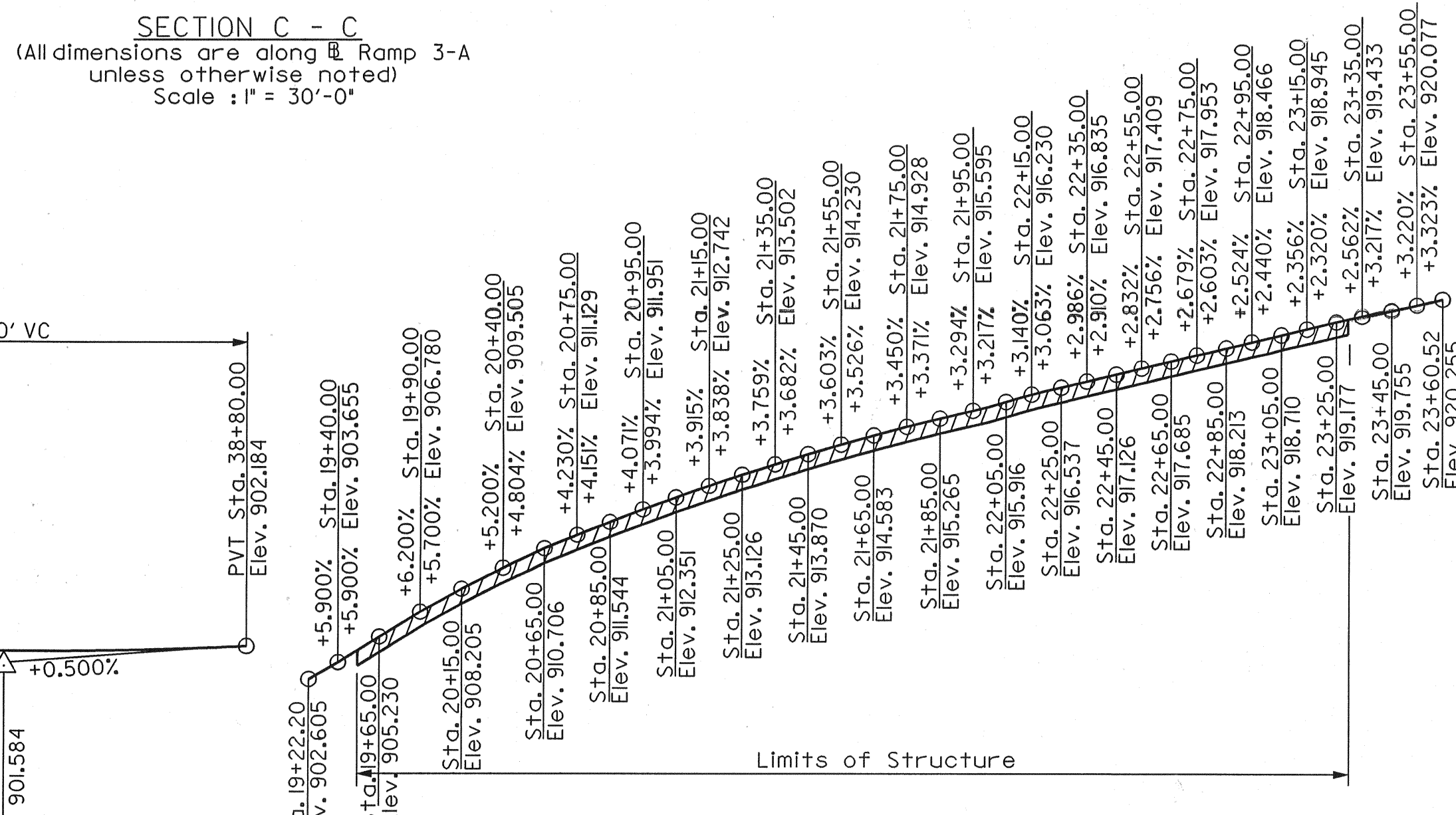
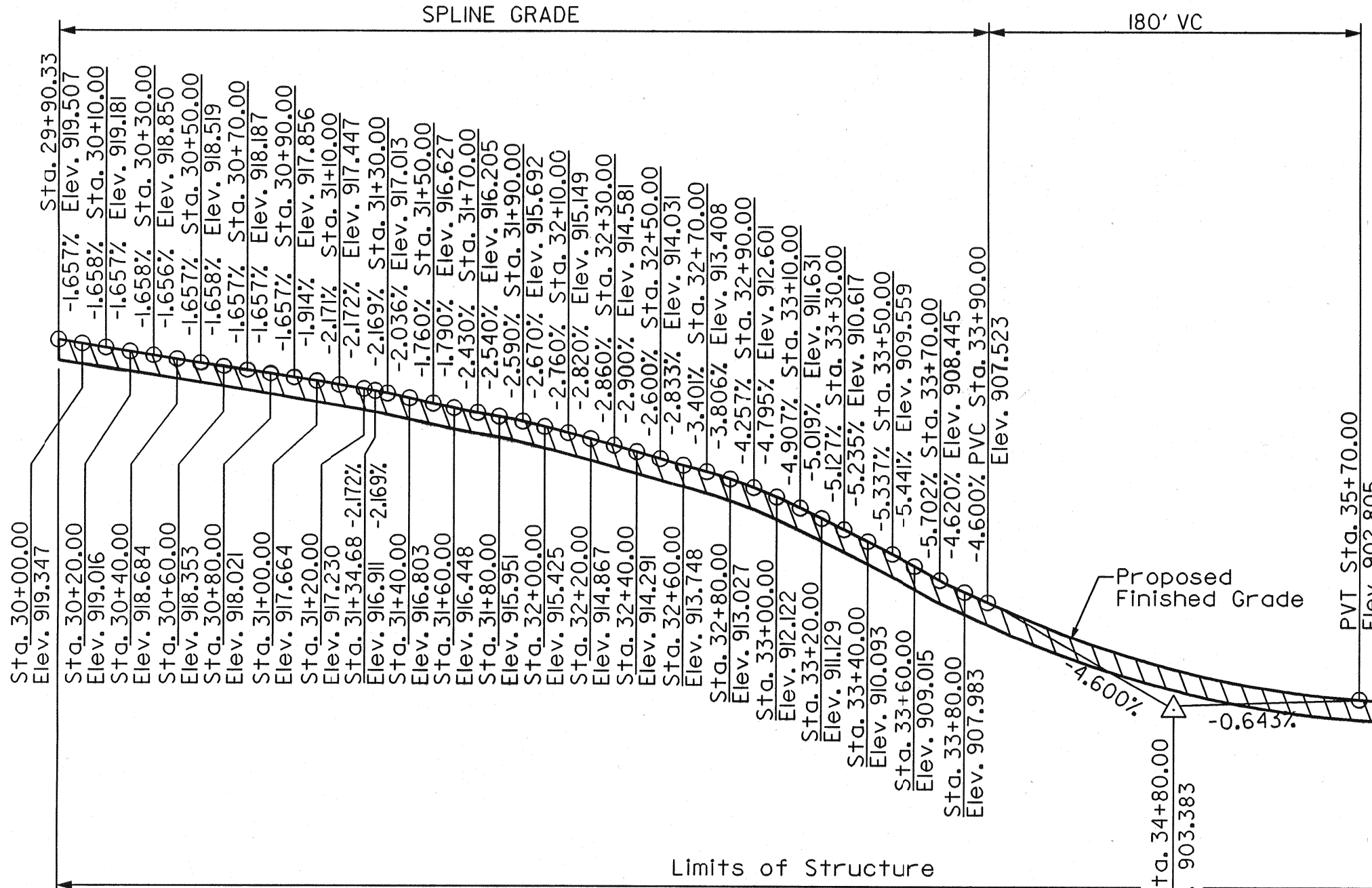
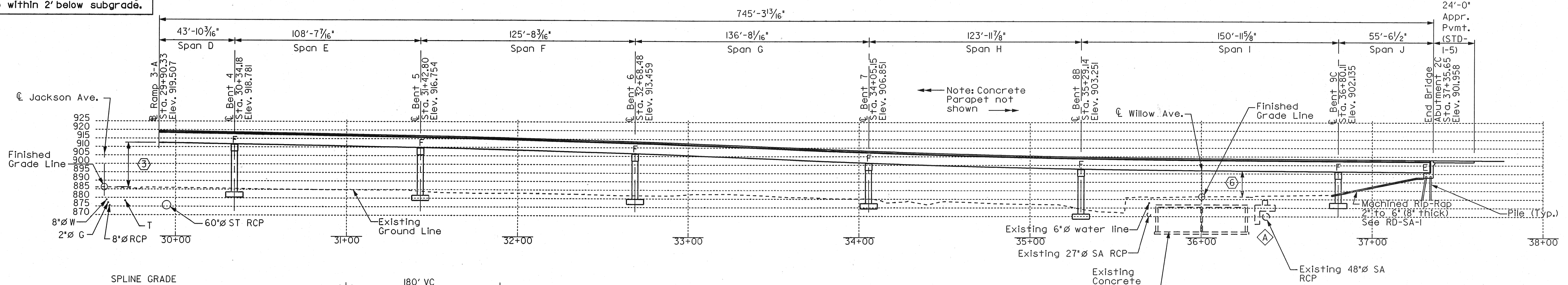
M-472-124

| CONSTR. NO. 47003-3154-44 | | | |
|---------------------------|----------|-----------|-------------------|
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 5-31-05 | CMD | GENERAL REVISIONS |
| 2 | 05-25-07 | SD | Revised Elevation |



NOTES:
 F - Denotes Fixed.
 E - Denotes Expansion.
 JWP - Denotes James White Pkwy. (S.B.)
 For Roadway Data not shown, see Roadway Plans.
 Design Velocity = 45 mph on Ramp 3-A, 3-B, & JWP.
 Design Velocity = 50 mph on Ramp I-B.

CAUTION:
 Contractor shall exercise extreme caution when working around these existing footings. Existing footings shall remain in place and undisturbed to avoid damage to existing 48" diameter Sanitary Sewer. Remove columns to within 2' below subgrade.



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
LAYOUT OF BRIDGE NO. 21(3) Δ
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 BRIDGE ID. NO. 47SRI580025
 KNOX COUNTY
 2005

DESIGNED BY: AAM
 DRAWN BY: LFB
 SUPERVISED BY: CCB
 CHECKED BY: CCS

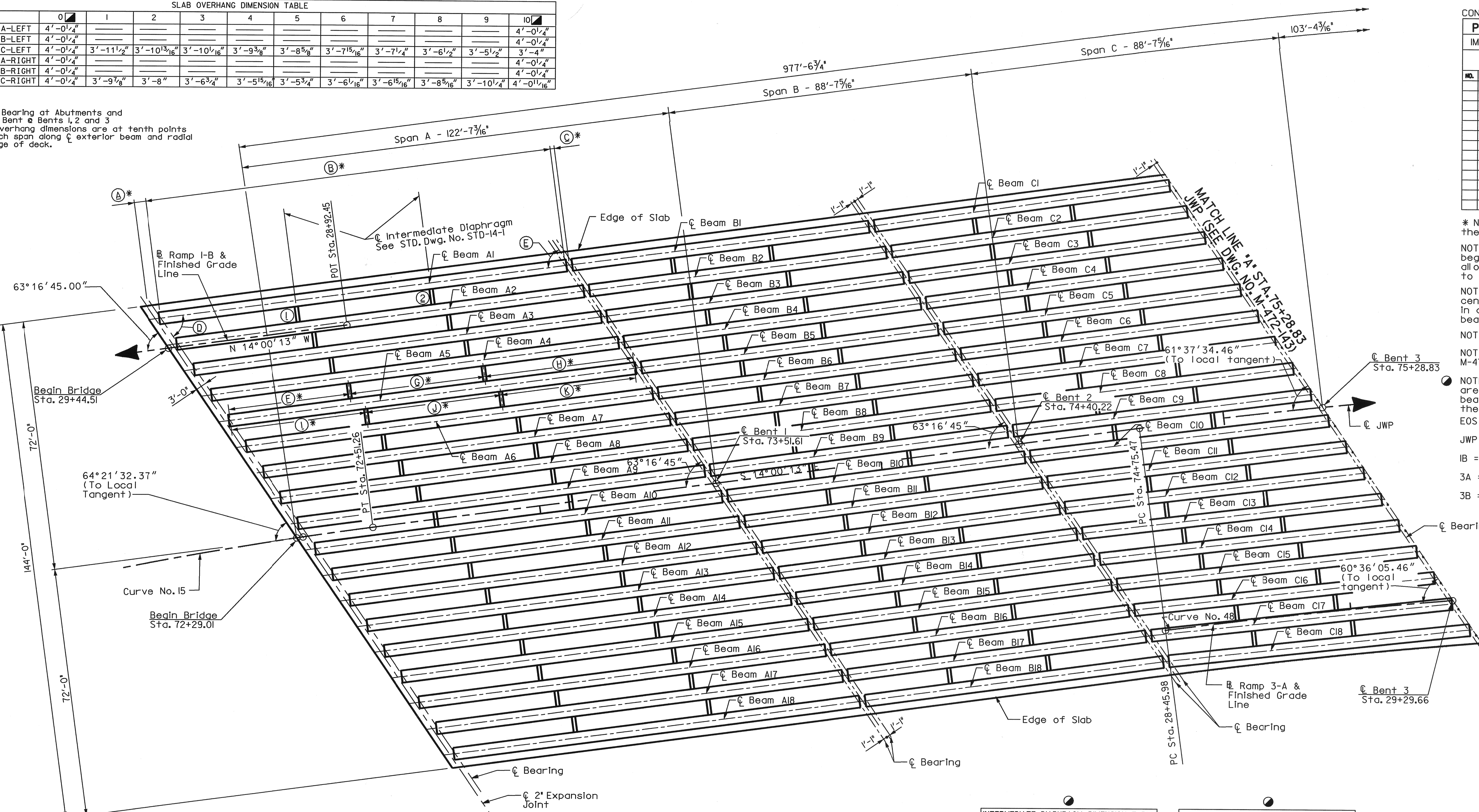
DATE: 1-04
 DATE: 1-04
 DATE: 1-04
 DATE: 10-04

CORRECT
 ENGINEER OF STRUCTURES
 APPROVED
 DIRECTOR OF HIGHWAYS

M-472-125

| | 0 | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 |
|--------------|-----------|------------|-------------|-------------|-------------|-----------|-------------|------------|------------|------------|------------|
| SPAN A-LEFT | 4'-0 1/4" | | | | | | | | | | 4'-0 1/4" |
| SPAN B-LEFT | 4'-0 1/4" | | | | | | | | | | 4'-0 1/4" |
| SPAN C-LEFT | 4'-0 1/4" | 3'-11 1/2" | 3'-10 3/16" | 3'-10 1/16" | 3'-9 3/8" | 3'-8 5/8" | 3'-7 15/16" | 3'-7 1/4" | 3'-6 1/2" | 3'-5 1/2" | 3'-4" |
| SPAN A-RIGHT | 4'-0 1/4" | | | | | | | | | | 4'-0 1/4" |
| SPAN B-RIGHT | 4'-0 1/4" | | | | | | | | | | 4'-0 1/4" |
| SPAN C-RIGHT | 4'-0 1/4" | 3'-9 1/8" | 3'-8" | 3'-6 3/4" | 3'-5 15/16" | 3'-5 3/4" | 3'-6 1/16" | 3'-6 5/16" | 3'-8 5/16" | 3'-10 1/4" | 4'-0 1/16" |

- Bearing at Abutments and Bent & Bents 1, 2 and 3
 Slab overhang dimensions are at tenth points of each span along centerline bearing and radial to edge of deck.



PARTIAL FRAMING PLAN
SPANS A-C
Scale: 1" = 15'-0"

| DIMENSION ALONG CENTERLINE BEARING ABUTMENT NO. 1 | | | | | | | | | | | | | | | | | | | | |
|---|-------------|-------------|-------------|-------------|-------------|-------------|-------------|-------------|-------------|------------|-------------|-------------|-------------|-------------|-------------|-------------|-------------|-------------|------------|------------|
| EOS TO A1 | A1 TO A2 | A2 TO 1B | 1B TO A3 | A3 TO A4 | A4 TO A5 | A5 TO A6 | A6 TO A7 | A7 TO A8 | A8 TO A9 | A9 TO JWP | JWP TO A10 | A10 TO A11 | A11 TO A12 | A12 TO A13 | A13 TO A14 | A14 TO A15 | A15 TO A16 | A16 TO EOS | | |
| 4'-6" | 8'-11 1/16" | 1'-1 3/16" | 7'-10 1/4" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 4'-7 3/4" | 4'-3 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 4'-6" | |
| DIMENSION ALONG CENTERLINE BENT NO. 1 | | | | | | | | | | | | | | | | | | | | |
| EOS TO B1 | B1 TO B2 | B2 TO B3 | B3 TO B4 | B4 TO B5 | B5 TO B6 | B6 TO B7 | B7 TO B8 | B8 TO B9 | B9 TO JWP | JWP TO B10 | B10 TO B11 | B11 TO B12 | B12 TO B13 | B13 TO B14 | B14 TO B15 | B15 TO B16 | B16 TO B17 | B17 TO B18 | B18 TO EOS | |
| 4'-6" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 4'-5 3/4" | 4'-5 3/4" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 4'-6" | |
| DIMENSION ALONG CENTERLINE BENT NO. 2 | | | | | | | | | | | | | | | | | | | | |
| EOS TO C1 | C1 TO C2 | C2 TO C3 | C3 TO C4 | C4 TO C5 | C5 TO C6 | C6 TO C7 | C7 TO C8 | C8 TO C9 | C9 TO JWP | JWP TO C10 | C10 TO C11 | C11 TO C12 | C12 TO C13 | C13 TO C14 | C14 TO C15 | C15 TO C16 | C16 TO C17 | C17 TO C18 | C18 TO EOS | |
| 4'-6" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 4'-5 3/4" | 4'-5 3/4" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 8'-11 1/16" | 4'-6" | |
| DIMENSION ALONG CENTERLINE BENT NO. 3 | | | | | | | | | | | | | | | | | | | | |
| EOS TO D1 | D1 TO D2 | D2 TO D3 | D3 TO D4 | D4 TO D5 | D5 TO D6 | D6 TO D7 | D7 TO D8 | D8 TO D9 | D9 TO JWP | JWP TO D10 | D10 TO D11 | D11 TO D12 | D12 TO D13 | D13 TO D14 | D14 TO D15 | D15 TO D16 | D16 TO 3A | 3A TO D17 | D17 TO D18 | D18 TO EOS |
| 3'-9" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 4'-6 1/16" | 4'-6 1/16" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 9'-1 5/8" | 1'-1 5/8" | 9'-1 5/8" | 4'-8" | |

** NOTE: Dimensions along Centerline Bent for beams in adjacent span are identical.

DESIGNED BY: BRW
 DRAWN BY: LBL
 SUPERVISED BY: FGB
 CHECKED BY: AAM
 DATE: 9-04
 DATE: 9-04
 DATE: 9-04
 DATE: 9-04

| | | | |
|---------------------------|-----------------------------|-----------|-----------|
| CONSTR. NO. 47003-3154-44 | PROJECT NO. IM/NH-40-8(I35) | YEAR 2005 | SHEET NO. |
|---------------------------|-----------------------------|-----------|-----------|

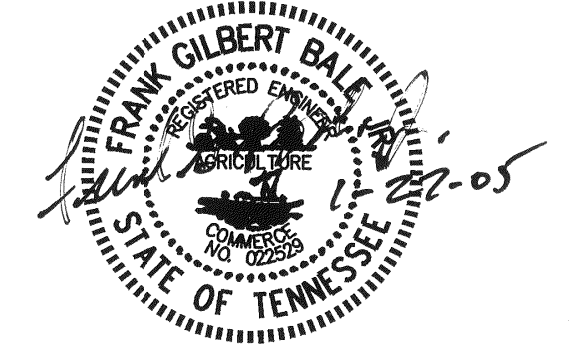
| NO. | DATE | BY | BRIEF DESCRIPTION |
|-----|------|----|-------------------|
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| | | | |

* NOTE: All dimensions are measured along the centerline of beam.
 NOTE: Dimension A in span A is from begin bridge to centerline bearing and in all other spans is from centerline bent to centerline bearing.
 NOTE: Dimension C in span I is from centerline bearing to end of bridge and in all other spans is from centerline bearing to centerline bent.
 NOTE: Dimensions A-K are typical each span.
 NOTE: For dimensions A-K see Dwg No. M-472-147 & 148.
 NOTE: All intermediate diaphragm dimensions are measured perpendicular from that beam centerline to centerline of beam to the right.
 EOS = Edge Of Slab
 JWP = Baseline James White Parkway South
 IB = Baseline Ramp I-B
 3A = Baseline Ramp 3-A
 3B = Baseline Ramp 3-B

| SPAN | BEAM | (1) | (2) |
|------|------|-------|-------|
| A | 1 | 8'-0" | 8'-0" |
| A | 2 | 8'-0" | 8'-0" |
| A | 3 | 8'-0" | 8'-0" |
| A | 4 | 8'-0" | 8'-0" |
| A | 5 | 8'-0" | 8'-0" |
| A | 6 | 8'-0" | 8'-0" |
| A | 7 | 8'-0" | 8'-0" |
| A | 8 | 8'-0" | 8'-0" |
| A | 9 | 8'-0" | 8'-0" |
| A | 10 | 8'-0" | 8'-0" |
| A | 11 | 8'-0" | 8'-0" |
| A | 12 | 8'-0" | 8'-0" |
| A | 13 | 8'-0" | 8'-0" |
| A | 14 | 8'-0" | 8'-0" |
| A | 15 | 8'-0" | 8'-0" |
| A | 16 | 8'-0" | 8'-0" |
| A | 17 | 8'-0" | 8'-0" |
| A | 18 | 8'-0" | 8'-0" |

| SPAN | BEAM | (1) | (2) |
|------|------|-------|-------|
| B | 1 | 8'-0" | 8'-0" |
| B | 2 | 8'-0" | 8'-0" |
| B | 3 | 8'-0" | 8'-0" |
| B | 4 | 8'-0" | 8'-0" |
| B | 5 | 8'-0" | 8'-0" |
| B | 6 | 8'-0" | 8'-0" |
| B | 7 | 8'-0" | 8'-0" |
| B | 8 | 8'-0" | 8'-0" |
| B | 9 | 8'-0" | 8'-0" |
| B | 10 | 8'-0" | 8'-0" |
| B | 11 | 8'-0" | 8'-0" |
| B | 12 | 8'-0" | 8'-0" |
| B | 13 | 8'-0" | 8'-0" |
| B | 14 | 8'-0" | 8'-0" |
| B | 15 | 8'-0" | 8'-0" |
| B | 16 | 8'-0" | 8'-0" |
| B | 17 | 8'-0" | 8'-0" |
| B | 18 | 8'-0" | 8'-0" |

| SPAN | BEAM | (1) | (2) |
|------|------|--------------|------------|
| C | 1 | 8'-0 1/2" | 8'-1 1/2" |
| C | 2 | 8'-0 3/4" | 8'-1 1/16" |
| C | 3 | 8'-0 11/16" | 8'-1 1/16" |
| C | 4 | 8'-0 5/8" | 8'-1 1/4" |
| C | 5 | 8'-0 1/2" | 8'-1 1/8" |
| C | 6 | 8'-0 1/8" | 8'-1 1/16" |
| C | 7 | 8'-0 5/16" | 8'-0 1/16" |
| C | 8 | 8'-0 1/4" | 8'-0 1/16" |
| C | 9 | 8'-0 1/8" | 8'-0 3/4" |
| C | 10 | 8'-0 1/16" | 8'-0 1/16" |
| C | 11 | 8'-0" | 8'-0 5/8" |
| C | 12 | 7'-11 1/16" | 8'-0 1/2" |
| C | 13 | 7'-11 13/16" | 8'-0 1/16" |
| C | 14 | 7'-11 1/16" | 8'-0 5/16" |
| C | 15 | 7'-11 5/8" | 8'-0 1/4" |
| C | 16 | 7'-11 1/2" | 8'-0 1/8" |
| C | 17 | 7'-11 1/16" | 8'-0 1/16" |
| C | 18 | 7'-11 1/16" | 8'-0 1/16" |

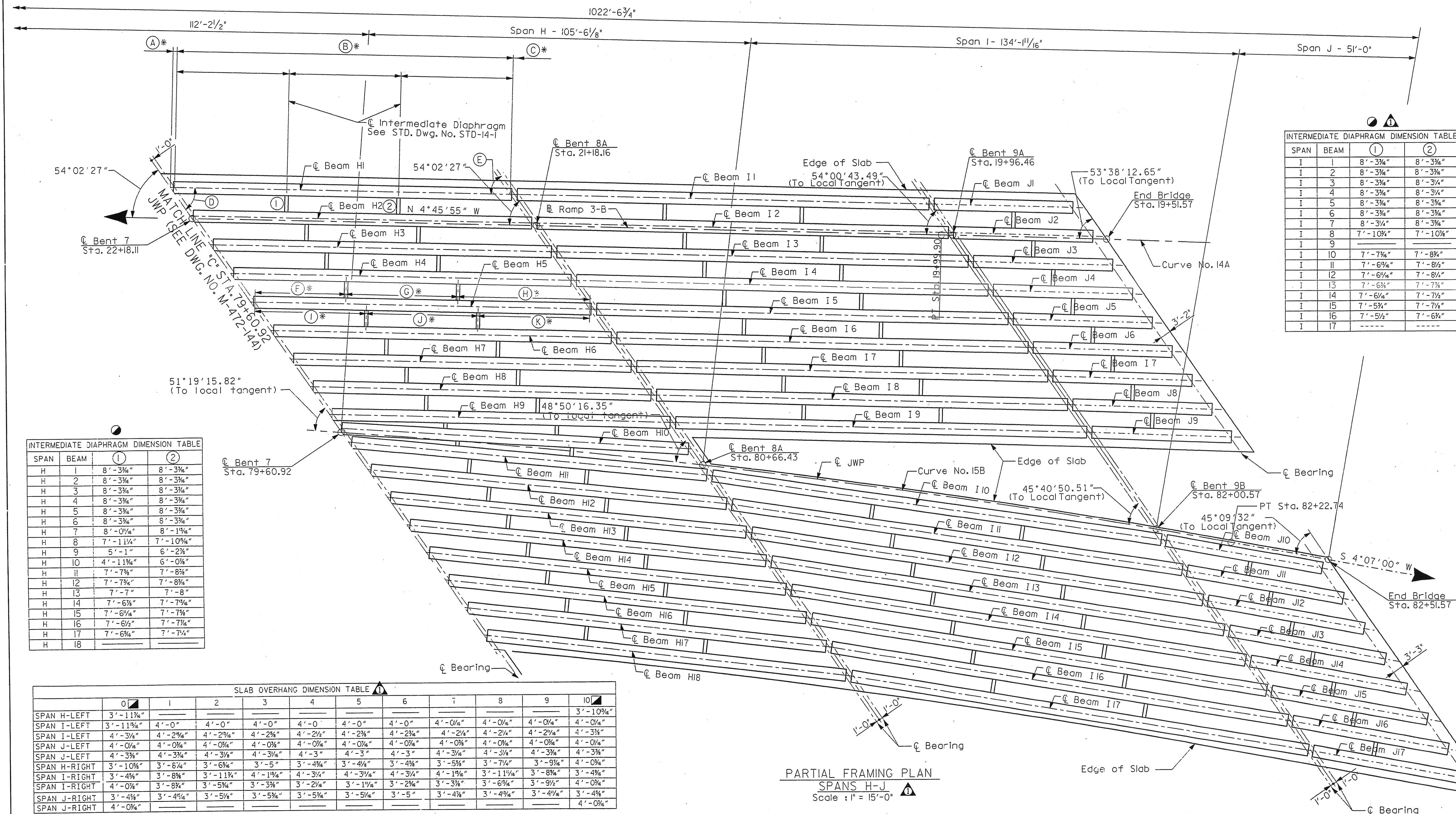


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (10)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

CORRECT ENGINEER OF STRUCTURES
 APPROVED DIRECTOR OF HIGHWAYS
 M-472-142

| PROJECT NO. | YEAR | SHEET NO. |
|-----------------|------|-----------|
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
|-----------|----------|-----|-------------------|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 10-28-05 | CMD | ADDED SPAN J |



INTERMEDIATE DIAPHRAGM DIMENSION TABLE

| SPAN | BEAM | (1) | (2) |
|------|------|------------|------------|
| I | 1 | 8'-3 3/4" | 8'-3 3/4" |
| I | 2 | 8'-3 3/4" | 8'-3 3/4" |
| I | 3 | 8'-3 3/4" | 8'-3 3/4" |
| I | 4 | 8'-3 3/4" | 8'-3 3/4" |
| I | 5 | 8'-3 3/4" | 8'-3 3/4" |
| I | 6 | 8'-3 3/4" | 8'-3 3/4" |
| I | 7 | 8'-3 3/4" | 8'-3 3/4" |
| I | 8 | 7'-10 1/4" | 7'-10 1/4" |
| I | 9 | | |
| I | 10 | 7'-7 3/4" | 7'-8 1/4" |
| I | 11 | 7'-6 3/4" | 7'-8 1/4" |
| I | 12 | 7'-6 3/4" | 7'-8 1/4" |
| I | 13 | 7'-6 3/4" | 7'-7 3/4" |
| I | 14 | 7'-6 3/4" | 7'-7 3/4" |
| I | 15 | 7'-5 1/2" | 7'-7 3/4" |
| I | 16 | 7'-5 1/2" | 7'-6 3/4" |
| I | 17 | | |

INTERMEDIATE DIAPHRAGM DIMENSION TABLE

| SPAN | BEAM | (1) | (2) |
|------|------|------------|------------|
| H | 1 | 8'-3 3/4" | 8'-3 3/4" |
| H | 2 | 8'-3 3/4" | 8'-3 3/4" |
| H | 3 | 8'-3 3/4" | 8'-3 3/4" |
| H | 4 | 8'-3 3/4" | 8'-3 3/4" |
| H | 5 | 8'-3 3/4" | 8'-3 3/4" |
| H | 6 | 8'-3 3/4" | 8'-3 3/4" |
| H | 7 | 8'-0 1/4" | 8'-1 1/4" |
| H | 8 | 7'-11 1/4" | 7'-10 1/4" |
| H | 9 | 5'-1" | 6'-2 1/4" |
| H | 10 | 4'-11 1/4" | 6'-0 1/4" |
| H | 11 | 7'-7 3/4" | 7'-8 1/4" |
| H | 12 | 7'-7 3/4" | 7'-8 1/4" |
| H | 13 | 7'-7" | 7'-8" |
| H | 14 | 7'-6 3/4" | 7'-7 3/4" |
| H | 15 | 7'-6 3/4" | 7'-7 3/4" |
| H | 16 | 7'-6 3/4" | 7'-7 3/4" |
| H | 17 | 7'-6 3/4" | 7'-7 3/4" |
| H | 18 | | |

INTERMEDIATE DIAPHRAGM DIMENSION TABLE

| SPAN | BEAM | (1) | (2) |
|------|------|------------|------|
| J | 1 | 8'-3 3/4" | ---- |
| J | 2 | 8'-3 3/4" | ---- |
| J | 3 | 8'-3 3/4" | ---- |
| J | 4 | 8'-3 3/4" | ---- |
| J | 5 | 8'-3 3/4" | ---- |
| J | 6 | 8'-3 3/4" | ---- |
| J | 7 | 8'-3 3/4" | ---- |
| J | 8 | 7'-10 1/4" | ---- |
| J | 9 | | ---- |
| J | 10 | 7'-7 3/4" | ---- |
| J | 11 | 7'-7 3/4" | ---- |
| J | 12 | 7'-7 3/4" | ---- |
| J | 13 | 7'-7 3/4" | ---- |
| J | 14 | 7'-6 3/4" | ---- |
| J | 15 | 7'-6 3/4" | ---- |
| J | 16 | 7'-6 3/4" | ---- |
| J | 17 | ---- | ---- |

SLAB OVERHANG DIMENSION TABLE

| | 0 | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 |
|---------------|------------|-----------|------------|-----------|-----------|-----------|-----------|-----------|------------|-----------|------------|
| SPAN H-LEFT | 3'-11 1/4" | | | | | | | | | | 3'-10 3/4" |
| SPAN I-LEFT | 3'-11 3/4" | 4'-0" | 4'-0" | 4'-0" | 4'-0" | 4'-0" | 4'-0" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" |
| SPAN I-MIDDLE | 4'-3 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-2 3/4" | 4'-3 3/4" |
| SPAN J-LEFT | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" | 4'-0 1/4" |
| SPAN J-MIDDLE | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" |
| SPAN H-RIGHT | 3'-10 3/4" | 3'-6 3/4" | 3'-6 3/4" | 3'-5" | 3'-4 3/4" | 3'-4 3/4" | 3'-4 3/4" | 3'-5 1/4" | 3'-7 1/4" | 3'-9 1/4" | 4'-0 3/4" |
| SPAN I-RIGHT | 3'-4 3/4" | 3'-8 3/4" | 3'-11 3/4" | 4'-1 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-3 3/4" | 4'-1 3/4" | 3'-11 3/4" | 3'-8 3/4" | 3'-4 3/4" |
| SPAN J-RIGHT | 4'-0 1/4" | 3'-8 3/4" | 3'-5 3/4" | 3'-3 3/4" | 3'-2 3/4" | 3'-1 3/4" | 3'-2 3/4" | 3'-3 3/4" | 3'-6 3/4" | 3'-9 1/4" | 4'-0 3/4" |
| SPAN J-MIDDLE | 3'-4 3/4" | 3'-4 3/4" | 3'-5 3/4" | 3'-5 3/4" | 3'-5 3/4" | 3'-5 3/4" | 3'-5 3/4" | 3'-4 3/4" | 3'-4 3/4" | 3'-4 3/4" | 4'-0 3/4" |
| SPAN J-RIGHT | 4'-0 3/4" | | | | | | | | | | 4'-0 3/4" |

□ Bearing at Abutments 2A & 2B
 ● Bent @ Bents 7, 8A, 9A and 9B
 Slab overhang dimensions are at tenth points of each span along centerline bearing and radial to edge of deck.

FRAMING PLAN DIMENSION TABLE

DIMENSION ALONG CENTERLINE BENT NO. 7

| | | | | | | | | | | | | | | | | | | | |
|------------|------------|------------|-----------|------------|------------|------------|------------|------------|----------|-----------|------------|------------|------------|------------|------------|------------|------------|------------|------------|
| EOS TO G1 | G1 TO G2 | G2 TO 3B | 3B TO G3 | G3 TO G4 | G4 TO G5 | G5 TO G6 | G6 TO G7 | G7 TO G8 | G8 TO G9 | G9 TO JWP | JWP TO G10 | G10 TO G11 | G11 TO G12 | G12 TO G13 | G13 TO G14 | G14 TO G15 | G15 TO G16 | G16 TO G17 | G17 TO G18 |
| 4'-11 1/2" | 10'-2 1/2" | 0'-10 3/4" | 9'-3 3/4" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 6'-5" | 3'-5 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-10 1/4" | 9'-8 3/4" |

DIMENSION ALONG CENTERLINE BENT NO. 8A

| | | | | | | | | | | | | | | | | | | | | |
|------------|------------|------------|-----------|------------|------------|------------|------------|----------|-----------|-----------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|
| EOS TO H1 | H1 TO H2 | H2 TO 3B | 3B TO H3 | H3 TO H4 | H4 TO H5 | H5 TO H6 | H6 TO H7 | H7 TO H8 | H8 TO H9 | H9 TO H10 | H10 TO JWP | JWP TO H11 | H11 TO H12 | H12 TO H13 | H13 TO H14 | H14 TO H15 | H15 TO H16 | H16 TO H17 | H17 TO H18 | H18 TO EOS |
| 4'-11 1/2" | 10'-2 1/2" | 0'-10 3/4" | 9'-3 3/4" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 9'-9" | 9'-5 3/4" | 5'-8 3/4" | 3'-8 3/4" | 3'-8 3/4" | 10'-2 1/4" | 10'-2 1/4" | 10'-2 1/4" | 10'-2 1/4" | 10'-2 1/4" | 10'-2 1/4" | 10'-2 1/4" | 5'-5 1/4" |

DIMENSION ALONG CENTERLINE BENT NOS. 9A AND 9B

| | | | | | | | | | | | | | | | | | | | | |
|------------|------------|------------|-----------|------------|------------|------------|------------|------------|-----------|-----------|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|
| EOS TO I1 | I1 TO I2 | I2 TO 3B | 3B TO I3 | I3 TO I4 | I4 TO I5 | I5 TO I6 | I6 TO I7 | I7 TO I8 | I8 TO I9 | I9 TO EOS | EOS TO JWP | JWP TO I10 | I10 TO I11 | I11 TO I12 | I12 TO I13 | I13 TO I14 | I14 TO I15 | I15 TO I16 | I16 TO I17 | I17 TO EOS |
| 4'-11 1/2" | 10'-2 1/2" | 0'-10 3/4" | 9'-3 3/4" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 9'-9 3/4" | 5'-3 3/4" | 0'-9 3/4" | 3'-11" | 10'-8 3/4" | 10'-8 3/4" | 10'-8 3/4" | 10'-8 3/4" | 10'-8 3/4" | 10'-8 3/4" | 10'-7 3/4" | 10'-7 3/4" |

DIMENSION ALONG CENTERLINE BEARING ABUTMENT NO. 2A

| | | | | | | | | | | |
|------------|------------|------------|-----------|------------|------------|------------|------------|-----------|------------|-----------|
| EOS TO J1 | J1 TO J2 | J2 TO 3B | 3B TO J3 | J3 TO J4 | J4 TO J5 | J5 TO J6 | J6 TO J7 | J7 TO J8 | J8 TO J9 | J9 TO EOS |
| 4'-11 1/2" | 10'-2 1/2" | 0'-10 3/4" | 9'-3 3/4" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 10'-2 1/2" | 9'-9 3/4" | 15'-2 3/4" | 3'-8 3/4" |

DIMENSION ALONG CENTERLINE BEARING ABUTMENT NO. 2B

| | | | | | | | | | |
|------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|
| EOS TO JWP | JWP TO J10 | J10 TO J11 | J11 TO J12 | J12 TO J13 | J13 TO J14 | J14 TO J15 | J15 TO J16 | J16 TO J17 | J17 TO EOS |
| 0'-9 3/4" | 3'-11 3/4" | 10'-9 3/4" | 10'-9 3/4" | 10'-8 3/4" | 10'-8 3/4" | 10'-8 3/4" | 10'-7 3/4" | 10'-7 3/4" | 5'-7 3/4" |



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (13)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN R.W.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

DESIGNED BY: BBW
 DRAWN BY: LBL
 SUPERVISED BY: FGB
 CHECKED BY: AAM
 DATE: 9-04
 DATE: 9-04
 DATE: 9-04

CORRECT _____
 ENGINEER OF STRUCTURES
 APPROVED _____
 DIRECTOR OF HIGHWAYS
 M-472-145

| | | | |
|---------------------------|----------|-----------|-------------------|
| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 10-28-05 | CMD | ADDED SPAN J |

* NOTE: All dimensions are measured along the centerline of beam.

NOTE: Dimension A in span A is from begin bridge to centerline bearing and in all other spans is from centerline bent to centerline bearing.

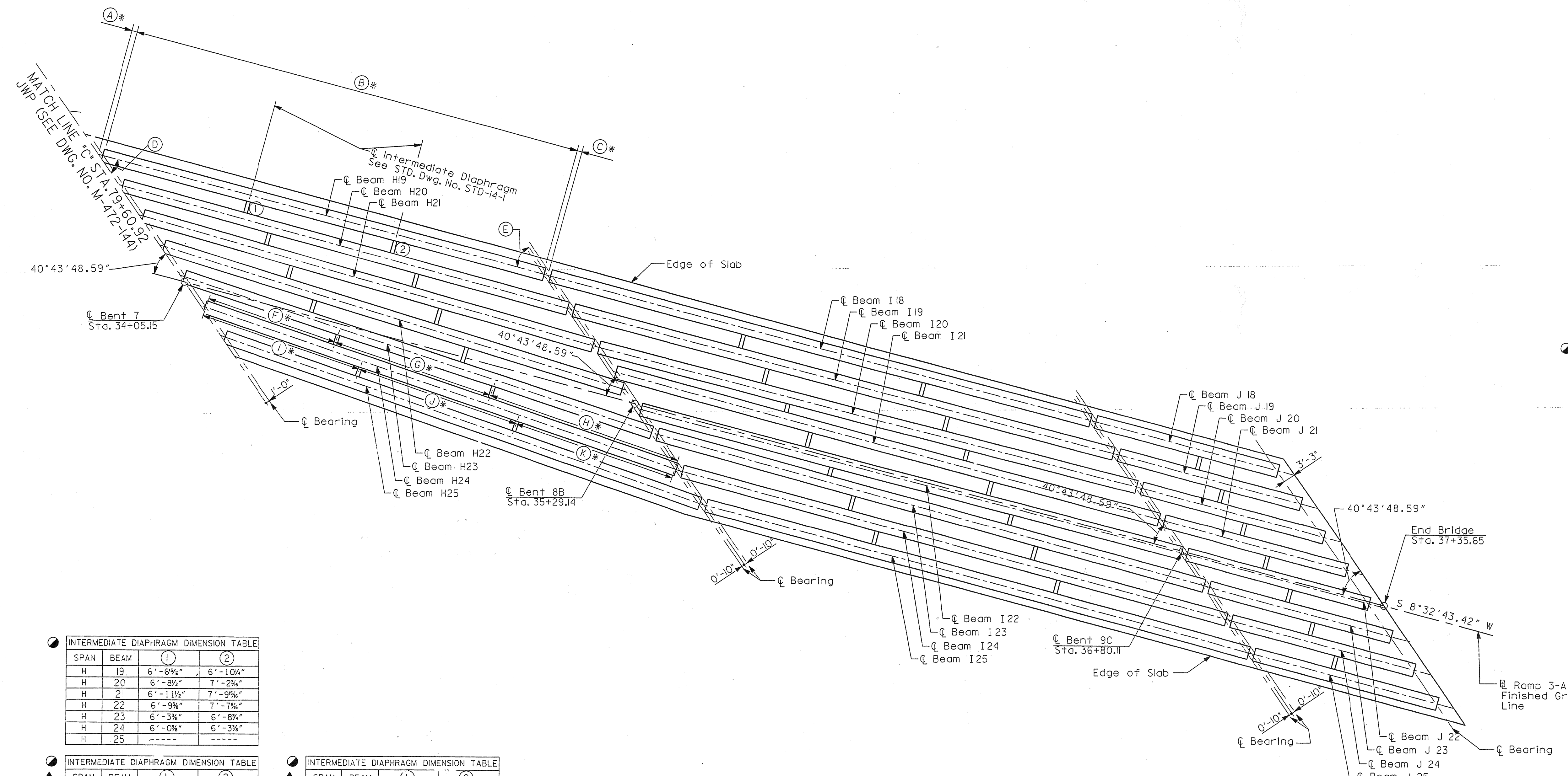
NOTE: Dimension C in span I is from centerline bearing to end-of bridge and in all other spans is from centerline bearing to centerline bent.

NOTE: Dimensions A-K are typical each span.

NOTE: For dimensions A-K see Dwg. No. M-472-147 & 148.

NOTE: All intermediate diaphragm dimensions are measured perpendicular from that beam centerline to centerline of beam to the right.

EOS = Edge Of Slab
 JWP = Baseline James White Parkway South
 IB = Baseline Ramp I-B
 3A = Baseline Ramp 3-A
 3B = Baseline Ramp 3-B



INTERMEDIATE DIAPHRAGM DIMENSION TABLE

| SPAN | BEAM | (1) | (2) |
|------|------|------------|------------|
| H | 19 | 6'-6 3/4" | 6'-10 1/4" |
| H | 20 | 6'-8 1/2" | 7'-2 1/4" |
| H | 21 | 6'-11 1/2" | 7'-9 1/4" |
| H | 22 | 6'-9 3/4" | 7'-7 1/4" |
| H | 23 | 6'-3 3/4" | 6'-8 1/4" |
| H | 24 | 6'-0 3/4" | 6'-3 3/4" |
| H | 25 | ----- | ----- |

INTERMEDIATE DIAPHRAGM DIMENSION TABLE

| SPAN | BEAM | (1) | (2) |
|------|------|------------|-----------|
| I | 18 | 7'-1 1/4" | 7'-1 3/4" |
| I | 19 | 7'-7 1/2" | 7'-4 3/4" |
| I | 20 | 5'-10 1/4" | 6'-4 3/4" |
| I | 21 | 7'-6 3/4" | 7'-3 3/4" |
| I | 22 | 5'-9 3/4" | 6'-4 3/4" |
| I | 23 | 7'-8 1/4" | 7'-3 3/4" |
| I | 24 | 7'-1 1/2" | 7'-0 3/4" |
| I | 25 | ----- | ----- |

INTERMEDIATE DIAPHRAGM DIMENSION TABLE

| SPAN | BEAM | (1) | (2) |
|------|------|------------|-------|
| J | 18 | 7'-0 1/4" | ----- |
| J | 19 | 7'-0 1/4" | ----- |
| J | 20 | 6'-11 1/4" | ----- |
| J | 21 | 7'-0 1/4" | ----- |
| J | 22 | 6'-11 1/4" | ----- |
| J | 23 | 7'-0 1/4" | ----- |
| J | 24 | 7'-0 1/4" | ----- |
| J | 25 | ----- | ----- |

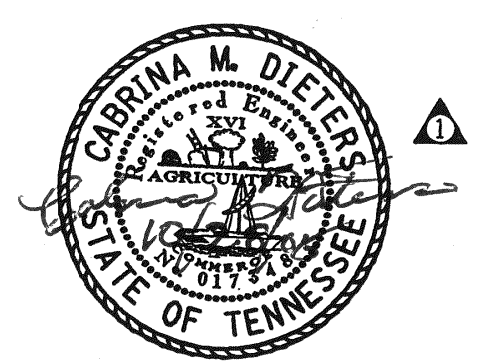
SLAB OVERHANG DIMENSION TABLE

| | 0' | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10' |
|--------------|------------|------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|
| SPAN H-LEFT | 4'-3 3/4" | | | | | | | | | | 4'-0 1/4" |
| SPAN I-LEFT | 4'-0 1/4" | | | | | | | | | | 3'-6" |
| SPAN J-LEFT | 3'-6" | | | | | | | | | | 3'-6" |
| SPAN H-RIGHT | 4'-11 1/4" | 4'-10 1/4" | 4'-9 1/4" | 4'-7 3/4" | 4'-6 1/4" | 4'-5 1/4" | 4'-4 1/4" | 4'-3 1/4" | 4'-1 1/4" | 4'-0 1/4" | 3'-8 3/4" |
| SPAN I-RIGHT | 3'-8 3/4" | | | | | | | | | | 3'-6" |
| SPAN J-RIGHT | 3'-6" | | | | | | | | | | 3'-6" |

PARTIAL FRAMING PLAN
 SPANS H-J
 Scale: 1" = 15'-0"

FRAMING PLAN DIMENSION TABLE

| DIMENSION ALONG CENTERLINE BENT NO. 7 | | | | | | | | | | | | |
|--|------------|------------|------------|------------|-----------|------------|------------|------------|------------|--|--|--|
| G18 TO G19 | G19 TO G20 | G20 TO G21 | G21 TO G22 | G22 TO G23 | G23 TO 3A | 3A TO G24 | G24 TO G25 | G25 TO EOS | | | | |
| 9'-8 1/4" | 9'-8 1/4" | 9'-8 1/4" | 9'-8 1/4" | 9'-8 1/4" | 2'-2" | 7'-6 1/4" | 9'-8 1/4" | 8'-3" | | | | |
| DIMENSION ALONG CENTERLINE BENT NO. 8B | | | | | | | | | | | | |
| EOS TO H18 | H18 TO H19 | H19 TO H20 | H20 TO H21 | H21 TO H22 | H22 TO 3A | 3A TO H23 | H23 TO H24 | H24 TO H25 | H25 TO EOS | | | |
| 6'-1 1/4" | 11'-0" | 12'-0" | 14'-0" | 4'-4 3/4" | 9'-7 3/4" | 12'-0" | 11'-0" | 5'-8 1/4" | | | | |
| DIMENSION ALONG CENTERLINE BENT NO. 9C | | | | | | | | | | | | |
| EOS TO I18 | I18 TO I19 | I19 TO I20 | I20 TO I21 | I21 TO 3A | 3A TO I22 | I22 TO I23 | I23 TO I24 | I24 TO I25 | I25 TO EOS | | | |
| 6'-1 1/4" | 11'-0" | 12'-0" | 8'-0" | 10'-4 3/4" | 1'-7 3/4" | 8'-0" | 12'-0" | 11'-0" | 5'-8 1/4" | | | |
| DIMENSION ALONG CENTERLINE BEARING ABUTMENT NO. 2C | | | | | | | | | | | | |
| EOS TO J18 | J18 TO J19 | J19 TO J20 | J20 TO J21 | J21 TO 3A | 3A TO J22 | J22 TO J23 | J23 TO J24 | J24 TO J25 | J25 TO EOS | | | |
| 5'-4 3/4" | 10'-8 3/4" | 10'-9 3/4" | 10'-7 3/4" | 9'-11 3/4" | 0'-9 3/4" | 10'-7 3/4" | 10'-9 3/4" | 10'-8 3/4" | 5'-4 3/4" | | | |



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
 BRIDGE NO. 21
 SUPERSTRUCTURE DETAILS (14)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN Rwy.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

DESIGNED BY: BBW
 DRAWN BY: LBL
 SUPERVISED BY: FCB
 CHECKED BY: AAM
 DATE: 9-14
 DATE: 9-14
 DATE: 9-14
 DATE: 9-04

CORRECT
 ENGINEER OF STRUCTURES
 APPROVED
 DIRECTOR OF HIGHWAYS
 M-472-146

FRAMING PLAN DIMENSION TABLE

| SPAN | BEAM | (A) | (B) | (C) | (D) | (E) | (F) | (G) | (H) | (I) | (J) | (K) |
|------|------|-----------|--------------|-----------|--------------|--------------|------------|-------------|-------------|-------------|-------------|-------------|
| H | 1 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'41.53" | 54'2'41.53" | ----- | ----- | ----- | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 2 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'41.01" | 54'2'41.01" | 26'-6" | 32'-5 1/4" | 38'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 3 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'40.48" | 54'2'40.48" | 26'-6" | 32'-5 1/4" | 38'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 4 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'39.96" | 54'2'39.96" | 26'-6" | 32'-5 1/4" | 38'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 5 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'39.43" | 54'2'39.43" | 26'-6" | 32'-5 1/4" | 38'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 6 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'38.91" | 54'2'38.91" | 26'-6" | 32'-5 1/4" | 38'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 7 | 1'-2 1/4" | 97'-5 3/4" | 1'-2 1/4" | 54'2'38.38" | 54'2'38.38" | 26'-6" | 32'-5 1/4" | 38'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" | 32'-5 1/4" |
| H | 8 | 1'-2 1/4" | 97'-8 1/4" | 1'-2 1/4" | 53'52'51.22" | 53'52'51.22" | 26'-8 1/4" | 32'-5 1/4" | 38'-5 1/4" | 32'-6 1/4" | 32'-6 1/4" | 32'-6 1/4" |
| H | 9 | 1'-2 1/4" | 97'-7 1/4" | 1'-2 1/4" | 53'55'48.32" | 53'55'48.32" | 26'-9" | 32'-6 1/4" | 38'-3 1/4" | 32'-6 1/4" | 32'-6 1/4" | 32'-6 1/4" |
| H | 10 | 1'-3 1/4" | 100'-3 3/4" | 1'-3 1/4" | 51'53'31.47" | 51'53'31.47" | 29'-7 1/4" | 32'-6 1/4" | 38'-1 3/4" | 33'-5 1/4" | 33'-5 1/4" | 33'-5 1/4" |
| H | 11 | 1'-3 1/4" | 103'-0 1/4" | 1'-3 1/4" | 49'57'42.22" | 49'57'42.22" | 30'-4 1/4" | 33'-5 1/4" | 39'-3" | 34'-4 1/4" | 34'-4 1/4" | 34'-4 1/4" |
| H | 12 | 1'-3 1/4" | 103'-3 1/4" | 1'-3 1/4" | 49'49'26.23" | 49'49'26.23" | 28'-0 1/4" | 34'-4 1/4" | 40'-10 1/4" | 34'-5 1/4" | 34'-5 1/4" | 34'-5 1/4" |
| H | 13 | 1'-3 1/4" | 103'-5 3/4" | 1'-3 1/4" | 49'41'12.25" | 49'41'12.25" | 28'-0 1/4" | 34'-5 1/4" | 40'-11 1/4" | 34'-5 1/4" | 34'-5 1/4" | 34'-5 1/4" |
| H | 14 | 1'-3 1/4" | 103'-8 1/4" | 1'-3 1/4" | 49'33'00.27" | 49'33'00.27" | 28'-1 1/4" | 34'-5 1/4" | 41'-1" | 34'-6 1/4" | 34'-6 1/4" | 34'-6 1/4" |
| H | 15 | 1'-3 1/4" | 103'-10 1/4" | 1'-3 1/4" | 49'24'50.27" | 49'24'50.27" | 28'-2" | 34'-6 1/4" | 41'-2 1/4" | 34'-7 1/4" | 34'-7 1/4" | 34'-7 1/4" |
| H | 16 | 1'-3 1/4" | 104'-1 1/4" | 1'-3 1/4" | 49'16'42.27" | 49'16'42.27" | 28'-2 1/4" | 34'-7 1/4" | 41'-3 1/4" | 34'-8 1/4" | 34'-8 1/4" | 34'-8 1/4" |
| H | 17 | 1'-3 1/4" | 104'-3 1/4" | 1'-3 1/4" | 49'08'36.25" | 49'08'36.25" | 28'-3 1/4" | 34'-8 1/4" | 41'-4 1/4" | 34'-9 1/4" | 34'-9 1/4" | 34'-9 1/4" |
| H | 18 | 1'-3 1/4" | 104'-6 1/4" | 1'-3 1/4" | 49'0'32.19" | 49'0'32.19" | 28'-3 1/4" | 34'-9 1/4" | 41'-5 1/4" | ----- | ----- | ----- |
| H | 19 | 1'-6 1/4" | 120'-10 1/4" | 1'-3 1/4" | 40'50'48.65" | 40'50'48.65" | ----- | ----- | ----- | 40'-3 1/4" | 40'-3 1/4" | 40'-3 1/4" |
| H | 20 | 1'-6 1/4" | 121'-10 1/4" | 1'-3 1/4" | 40'27'24.97" | 40'27'24.97" | 32'-1 1/4" | 40'-3 1/4" | 48'-7 1/4" | 40'-7 1/4" | 40'-7 1/4" | 40'-7 1/4" |
| H | 21 | 1'-6 1/4" | 123'-6 1/4" | 1'-3 1/4" | 39'46'53.18" | 39'46'53.18" | 33'-2 1/4" | 40'-7 1/4" | 49'-8 1/4" | 41'-2 1/4" | 41'-2 1/4" | 41'-2 1/4" |
| H | 22 | 1'-7 1/4" | 126'-10 1/4" | 1'-4 1/4" | 38'34'02.52" | 38'34'02.52" | 33'-8 1/4" | 41'-2 1/4" | 51'-11 1/4" | 42'-3 1/4" | 42'-3 1/4" | 42'-3 1/4" |
| H | 23 | 1'-7 1/4" | 130'-1 1/4" | 1'-4 1/4" | 37'24'52.40" | 37'24'52.40" | 34'-7 1/4" | 42'-3 1/4" | 53'-2 1/4" | 43'-4 1/4" | 43'-4 1/4" | 43'-4 1/4" |
| H | 24 | 1'-8" | 131'-11 1/4" | 1'-4 1/4" | 36'49'19.62" | 36'49'19.62" | 35'-7 1/4" | 43'-4 1/4" | 52'-10 1/4" | 43'-11 1/4" | 43'-11 1/4" | 43'-11 1/4" |
| H | 25 | 1'-8 1/4" | 132'-11 1/4" | 1'-4 1/4" | 36'29'40.65" | 36'29'40.65" | 36'-2 1/4" | 43'-11 1/4" | 52'-9 1/4" | ----- | ----- | ----- |

FRAMING PLAN DIMENSION TABLE

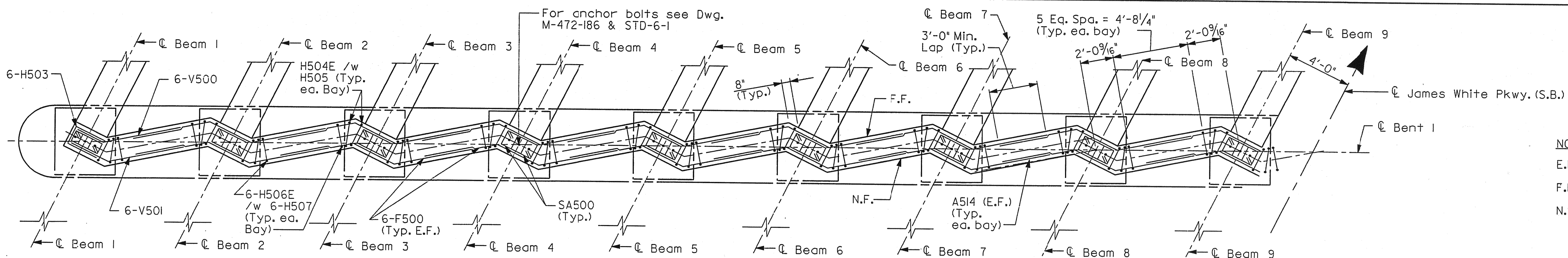
| SPAN | BEAM | (A) | (B) | (C) | (D) | (E) | (F) | (G) | (H) | (I) | (J) | (K) |
|------|------|-----------|--------------|-----------|--------------|--------------|-------------|-------------|-------------|-------------|-------------|-------------|
| I | 1 | 1'-2 1/4" | 119'-2 1/4" | 1'-2 1/4" | 54'02'27.79" | 54'02'27.79" | ----- | ----- | ----- | 42'-6 1/4" | 40'-0 1/4" | 36'-7" |
| I | 2 | 1'-2 1/4" | 119'-2 1/4" | 1'-2 1/4" | 54'02'25.95" | 54'02'25.95" | 36'-7" | 40'-0 1/4" | 42'-6 1/4" | 42'-6 1/4" | 40'-0 1/4" | 36'-7" |
| I | 3 | 1'-2 1/4" | 119'-2 1/4" | 1'-2 1/4" | 54'02'17.88" | 54'02'17.88" | 36'-7" | 40'-0 1/4" | 42'-6 1/4" | 42'-6 1/4" | 40'-0 1/4" | 36'-7" |
| I | 4 | 1'-2 1/4" | 119'-2 1/4" | 1'-2 1/4" | 54'02'02.21" | 54'02'02.21" | 36'-7" | 40'-0 1/4" | 42'-7" | 42'-6 1/4" | 40'-0 1/4" | 36'-7" |
| I | 5 | 1'-2 1/4" | 119'-2 1/4" | 1'-2 1/4" | 54'01'38.91" | 54'01'38.91" | 36'-7" | 40'-0 1/4" | 42'-7 1/4" | 42'-7" | 40'-0 1/4" | 36'-7 1/4" |
| I | 6 | 1'-2 1/4" | 119'-3 1/4" | 1'-2 1/4" | 54'01'08.11" | 54'01'08.11" | 36'-7 1/4" | 40'-0 1/4" | 42'-7 1/4" | 42'-7 1/4" | 40'-0 1/4" | 36'-7 1/4" |
| I | 7 | 1'-2 1/4" | 119'-3 1/4" | 1'-2 1/4" | 54'00'29.58" | 54'00'29.58" | 36'-7 1/4" | 40'-0 1/4" | 42'-7 1/4" | 42'-7 1/4" | 40'-1" | 36'-7 1/4" |
| I | 8 | 1'-2 1/4" | 119'-3 1/4" | 1'-2 1/4" | 53'59'43.45" | 53'59'43.45" | 36'-7 1/4" | 40'-1" | 42'-7 1/4" | 42'-7 1/4" | 40'-1 1/4" | 36'-7 1/4" |
| I | 9 | 1'-2 1/4" | 119'-3 1/4" | 1'-2 1/4" | 53'58'52.17" | 53'58'52.17" | 36'-10 1/4" | 40'-1 1/4" | 42'-4 1/4" | ----- | ----- | ----- |
| I | 10 | 1'-4 1/4" | 131'-6 1/4" | 1'-4 1/4" | 47'12'00.56" | 47'12'00.56" | ----- | ----- | ----- | 47'-1 1/4" | 44'-3" | 40'-1 1/4" |
| I | 11 | 1'-4 1/4" | 131'-10 1/4" | 1'-4 1/4" | 47'01'48.82" | 47'01'48.82" | 40'-2 1/4" | 44'-3" | 47'-4 1/4" | 47'-3 1/4" | 44'-4 1/4" | 40'-2 1/4" |
| I | 12 | 1'-4 1/4" | 132'-3" | 1'-4 1/4" | 46'51'39.74" | 46'51'39.74" | 40'-4 1/4" | 44'-4 1/4" | 47'-6 1/4" | 47'-5 1/4" | 44'-6" | 40'-3 1/4" |
| I | 13 | 1'-4 1/4" | 132'-7 1/4" | 1'-4 1/4" | 46'41'29.99" | 46'41'29.99" | 40'-5 1/4" | 44'-6" | 47'-8" | 47'-6 1/4" | 44'-7 1/4" | 40'-5 1/4" |
| I | 14 | 1'-4 1/4" | 132'-11 1/4" | 1'-4 1/4" | 46'31'47.72" | 46'31'47.72" | 40'-6 1/4" | 44'-7 1/4" | 47'-9 1/4" | 47'-8 1/4" | 44'-8 1/4" | 40'-6 1/4" |
| I | 15 | 1'-4 1/4" | 133'-3 1/4" | 1'-4 1/4" | 46'22'38.41" | 46'22'38.41" | 40'-8 1/4" | 44'-8 1/4" | 47'-10 1/4" | 47'-9 1/4" | 44'-10 1/4" | 40'-7 1/4" |
| I | 16 | 1'-4 1/4" | 133'-7 1/4" | 1'-4 1/4" | 46'14'01.59" | 46'14'01.59" | 40'-9 1/4" | 44'-10 1/4" | 47'-11 1/4" | 47'-11 1/4" | 44'-11 1/4" | 40'-8 1/4" |
| I | 17 | 1'-4 1/4" | 133'-11 1/4" | 1'-4 1/4" | 46'05'57.43" | 46'05'57.43" | 40'-10 1/4" | 44'-11 1/4" | 48'-1" | ----- | ----- | ----- |
| I | 18 | 1'-3 1/4" | 147'-9 1/4" | 1'-3 1/4" | 40'55'25.49" | 40'55'25.49" | ----- | ----- | ----- | 53'-2 1/4" | 49'-7 1/4" | 44'-11 1/4" |
| I | 19 | 1'-3 1/4" | 147'-7 1/4" | 1'-3 1/4" | 40'59'15.00" | 40'59'15.00" | 44'-11" | 49'-7 1/4" | 53'-1 1/4" | 53'-5 1/4" | 49'-6 1/4" | 44'-7 1/4" |
| I | 20 | 1'-3 1/4" | 146'-8 1/4" | 1'-3 1/4" | 41'17'34.75" | 41'17'34.75" | 44'-4 1/4" | 49'-6 1/4" | 52'-9 1/4" | 52'-0 1/4" | 49'-3 1/4" | 45'-5" |
| I | 21 | 1'-3 1/4" | 148'-8 1/4" | 1'-3 1/4" | 40'38'16.99" | 40'38'16.99" | 46'-0 1/4" | 49'-3 1/4" | 53'-4 1/4" | 53'-9 1/4" | 49'-10 1/4" | 44'-11 1/4" |
| I | 22 | 1'-3 1/4" | 147'-9 1/4" | 1'-3 1/4" | 40'56'21.31" | 40'56'21.31" | 44'-8 1/4" | 49'-11" | 53'-2 1/4" | 52'-5 1/4" | 49'-7 1/4" | 45'-8 1/4" |
| I | 23 | 1'-3 1/4" | 149'-8 1/4" | 1'-3 1/4" | 40'17'36.61" | 40'17'36.61" | 46'-4 1/4" | 49'-7 1/4" | 53'-9 1/4" | 54'-1 1/4" | 50'-3 1/4" | 45'-3 1/4" |
| I | 24 | 1'-3 1/4" | 148'-10" | 1'-3 1/4" | 40'35'25.74" | 40'35'25.74" | 45'-0 1/4" | 50'-3 1/4" | 53'-6 1/4" | 53'-6 1/4" | 49'-11 1/4" | 45'-3 1/4" |
| I | 25 | 1'-3 1/4" | 148'-7 1/4" | 1'-3 1/4" | 40'39'12.16" | 40'39'12.16" | 45'-2 1/4" | 49'-11 1/4" | 53'-5 1/4" | ----- | ----- | ----- |

FRAMING PLAN DIMENSION TABLE

| SPAN | BEAM | (A) | (B) | (C) | (D) | (E) | (F) | (G) | (H) | (I) | (J) | (K) |
|------|------|-----------|-------------|------------|--------------|--------------|-------------|-------|-------------|-------------|-------|-------------|
| J | 1 | 1'-2 1/4" | 39'-8 1/4" | 3'-11 1/4" | 53'53'44.70" | 53'53'44.70" | ----- | ----- | ----- | 22'-10" | 0" | 16'-10 1/4" |
| J | 2 | 1'-2 1/4" | 39'-8 1/4" | 3'-11 1/4" | 53'50'43.29" | 53'50'43.29" | 16'-9 1/4" | 0" | 22'-10 1/4" | 22'-10 1/4" | 0" | 16'-10 1/4" |
| J | 3 | 1'-2 1/4" | 39'-8 1/4" | 3'-11 1/4" | 53'47'41.35" | 53'47'41.35" | 16'-9 1/4" | 0" | 22'-11 1/4" | 22'-10 1/4" | 0" | 16'-10 1/4" |
| J | 4 | 1'-2 1/4" | 39'-9 1/4" | 3'-11 1/4" | 53'44'38.87" | 53'44'38.87" | 16'-9 1/4" | 0" | 22'-11 1/4" | 22'-10 1/4" | 0" | 16'-10 1/4" |
| J | 5 | 1'-2 1/4" | 39'-9 1/4" | 3'-11 1/4" | 53'41'35.85" | 53'41'35.85" | 16'-10" | 0" | 22'-11 1/4" | 22'-10 1/4" | 0" | 16'-10 1/4" |
| J | 6 | 1'-2 1/4" | 39'-9 1/4" | 3'-11 1/4" | 53'38'32.29" | 53'38'32.29" | 16'-10 1/4" | 0" | 22'-11 1/4" | 22'-10 1/4" | 0" | 16'-10 1/4" |
| J | 7 | 1'-2 1/4" | 39'-10 1/4" | 3'-11 1/4" | 53'35'28.17" | 53'35'28.17" | 16'-10 1/4" | 0" | 23'-0" | 22'-11 1/4" | 0" | 16'-11" |
| J | 8 | 1'-2 1/4" | 39'-10 1/4" | 3'-11 1/4" | 53'32'23.51" | 53'32'23.51" | 16'-10 1/4" | 0" | 23'-0 1/4" | 22'-11 1/4" | 0" | 16'-11 1/4" |
| J | 9 | 1'-2 1/4" | 39'-10 1/4" | 3'-11 1/4" | 53'29'26.63" | 53'29'26.63" | 17'-1 1/4" | 0" | 22'-9 1/4" | ----- | ----- | ----- |
| J | 10 | 1'-4 1/4" | 45'-0 1/4" | 4'-6 1/4" | 45'15'16.38" | 45'15'16.38" | ----- | ----- | ----- | 24'-5 1/4" | 0" | 20'-6 1/4" |
| J | 11 | 1'-4 1/4" | 45'-0 1/4" | 4'-6 1/4" | 45'11'40.30" | 45'11'40.30" | 16'-11 1/4" | 0" | 28'-1 1/4" | 24'-6" | 0" | 20'-6 1/4" |
| J | 12 | 1'-4 1/4" | 45'-1 1/4" | 4'-7" | 45'09'48.68" | 45'09'48.68" | 16'-11 1/4" | 0" | 28'-2" | 24'-6 1/4" | 0" | 20'-7 1/4" |
| J | 13 | 1'-4 1/4" | 45'-1 1/4" | 4'-7" | 45'09'31.91" | 45'09'31.91" | 16'-11 1/4" | 0" | 28'-2" | 24'-6 1/4" | 0" | 20'-7 1/4" |
| J | 14 | 1'-4 1/4" | 45'-1 1/4" | 4'-7" | 45'09'31.91" | 45'09'31.91" | 16'-11 1/4" | 0" | 28'-1 1/4" | 24'-6 1/4" | 0" | 20'-7 1/4" |
| J | 15 | 1'-4 1/4" | 45'-1 1/4" | 4'-7" | 45'09'31.91" | 45'09'31.91" | 16'-11 1/4" | 0" | 28'-1 1/4" | 24'-6 1/4" | 0" | 20'-7 1/4" |
| J | 16 | 1'-4 1/4" | 45'-1 1/4" | 4'-7" | 45'09'31.91" | 45'09'31.91" | 16'-11 1/4" | 0" | 28'-1 1/4" | 24'-6 1/4" | 0" | 20'-7 1/4" |
| J | 17 | 1'-4 1/4" | 45'-1 1/4" | 4'-7" | 45'09'31.91" | 45'09'31.91" | 17'-0 1/4" | 0" | 28'-1 1/4" | ----- | ----- | ----- |
| J | 18 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | ----- | ----- | ----- | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 19 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-6 1/4" | 0" | 28'-9 1/4" | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 20 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-5 1/4" | 0" | 28'-9 1/4" | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 21 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-7 1/4" | 0" | 28'-8 1/4" | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 22 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-5 1/4" | 0" | 28'-9 1/4" | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 23 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-7 1/4" | 0" | 28'-8 1/4" | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 24 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-5 1/4" | 0" | 28'-9 1/4" | 28'-7 1/4" | 0" | 20'-7 1/4" |
| J | 25 | 1'-3 1/4" | 49'-3 1/4" | 4'-11 1/4" | 40'43'48.58" | 40'43'48.58" | 20'-6 1/4" | 0" | 28'-9 1/4" | ----- | ----- | ----- |

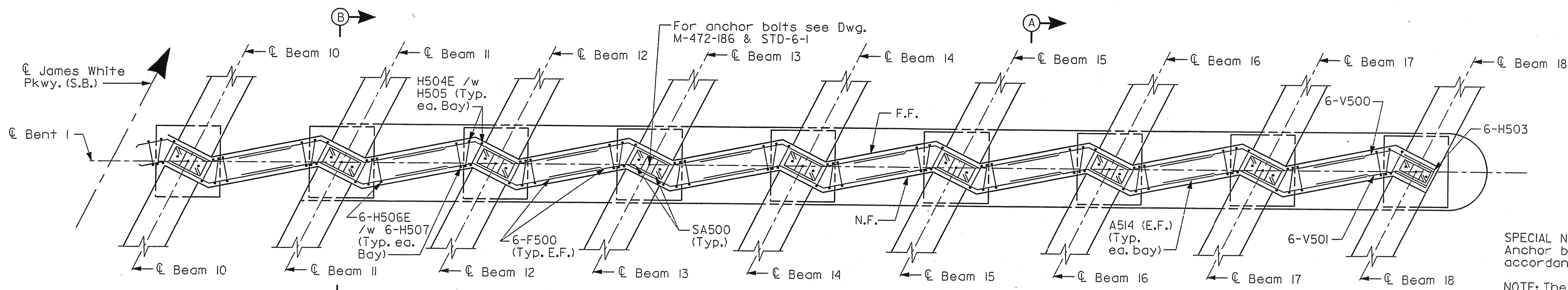
| PROJECT NO. | YEAR | SHEET NO. |
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| IM/NH-40-8(135) | 2005 | |

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| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 6-05-06 | CMD | GENERAL REVISIONS |
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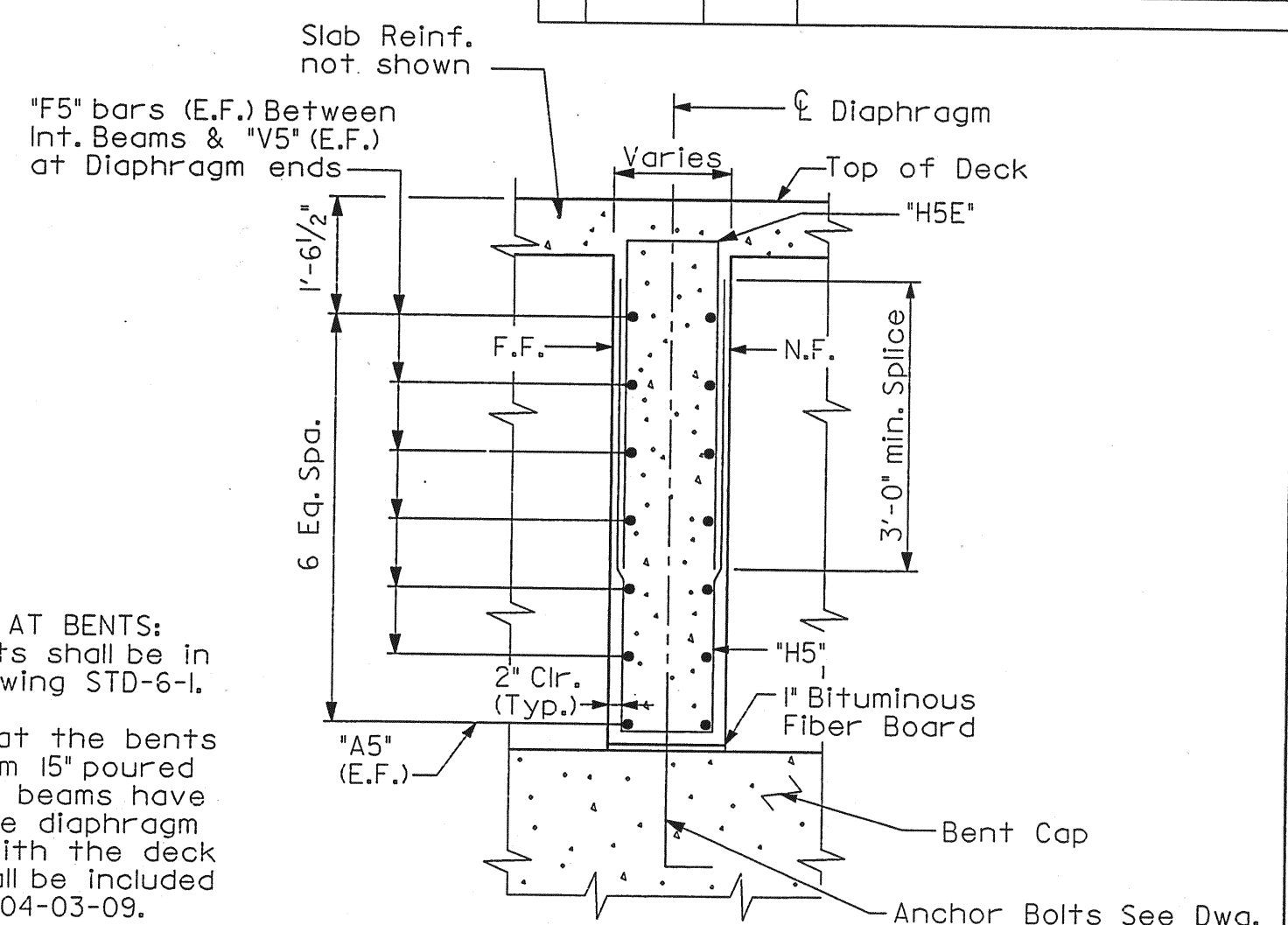
PART PLAN - BENT 1 DIAPHRAGM
Scale: 1/4" = 1'-0"

NOTE:
E.F. - Denotes Each Face
F.F. - Denotes Far Face
N.F. - Denotes Near Face

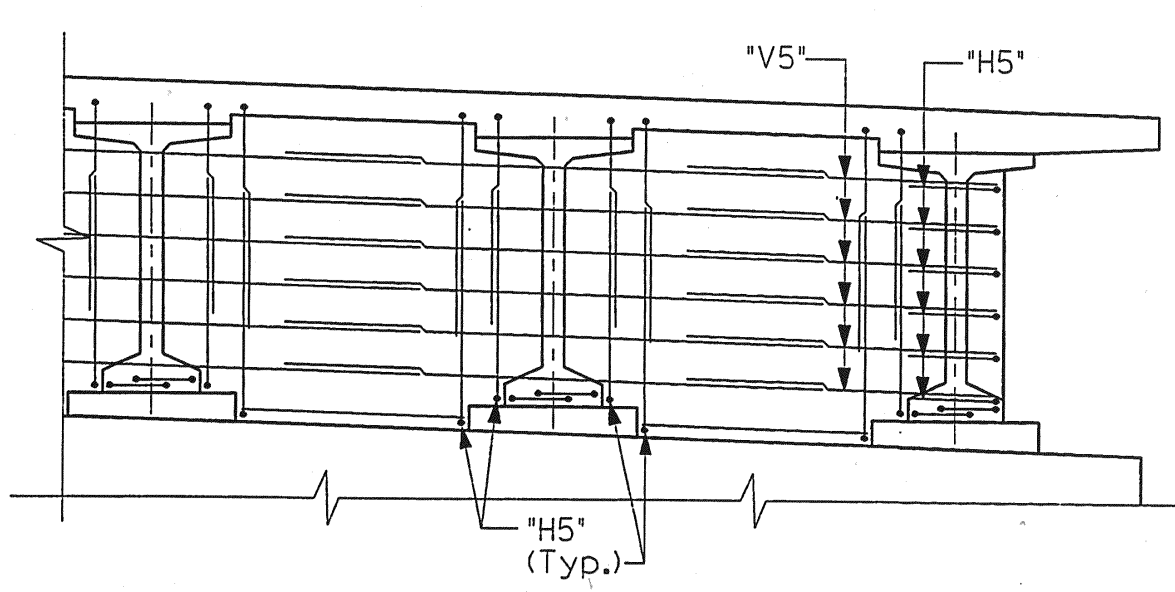
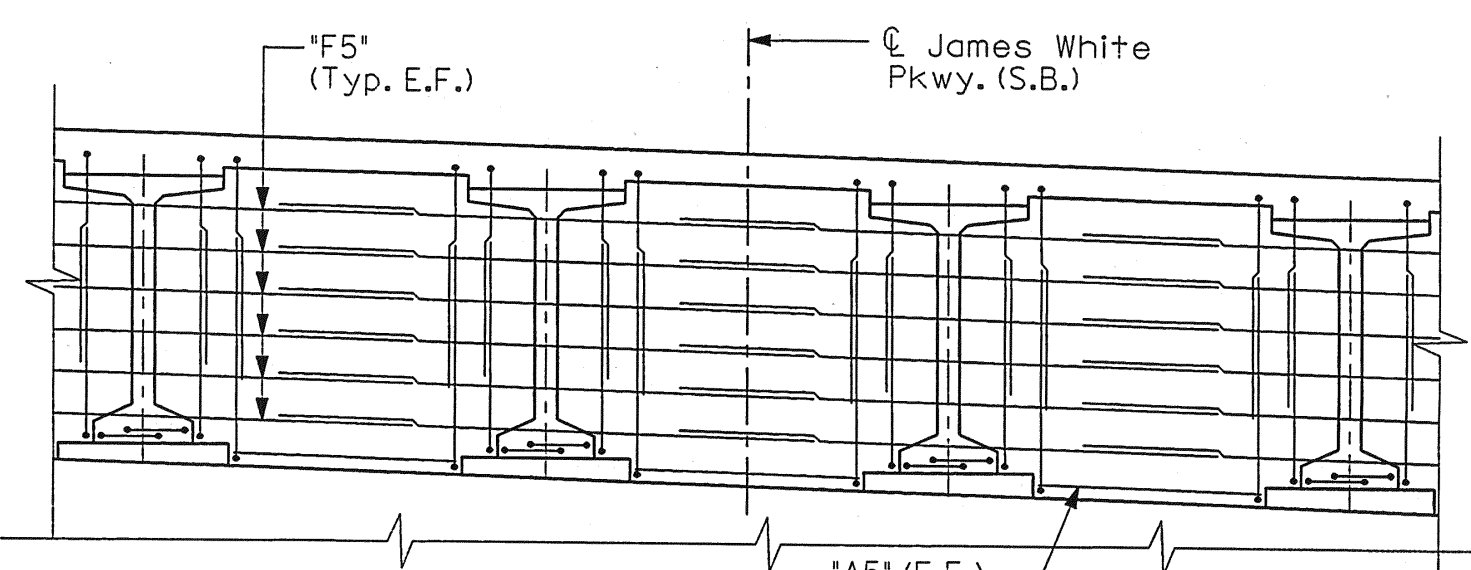
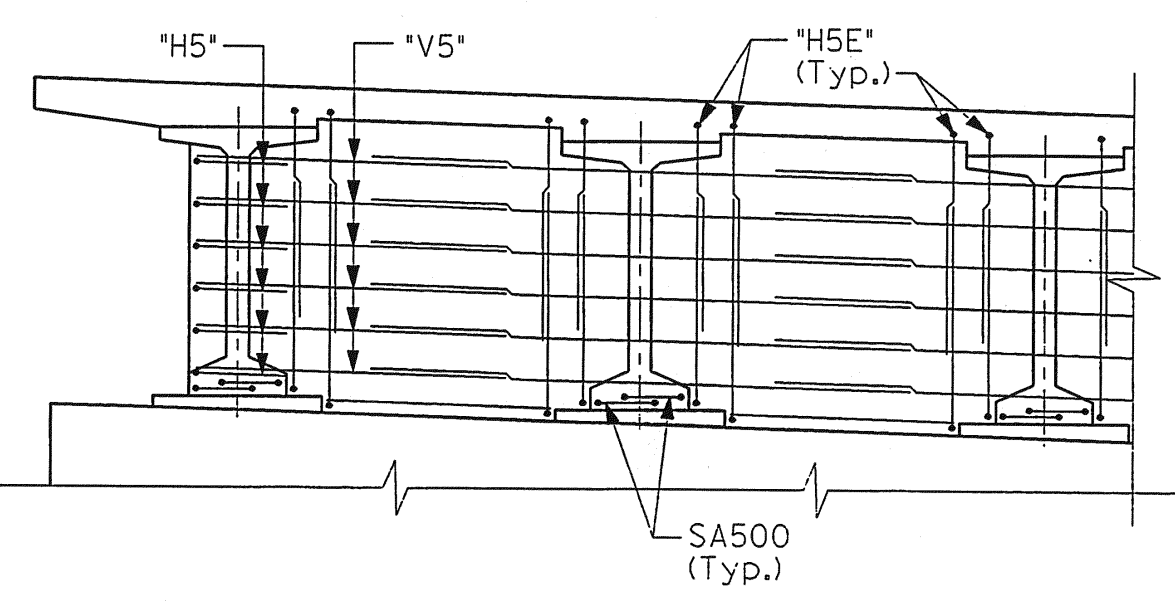


PART PLAN - BENT 1 DIAPHRAGM
Scale: 1/4" = 1'-0"

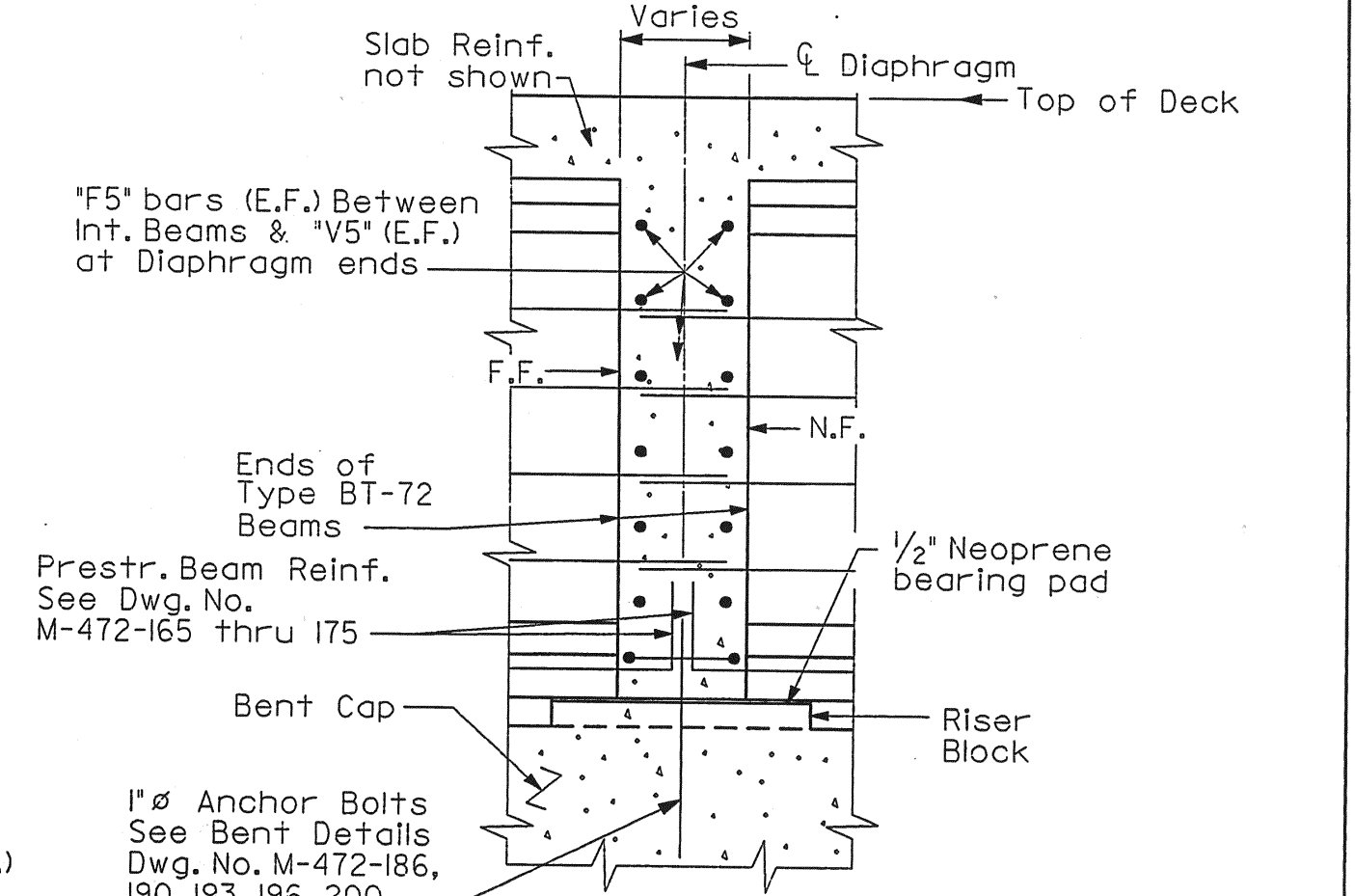
SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS:
Anchor bolt assemblies at bents shall be in accordance with Standard Drawing STD-6-1.
NOTE: The support diaphragms at the bents shall be formed and the bottom 15' poured as soon as possible after the beams have been set. The remainder of the diaphragm shall be poured concurrently with the deck slab. All diaphragm concrete shall be included in the quantity for item No. 604-03-09.



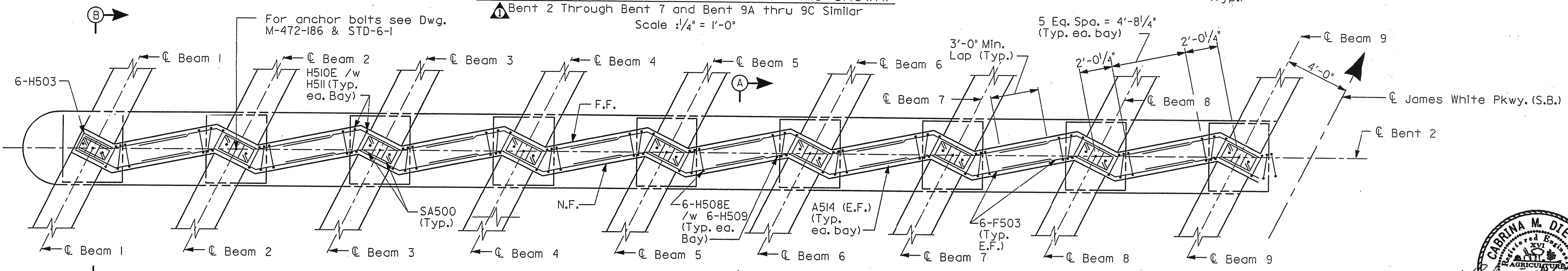
SECTION A-A
Bent 1 as Shown
(Bent 2 through Bent 9C Similar)
Not To Scale



PART ELEVATION - BENT 1 DIAPHRAGM (As Shown)



SECTION B-B
Bent 1 as Shown
(Bent 2 through Bent 9C Similar)
Not To Scale



PART PLAN - BENT 2 DIAPHRAGM
Scale: 1/4" = 1'-0"

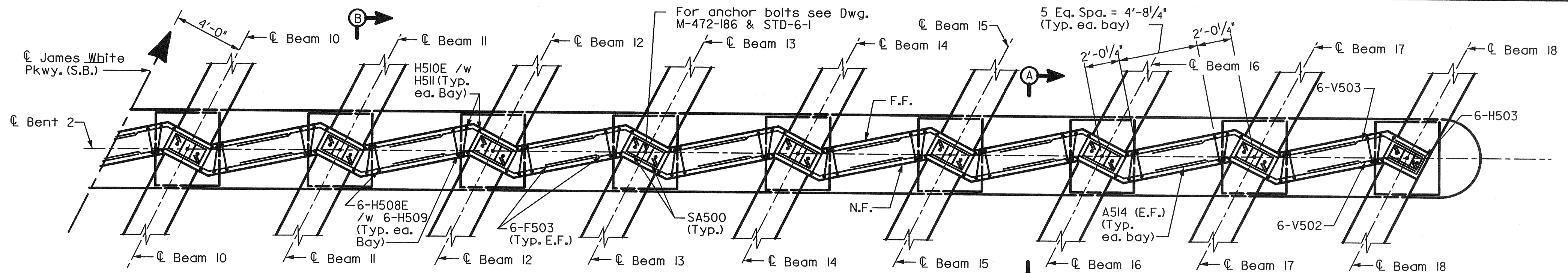


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (17)
JAMES WHITE PKWY. OVER
NORFOLK SOUTHERN R.W.,
WILLOW AVE. AND JACKSON AVE.
STA. 81+37.61 @ JWP (S.B.)
KNOX COUNTY
2005

DESIGNED BY AAM DATE 9-04
DRAWN BY VD DATE 9-04
SUPERVISED BY FCB DATE 9-04
CHECKED BY FCB, VD, CED DATE 1-24

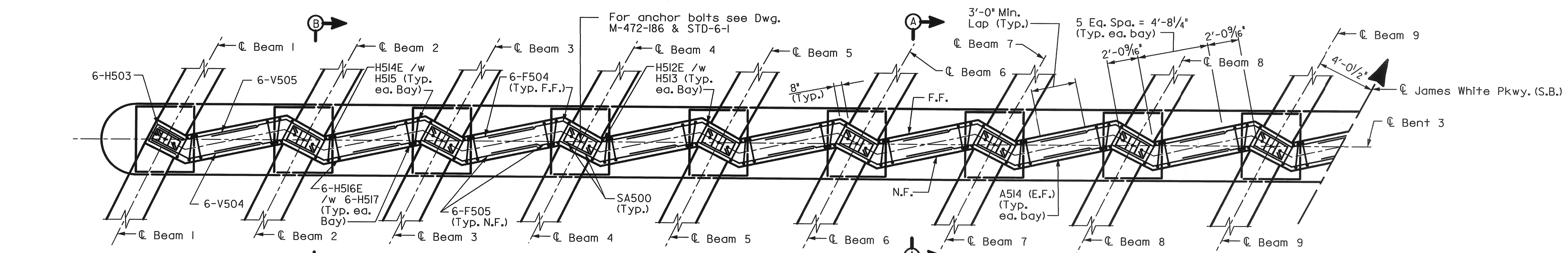
CORRECT ENGINEER OF STRUCTURES
APPROVED DIRECTOR OF HIGHWAYS

SCANNED



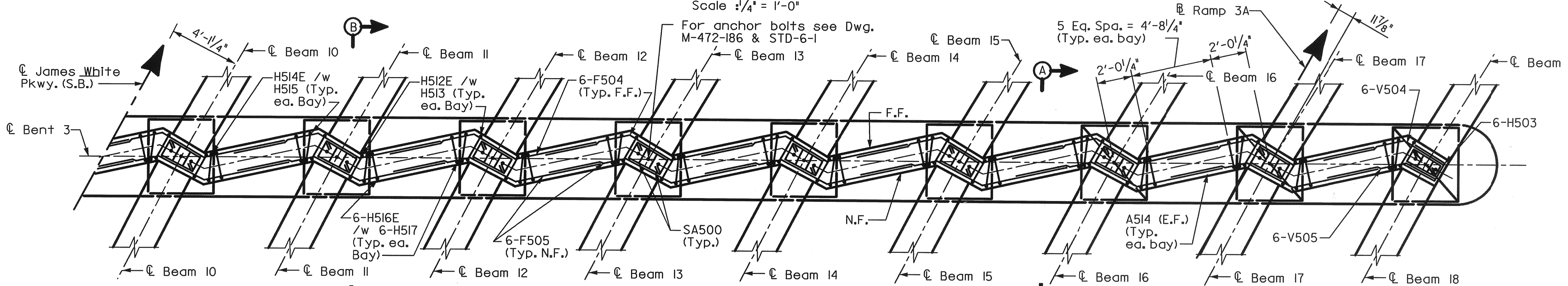
PART PLAN - BENT 2 DIAPHRAGM

Scale: 1/4" = 1'-0"



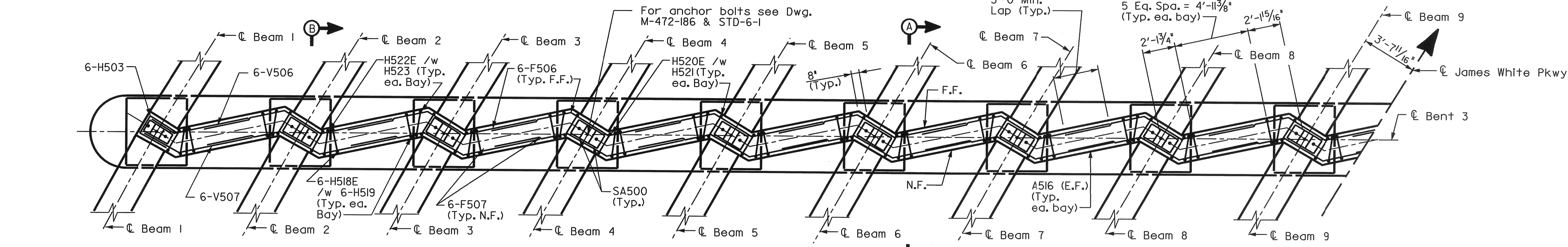
PART PLAN - BENT 3 DIAPHRAGM

Scale: 1/4" = 1'-0"



PART PLAN - BENT 3 DIAPHRAGM

Scale: 1/4" = 1'-0"



PART PLAN - BENT 4 DIAPHRAGM

Scale: 1/4" = 1'-0"

CONSTR. NO. 47003-3154-44

| PROJECT NO. | YEAR | SHEET NO. |
|-----------------|------|-----------|
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
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NOTE:
 E.F. - Denotes Each Face
 F.F. - Denotes Far Face
 N.F. - Denotes Near Face
 For Notes and Sections A-A and B-B See Dwg. No. M-472-149



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (18)
 JAMES WHITE PKWY. OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.,
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

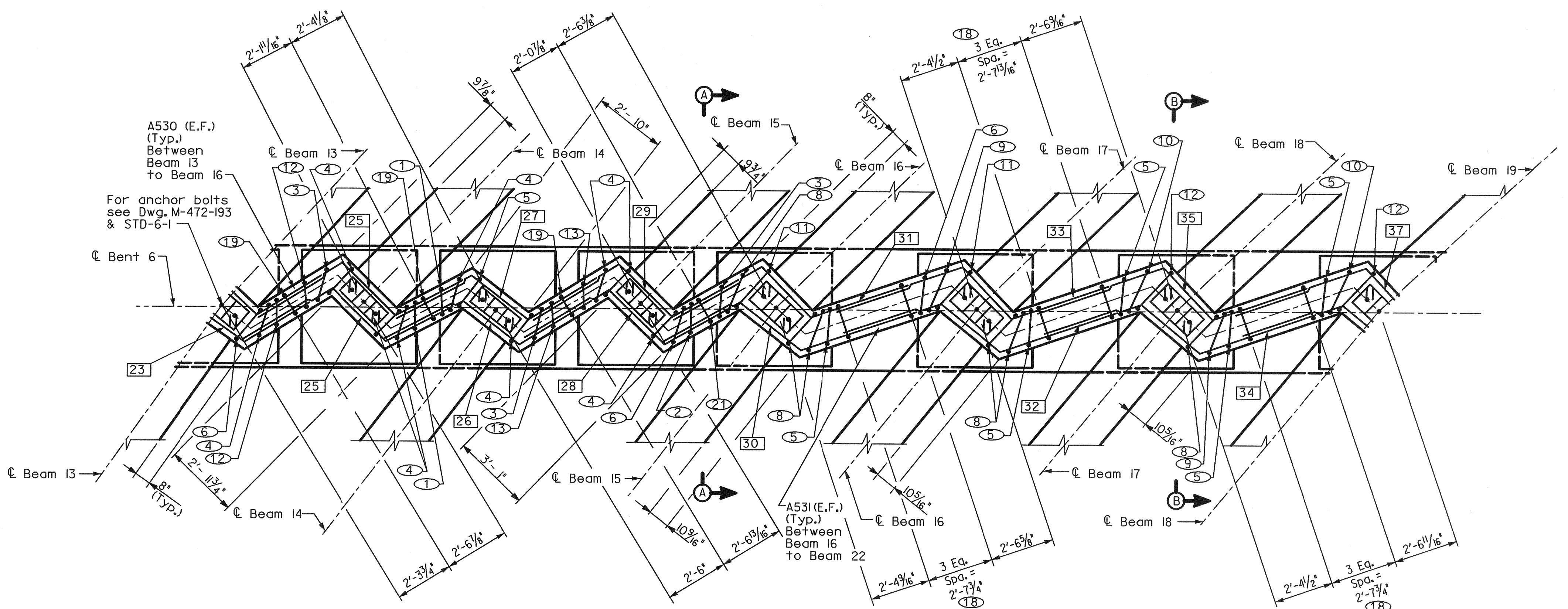
DESIGNED BY: AAM DATE: 1-04
 DRAWN BY: VD DATE: 1-04
 SUPERVISED BY: FCB DATE: 1-04
 CHECKED BY: FCB, VD, CED DATE: 1-24

CORRECT: _____ ENGINEER OF STRUCTURES
 APPROVED: _____ DIRECTOR OF HIGHWAYS
 M-472-150

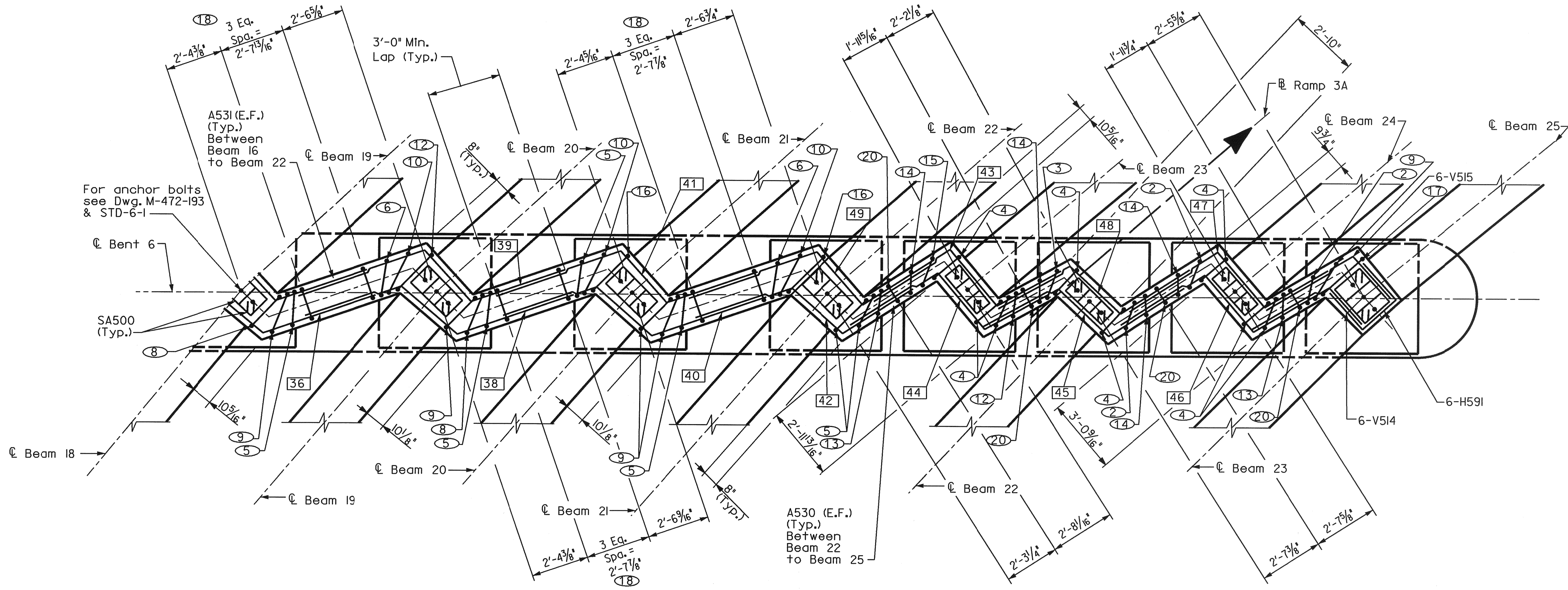
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| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

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NOTE:
 E.F. - Denotes Each Face
 F.F. - Denotes Far Face
 N.F. - Denotes Near Face
 For Notes and Sections A-A and B-B
 See Dwg. No. M-472-149



PART PLAN - BENT 6 DIAPHRAGM
 Scale 1/4" = 1'-0"



PART PLAN - BENT 6 DIAPHRAGM
 Scale 1/4" = 1'-0"



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (22)
 JAMES WHITE PKWY. OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

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|---------------|--------------|------|------|
| DESIGNED BY | AAMM | DATE | 1-04 |
| DRAWN BY | VD | DATE | 1-04 |
| SUPERVISED BY | FGB | DATE | 1-04 |
| CHECKED BY | FGB, VD, CFD | DATE | 1-24 |

CORRECT _____ ENGINEER OF STRUCTURES
 APPROVED _____ DIRECTOR OF HIGHWAYS
 M-472-154

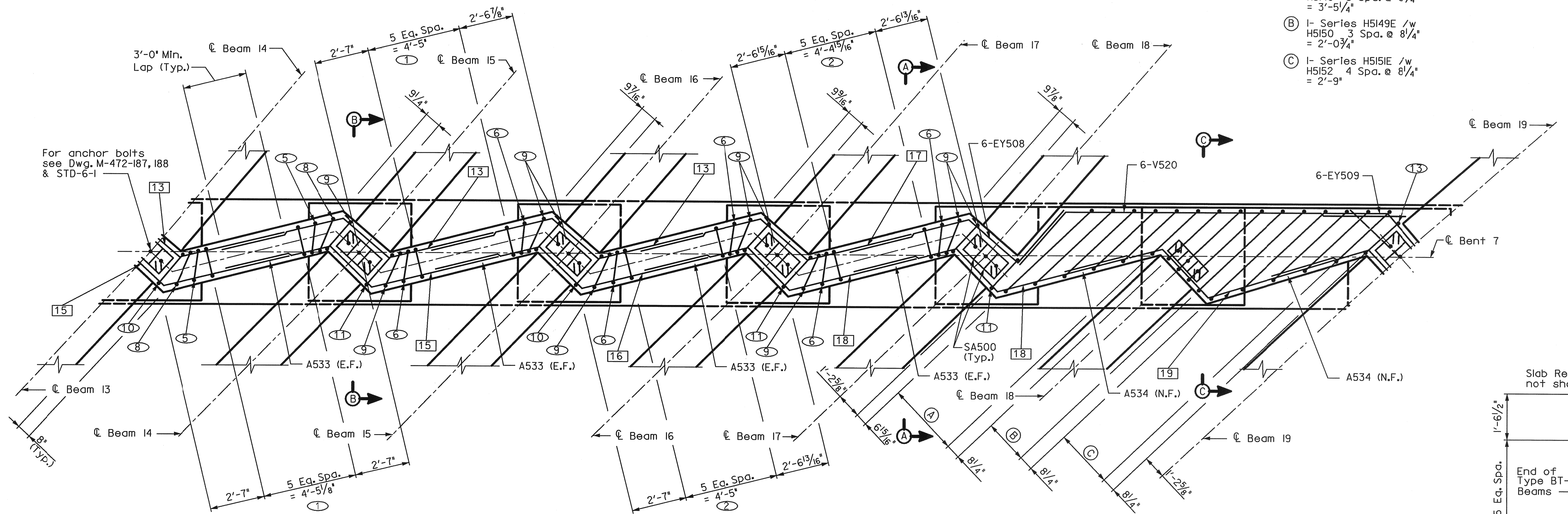
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| CONSTR. NO. 47003-3154-44 | | |
| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

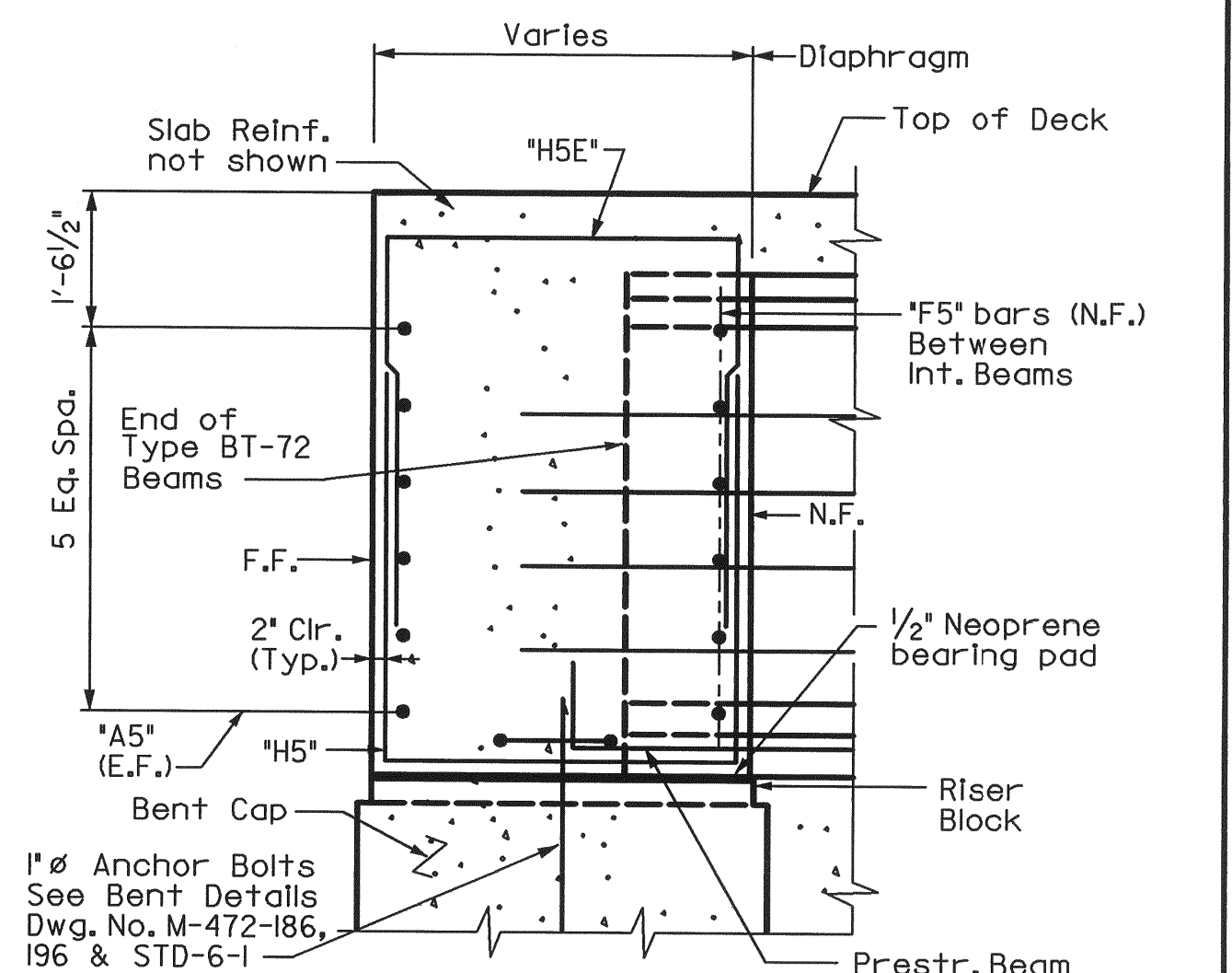
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- (A) I- Series H5147E /w H5148 5 Spa. @ 8 1/4" = 3'-5 1/4"
- (B) I- Series H5149E /w H5150 3 Spa. @ 8 1/4" = 2'-0 3/4"
- (C) I- Series H5151E /w H5152 4 Spa. @ 8 1/4" = 2'-9"

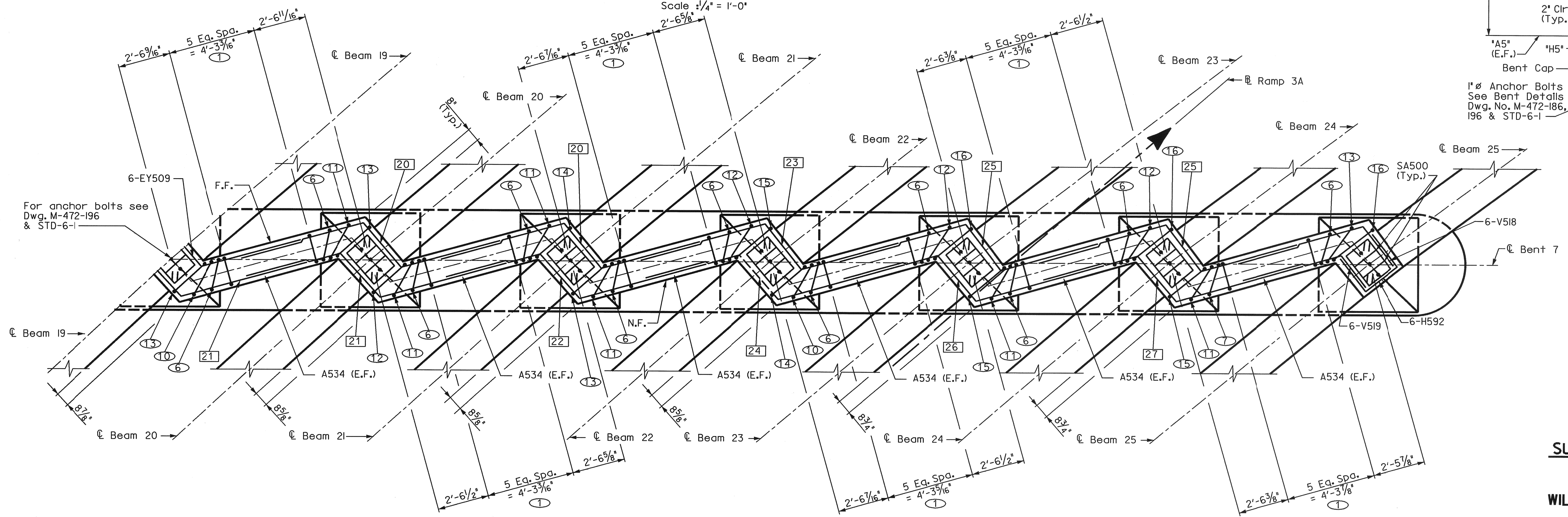
NOTE:
 E.F. - Denotes Each Face
 F.F. - Denotes Far Face
 N.F. - Denotes Near Face
 For Notes and Sections A-A and B-B See Dwg. No. M-472-149



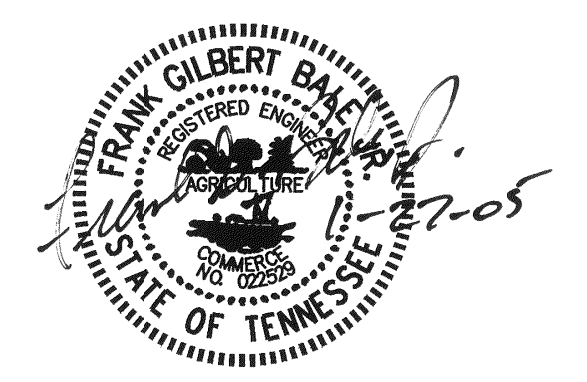
PART PLAN - BENT 7 DIAPHRAGM
 Scale: 1/4" = 1'-0"



SECTION C-C
 Bent 7 as Shown (Bent 8A Similar)
 Not To Scale



PART PLAN - BENT 7 DIAPHRAGM
 Scale: 1/4" = 1'-0"



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (24)
 JAMES WHITE PKWY. OVER
 NORFOLK SOUTHERN Rwy.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

DESIGNED BY: AAM DATE: 1-04
 DRAWN BY: VD DATE: 1-04
 SUPERVISED BY: FGB DATE: 1-04
 CHECKED BY: FGB, VD, CED DATE: 1-24

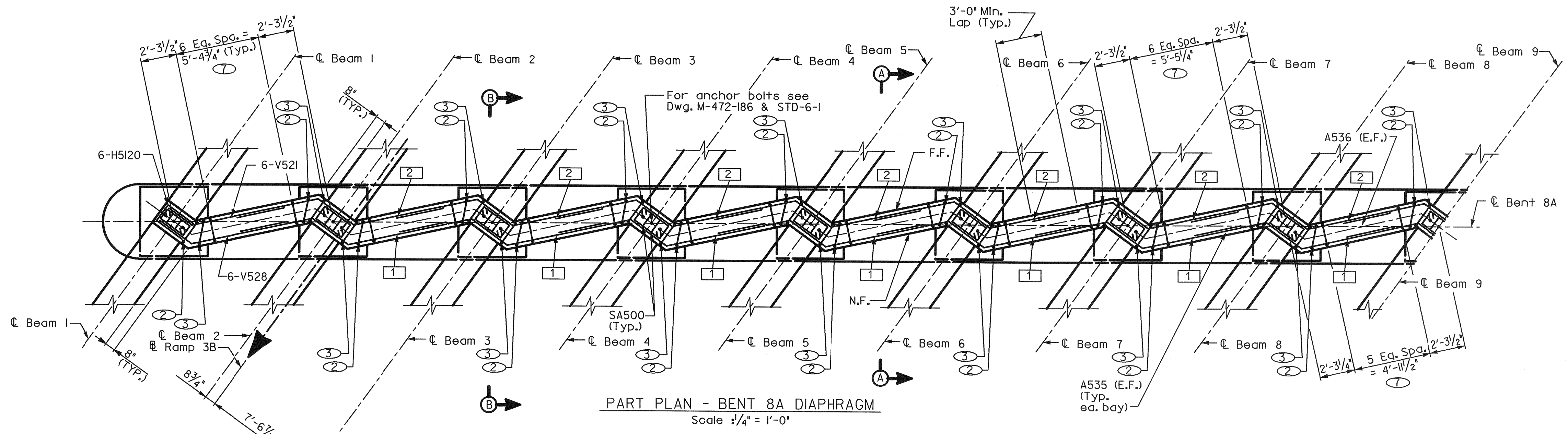
CORRECT ENGINEER OF STRUCTURES
 APPROVED DIRECTOR OF HIGHWAYS
 M-472-156

SCANNED

| CONSTR. NO. 47003-3154-44 | | |
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| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
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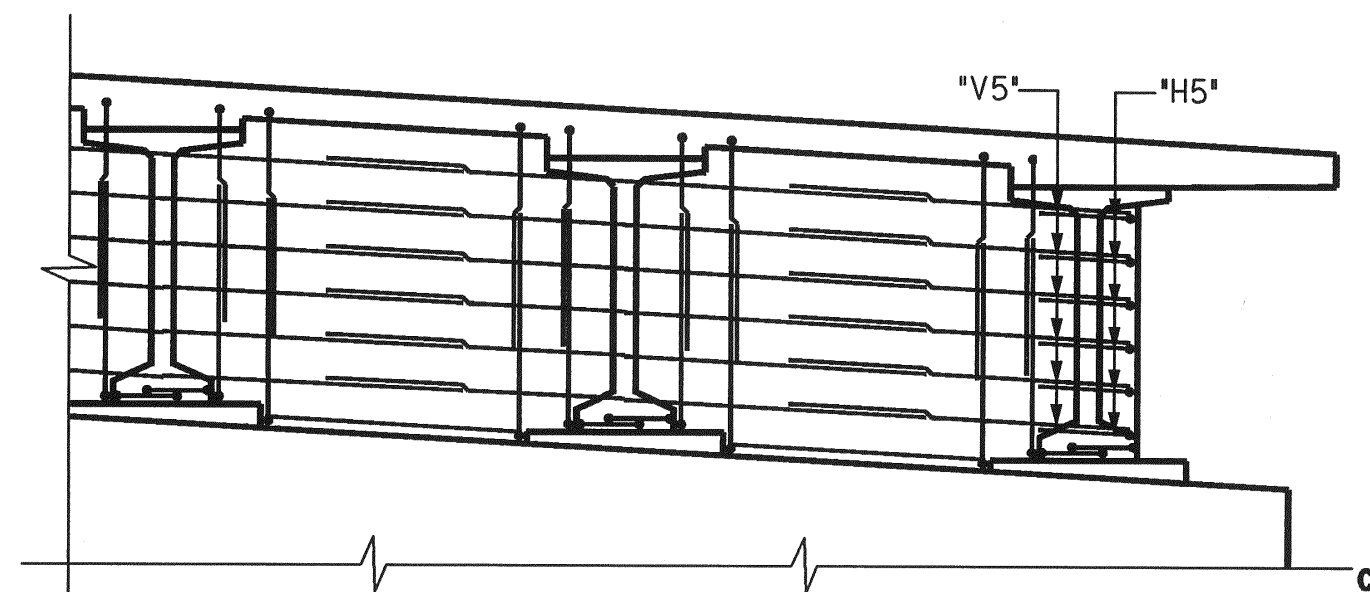
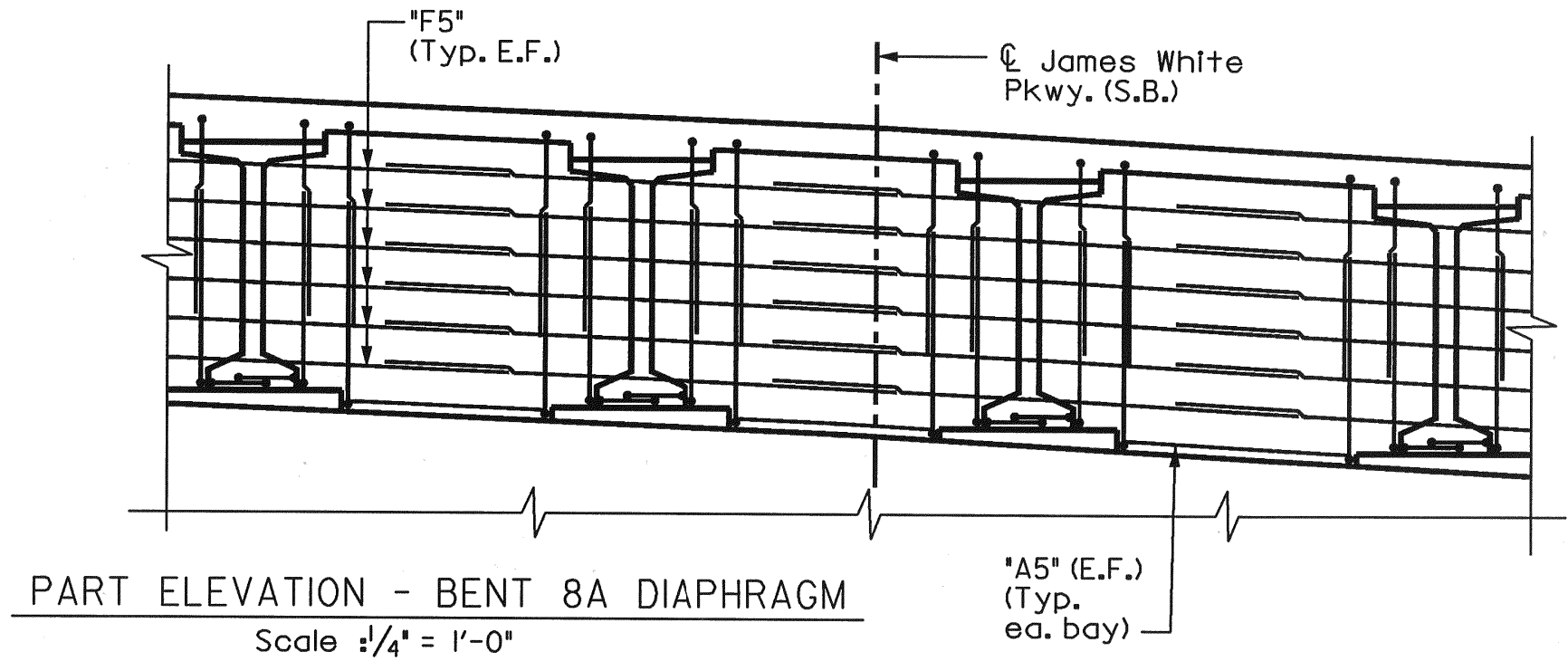
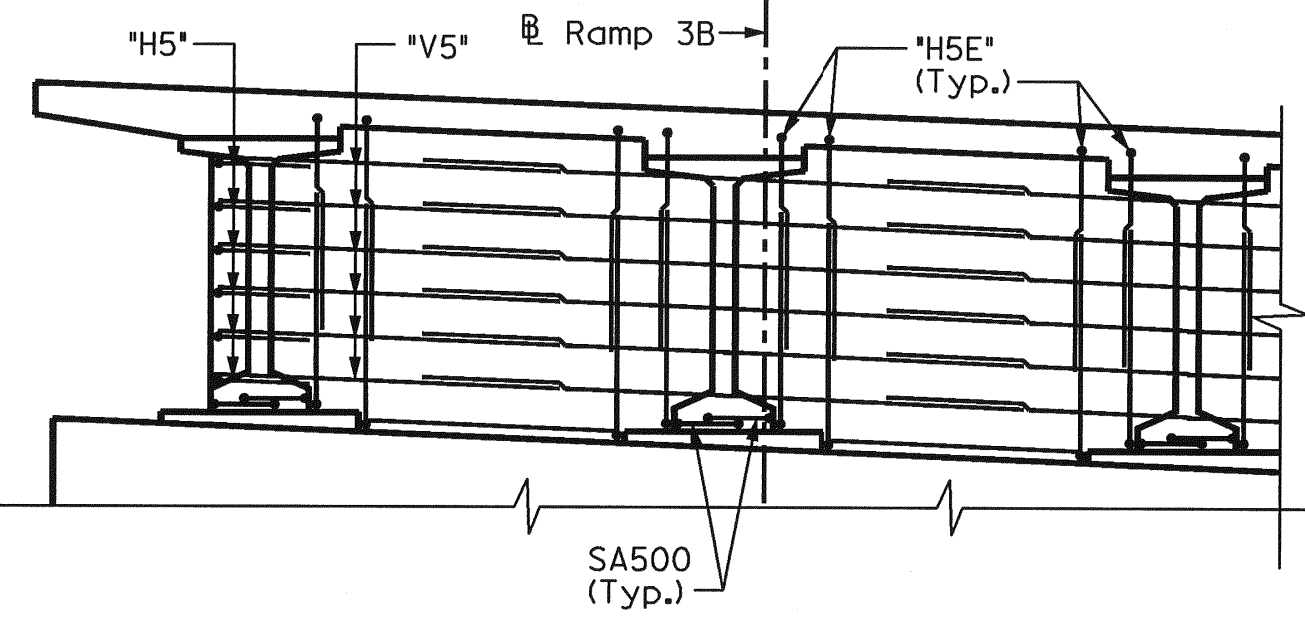
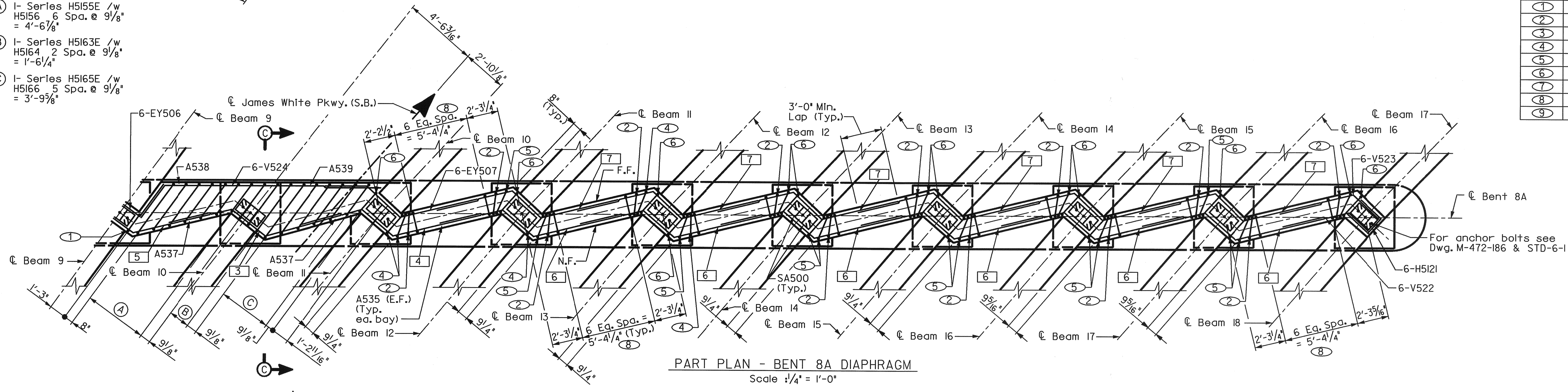
NOTE:
 E.F. - Denotes Each Face
 F.F. - Denotes Far Face
 N.F. - Denotes Near Face
 For Notes and Sections A-A and B-B See Dwg. No. M-472-149.
 For Section C-C See Dwg. No. M-472-156.



- (A) 1- Series H5155E /w H5156 6 Spa. @ 9 1/8" = 4'-6 7/8"
- (B) 1- Series H5163E /w H5164 2 Spa. @ 9 1/8" = 1'-6 1/4"
- (C) 1- Series H5165E /w H5166 5 Spa. @ 9 1/8" = 3'-9 3/8"

| BENT 8A DIAPHRAGM REINFORCEMENT BARS | |
|--------------------------------------|------------------|
| "H" BARS | |
| ① | 6-H5153E/6-H5154 |
| ② | 6-H5157E/6-H5158 |
| ③ | 6-H5159E/6-H5160 |
| ④ | 6-H5161E/6-H5162 |
| ⑤ | 6-H5167E/6-H5168 |
| ⑥ | 6-H5169E/6-H5170 |
| ⑦ | 6-H5171E/6-H5172 |
| ⑧ | 6-H5173E/6-H5174 |
| ⑨ | 6-H5175E/6-H5176 |

| BENT 8A DIAPHRAGM REINFORCEMENT BARS | |
|--------------------------------------|---------|
| "F" BARS | |
| ① | 6-F5118 |
| ② | 6-F5119 |
| ③ | 6-F5120 |
| ④ | 6-F5121 |
| ⑤ | 6-F5122 |
| ⑥ | 6-F5124 |
| ⑦ | 6-F5125 |



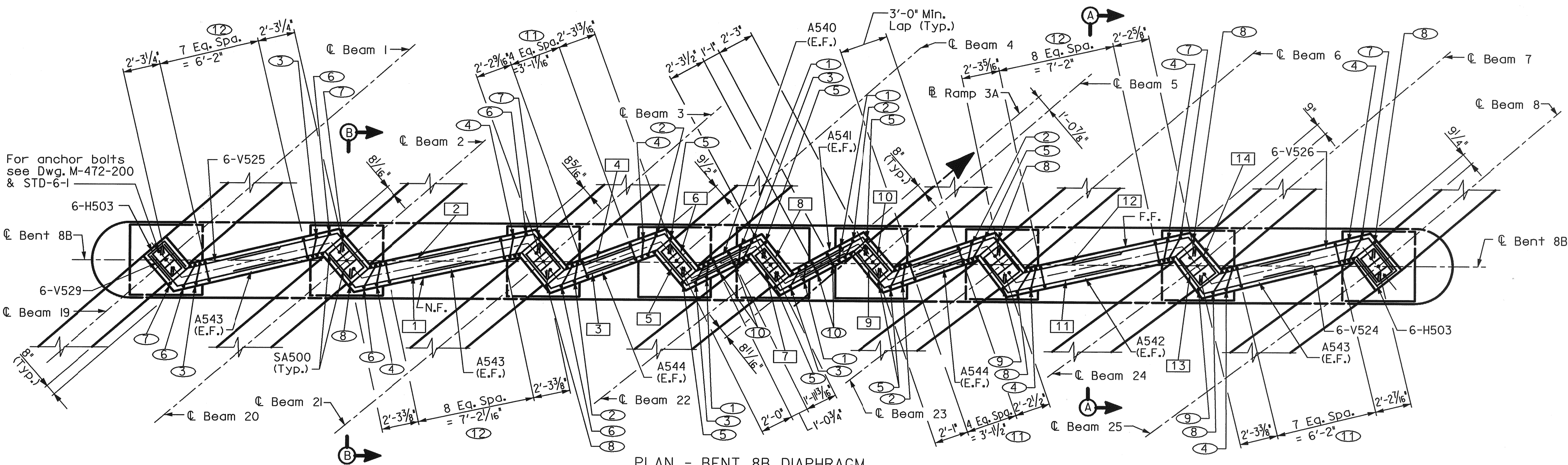
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (25)
 JAMES WHITE PKWY. OVER
 NORFOLK SOUTHERN RY.,
 WILLOW AVE. AND JACKSON AVE.,
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

DESIGNED BY AAM DATE 1-04
 DRAWN BY VD DATE 1-04
 SUPERVISED BY FGB DATE 1-04
 CHECKED BY FGB, VD, CED DATE 1-24

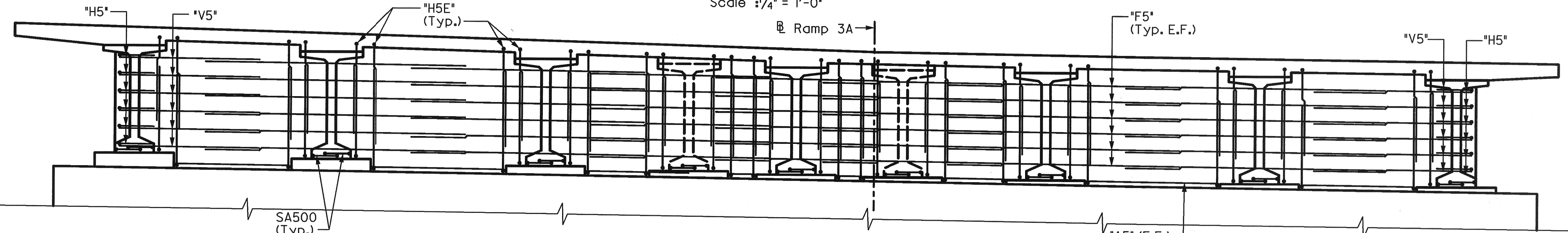
CORRECT ENGINEER OF STRUCTURES
 APPROVED DIRECTOR OF HIGHWAYS M-472-157

| | | |
|-----------------|------|-----------|
| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

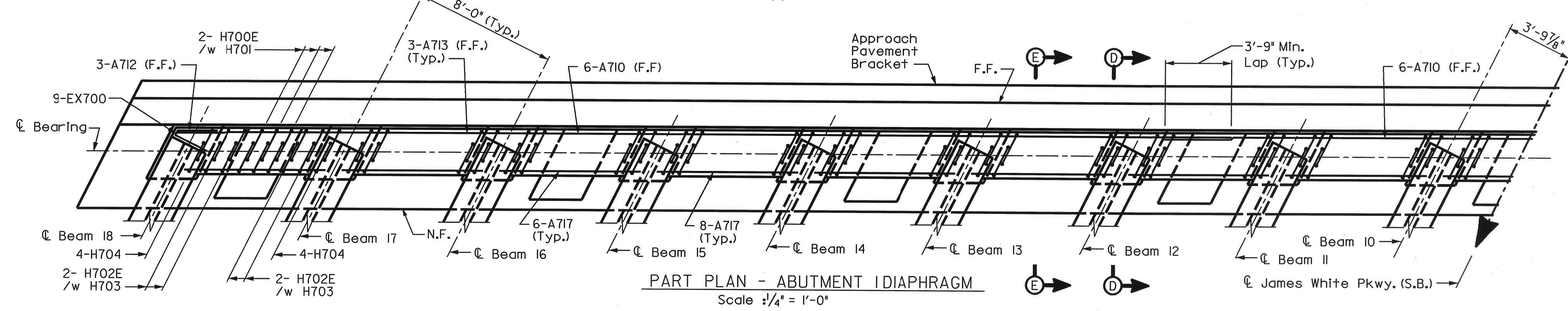
| REVISIONS | | | |
|-----------|------|----|-------------------|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| | | | |
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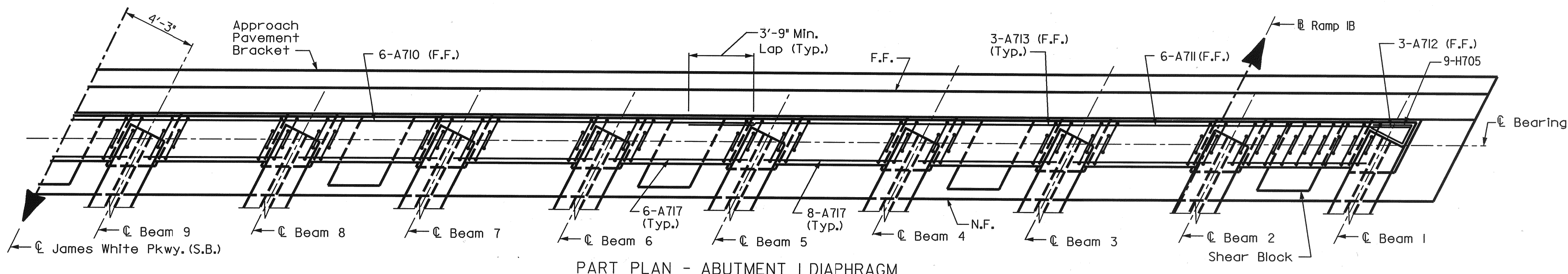
PLAN - BENT 8B DIAPHRAGM
Scale: 1/4" = 1'-0"



ELEVATION - BENT 8B DIAPHRAGM
Scale: 1/4" = 1'-0"



PART PLAN - ABUTMENT I DIAPHRAGM
Scale: 1/4" = 1'-0"

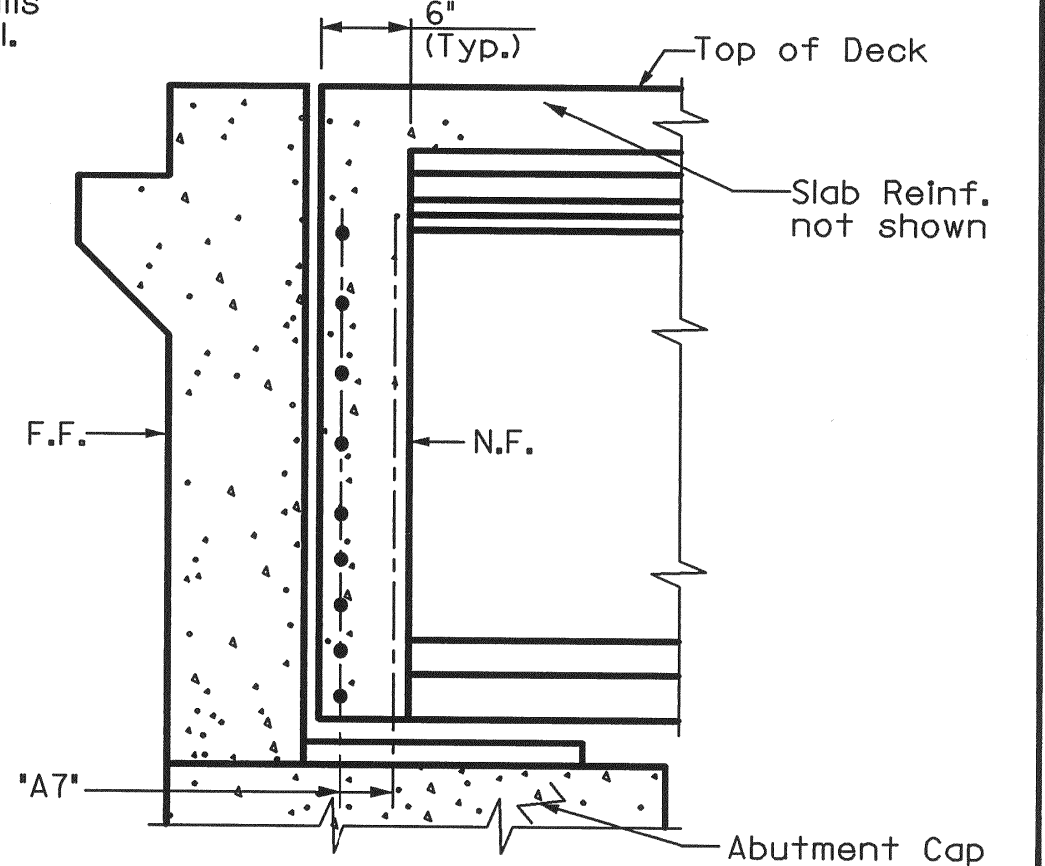


PART PLAN - ABUTMENT I DIAPHRAGM
Scale: 1/4" = 1'-0"

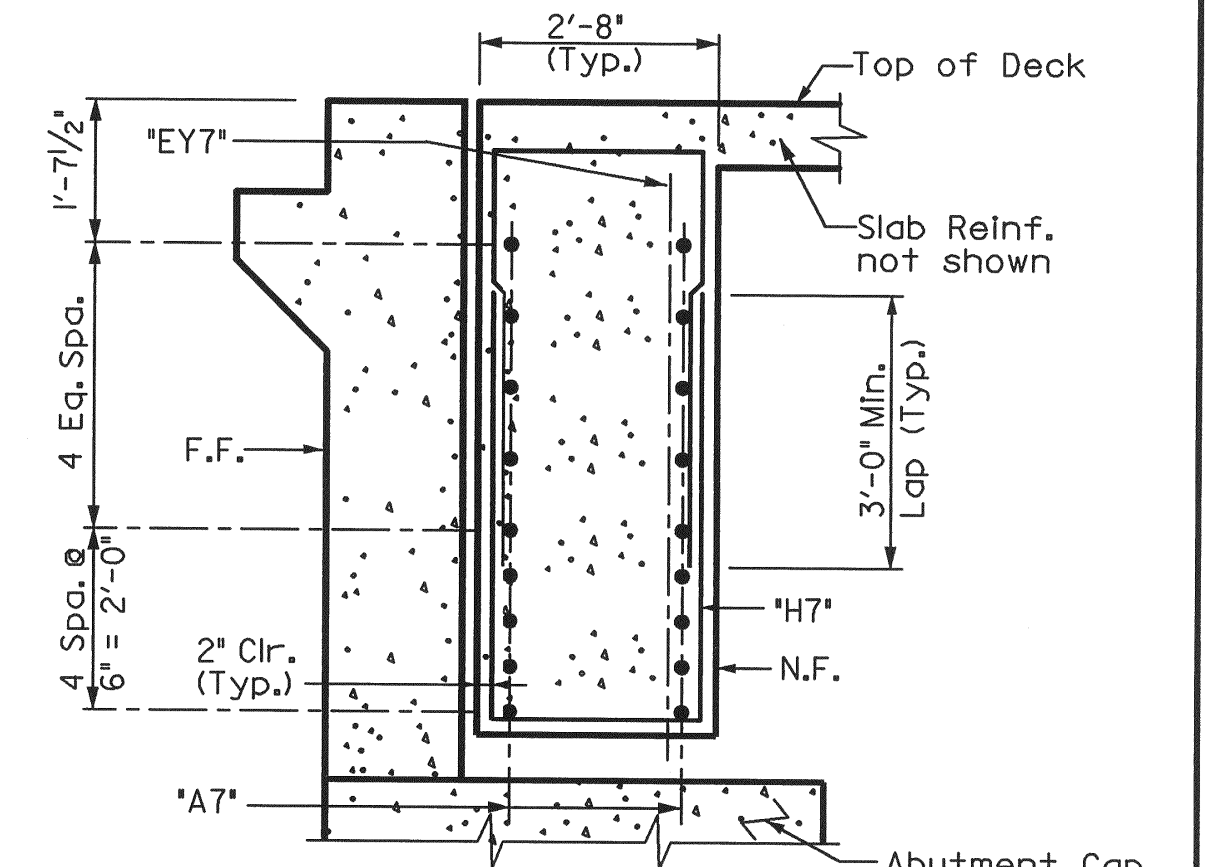
NOTE:
E.F. - Denotes Each Face
F.F. - Denotes Far Face
N.F. - Denotes Near Face
For Notes and Sections A-A and B-B See Dwg. No. M-472-149.
For Joint Details At Abutments See Dwg. No. M-472-185.
For Shear Key Details See Dwg. No. M-472-181.

| BENT 8B DIAPHRAGM REINFORCEMENT BARS | |
|--------------------------------------|------------------|
| "H" BARS | |
| 1 | 6-H5177E/6-H5178 |
| 2 | 6-H5125E/6-H5126 |
| 3 | 6-H5127E/6-H5128 |
| 4 | 6-H5129E/6-H5130 |
| 5 | 6-H5131E/6-H5132 |
| 6 | 6-H5133E/6-H5134 |
| 7 | 6-H5135E/6-H5136 |
| 8 | 6-H5137E/6-H5138 |
| 9 | 6-H5139E/6-H5140 |
| 10 | 6-H5141E/6-H5142 |
| 11 | 6-H5143E/6-H5144 |
| 12 | 6-H5145E/6-H5146 |

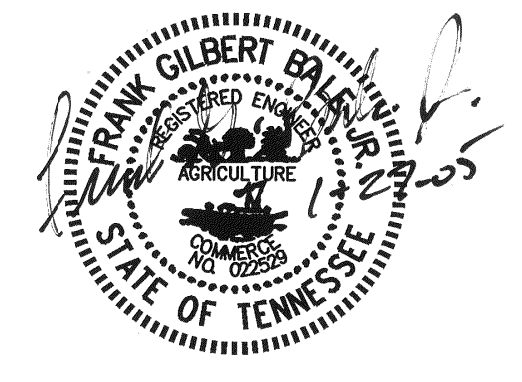
| BENT 8B DIAPHRAGM REINFORCEMENT BARS | |
|--------------------------------------|---------|
| "F" BARS | |
| 1 | 6-F5126 |
| 2 | 6-F5127 |
| 3 | 6-F5128 |
| 4 | 6-F5129 |
| 5 | 6-F5130 |
| 6 | 6-F5131 |
| 7 | 6-F5132 |
| 8 | 6-F5133 |
| 9 | 6-F5134 |
| 10 | 6-F5135 |
| 11 | 6-F5136 |
| 12 | 6-F5137 |
| 13 | 6-F5138 |
| 14 | 6-F5139 |



SECTION D-D
Abutment I as Shown
(Abutment 2A, 2B, 2C Similar)
Not To Scale



SECTION E-E
Abutment I as Shown
(Abutment 2A, 2B, 2C Similar)
Not To Scale



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (26)
JAMES WHITE PKWY. OVER
NORFOLK SOUTHERN RWY.,
WILLOW AVE. AND JACKSON AVE.
STA. 81+37.61 @ JWP (S.B.)
KNOX COUNTY
2005

DESIGNED BY: AAM DATE: 1-04
DRAWN BY: VD DATE: 1-04
SUPERVISED BY: FGB DATE: 1-04
CHECKED BY: FGB, VD, CED DATE: 1-24

CORRECT ENGINEER OF STRUCTURES
APPROVED DIRECTOR OF HIGHWAYS
M-472-158

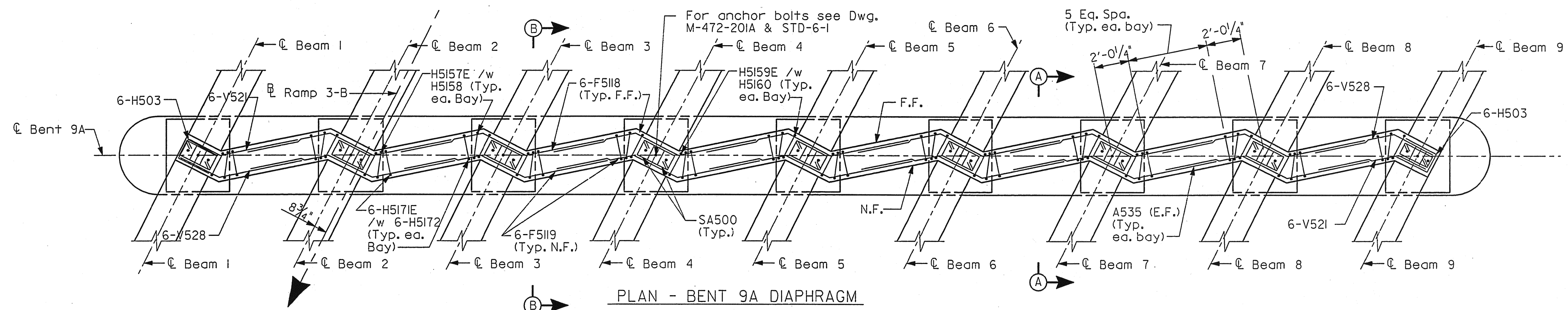
| PROJECT NO. | YEAR | SHEET NO. |
|-----------------|------|-----------|
| IM/NH-40-8(135) | 2005 | |

REVISIONS

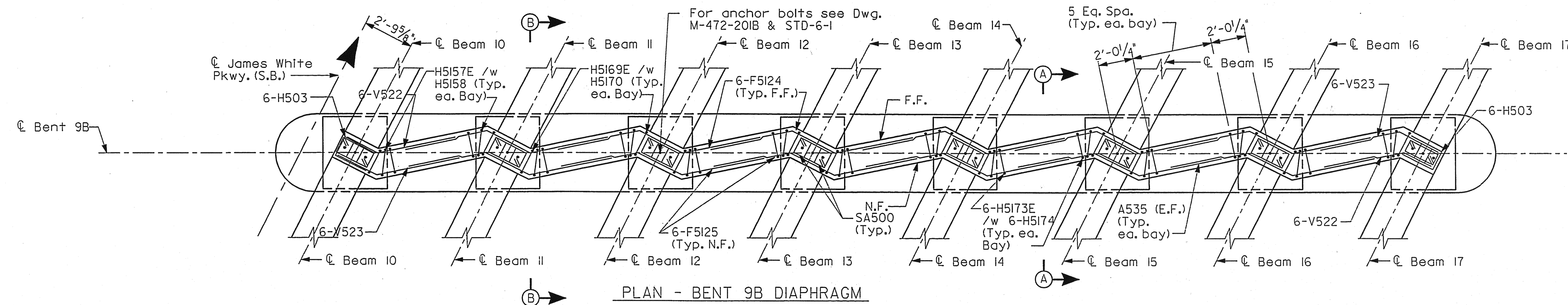
| NO. | DATE | BY | BRIEF DESCRIPTION |
|-----|---------|-----|---------------------------|
| 1 | 6-05-06 | CMD | THIS SHEET ADDED TO PLANS |
| | | | |
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| | | | |
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NOTE:

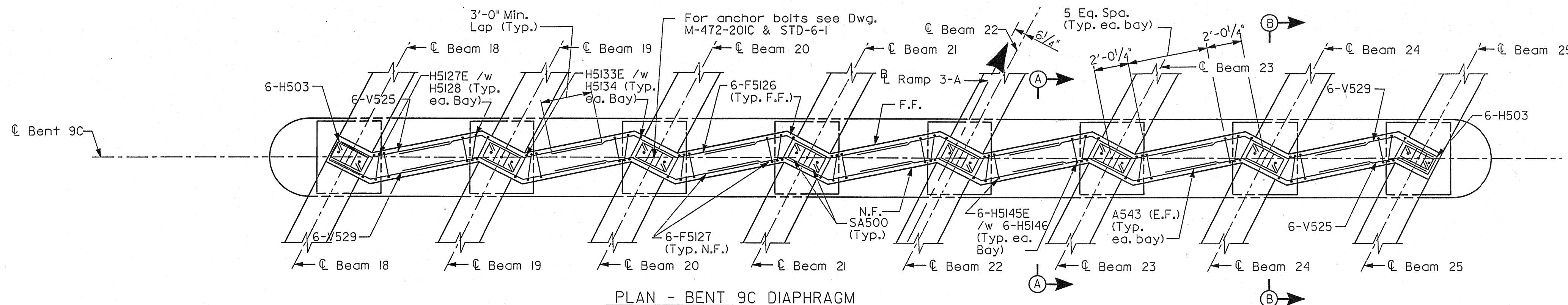
E.F. - Denotes Each Face
 F.F. - Denotes Far Face
 N.F. - Denotes Near Face
 For Notes and Sections A-A and B-B See Dwg. No. M-472-149



PLAN - BENT 9A DIAPHRAGM

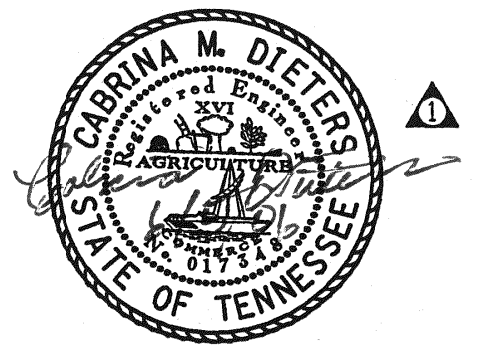


PLAN - BENT 9B DIAPHRAGM



PLAN - BENT 9C DIAPHRAGM

\$USERS
 \$TIMES
 \$DATES
 \$FILES



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
SUPERSTRUCTURE DETAILS (29)
 JAMES WHITE PKWY. OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

DESIGNED BY AAM DATE 1-04
 DRAWN BY VD DATE 1-04
 SUPERVISED BY FCB DATE 1-04
 CHECKED BY FCB, VD, CED DATE 1-24

CORRECT _____ ENGINEER OF STRUCTURES

APPROVED _____ DIRECTOR OF HIGHWAYS

M-472-160A

CONST. NO. 47003-3154-44

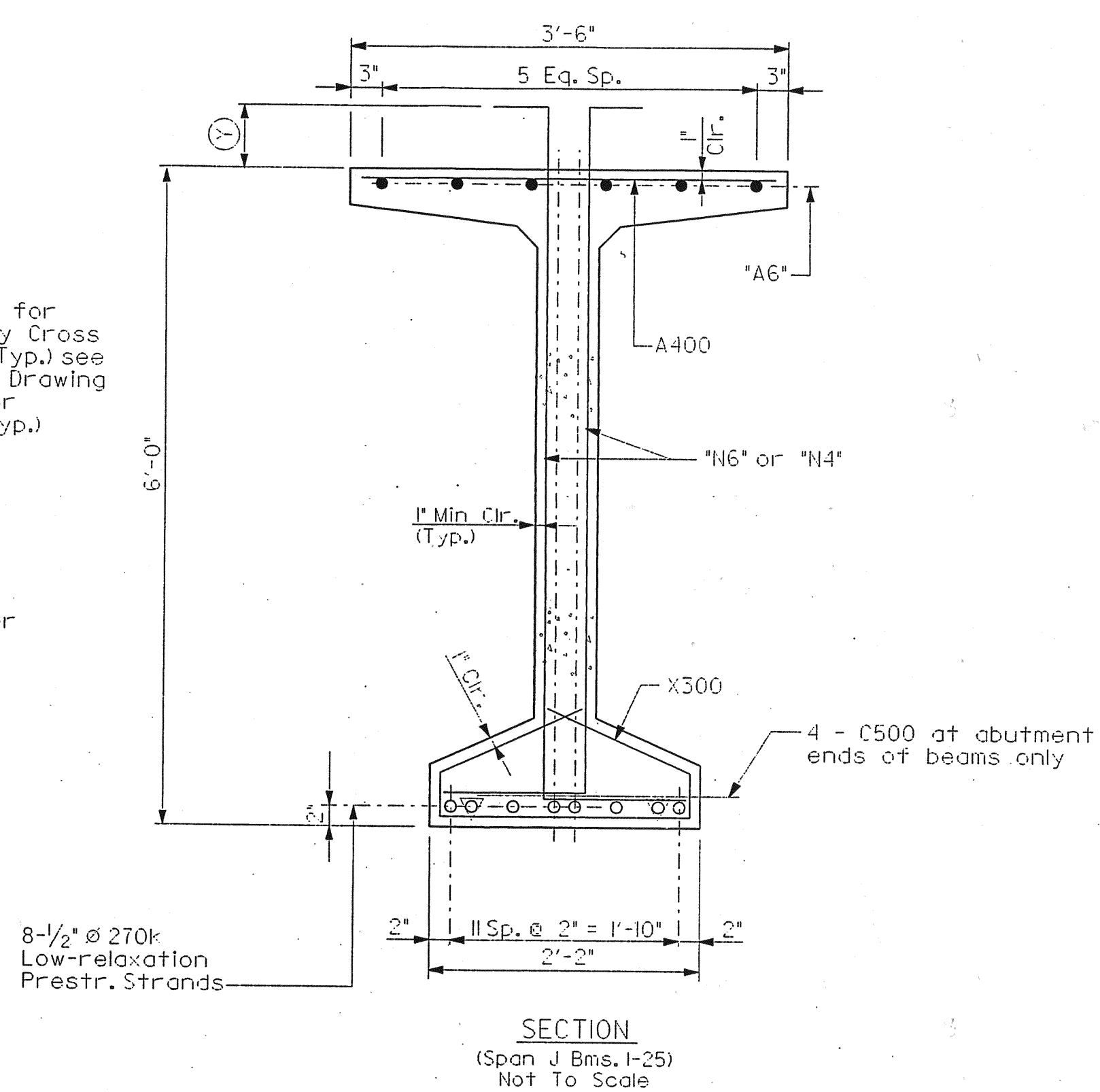
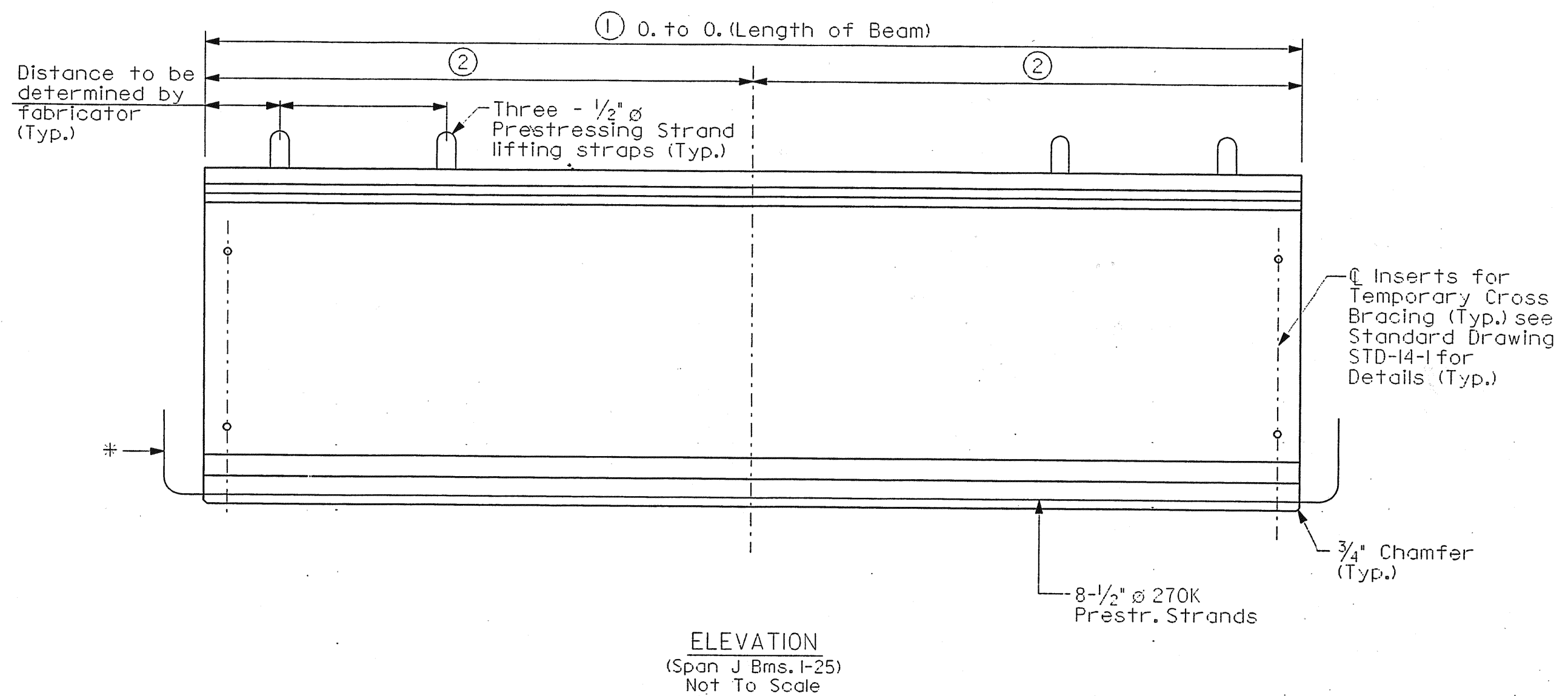
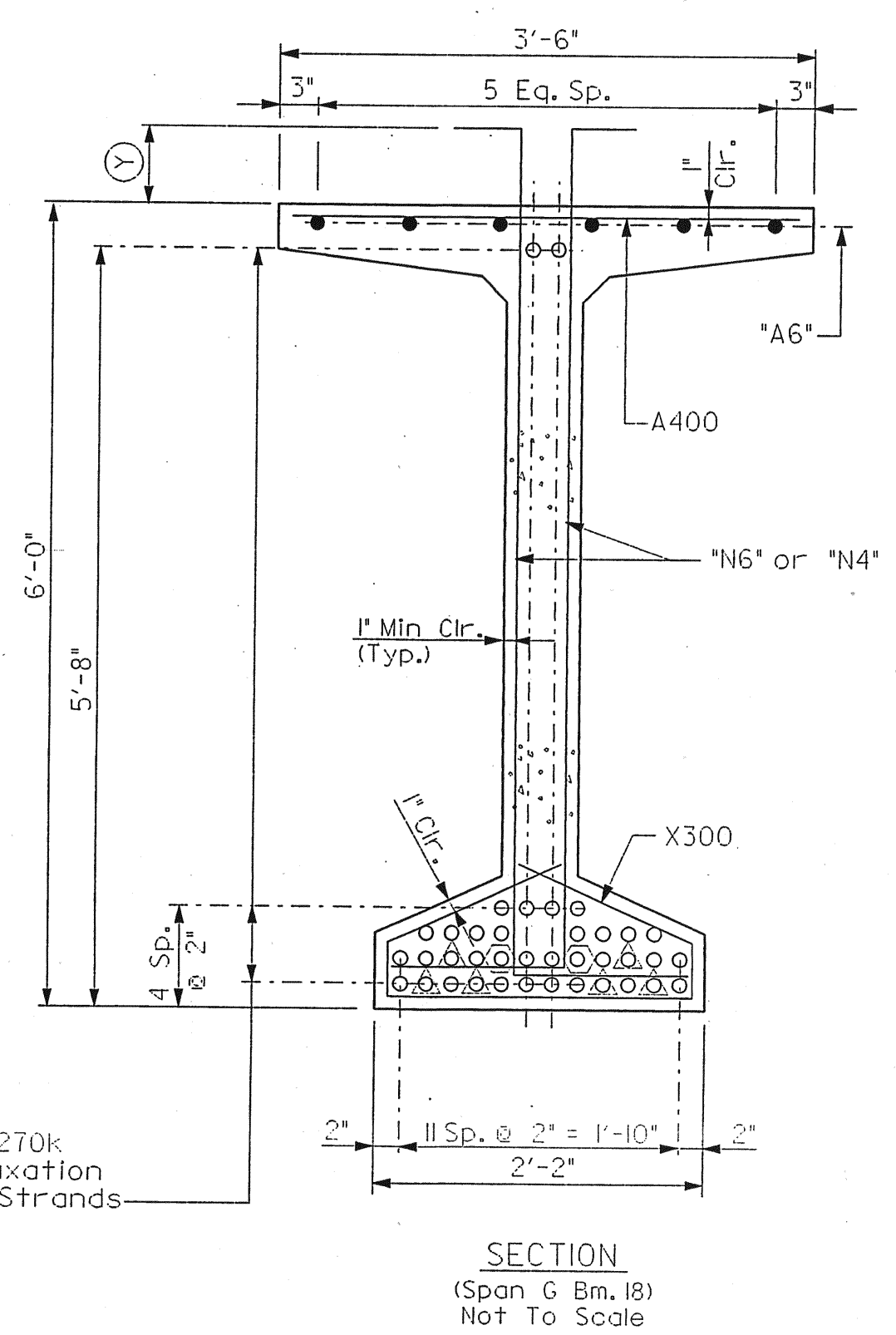
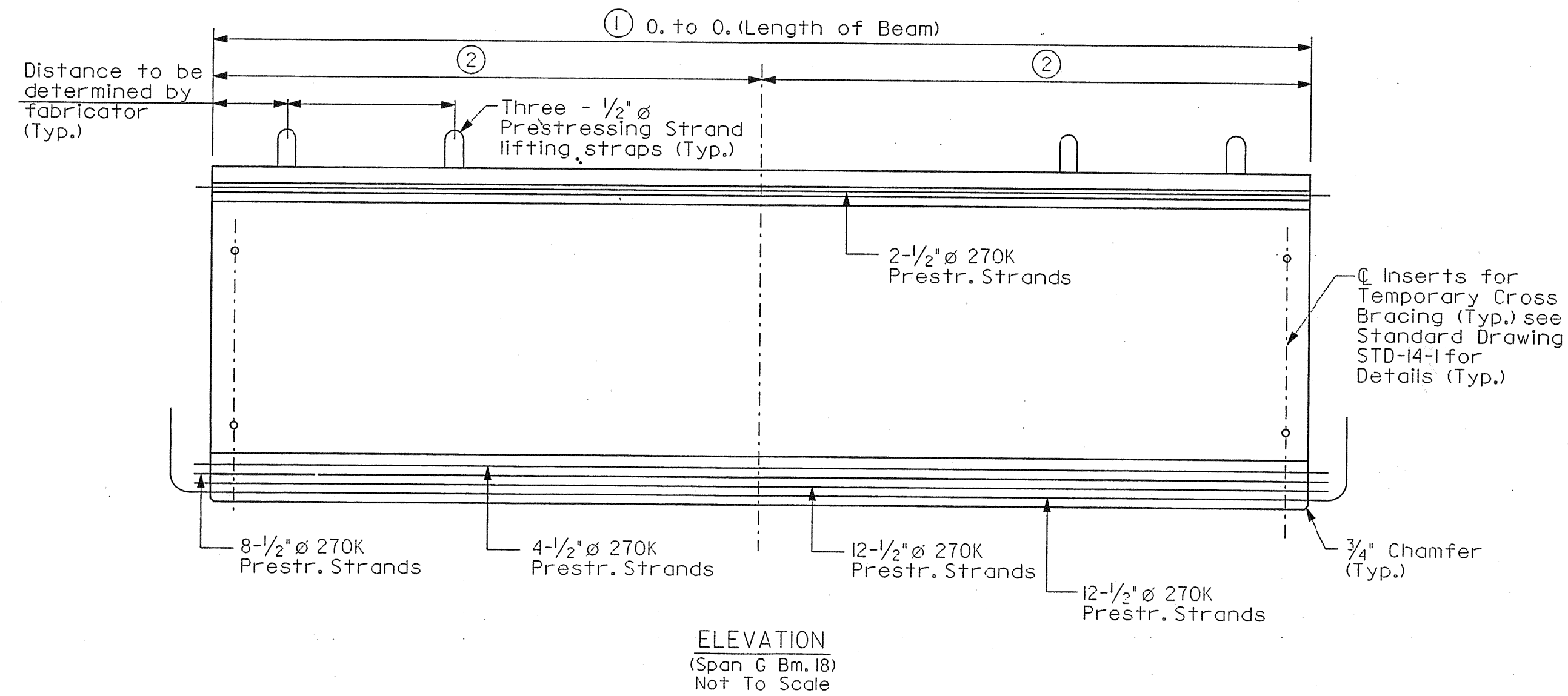
| PROJECT NO. | YEAR | SHEET NO. |
|-----------------|------|-----------|
| IM/NH-40-8(135) | 2005 | |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|-----|---------|-----|--------------------|
| 1 | 1-21-06 | CMD | ADDED SPAN J BEAMS |
| | | | |
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| | | | |
| | | | |

- - Grade 60 Reinforcing
- - Fully bonded prestressing strands
- ⊙ - Debond 2'-0" at each end of beams
- △ - Debond 4'-0" at each end of beams
- ▽ - Debond 8'-0" at each end of beams

For additional details, see
DWG Nos. M-472-165 and
M-472-172 thru M-472-175



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 21
PRESTRESSED BEAM DETAILS (7)
JAMES WHITE PKWY OVER
NORFOLK SOUTHERN RWY.,
WILLOW AVE. AND JACKSON AVE.
STA. 81+37.61 Q. JWP (S.B.)
KNOX COUNTY
2005

* For beam ends at abutments, do not bend bottom strands.
1" min. & 4" max. straight projections

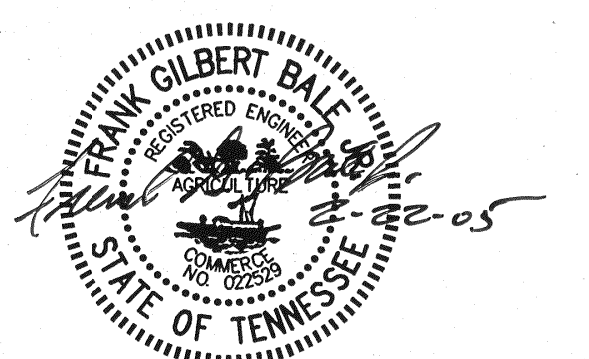
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|---------------|-----|------|-------|
| DESIGNED BY | BBW | DATE | 9-04 |
| DRAWN BY | RMH | DATE | 12-04 |
| SUPERVISED BY | FCB | DATE | 12-04 |
| CHECKED BY | CEH | DATE | 12-04 |

CORRECT
ENGINEER OF STRUCTURES
APPROVED
DIRECTOR OF HIGHWAYS

M-472-171

Table with columns: BEAM, DIMENSIONS (1-8), DEAD LOAD CORRECTION (A-C), LL DISTRIBUTION FACTOR (wheels), COMPOSITE DEAD LOAD (lb/ft), f_c (psi), f_ci (psi), A400 (BAR SIZE, NUMBER, LENGTH), A500 (BAR SIZE, NUMBER, LENGTH). Rows include beams A1 through F23.

PROJECT NO. 47003-3154-44, YEAR 2005, SHEET NO. IM/NH-40-8(135). REVISIONS table with columns: NO., DATE, BY, BRIEF DESCRIPTION. Revision 1: 2-11-05, CEH, FONTS.



STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS BRIDGE NO. 21 PRESTRESSED BEAM DETAILS (8) JAMES WHITE PKWY OVER NORFOLK SOUTHERN RWY., WILLOW AVE. AND JACKSON AVE. STA. 81+37.61 Q JWP (S.B.) KNOX COUNTY 2005

DESIGNED BY BRW DATE 9-04
DRAWN BY DM DATE 1-05
SUPERVISED BY FGB DATE 1-05
CHECKED BY CFH DATE 1-05

CORRECT ENGINEER OF STRUCTURES
APPROVED DIRECTOR OF HIGHWAYS

SCANNED
RHALL
10/22/2005
6:11:40 PM
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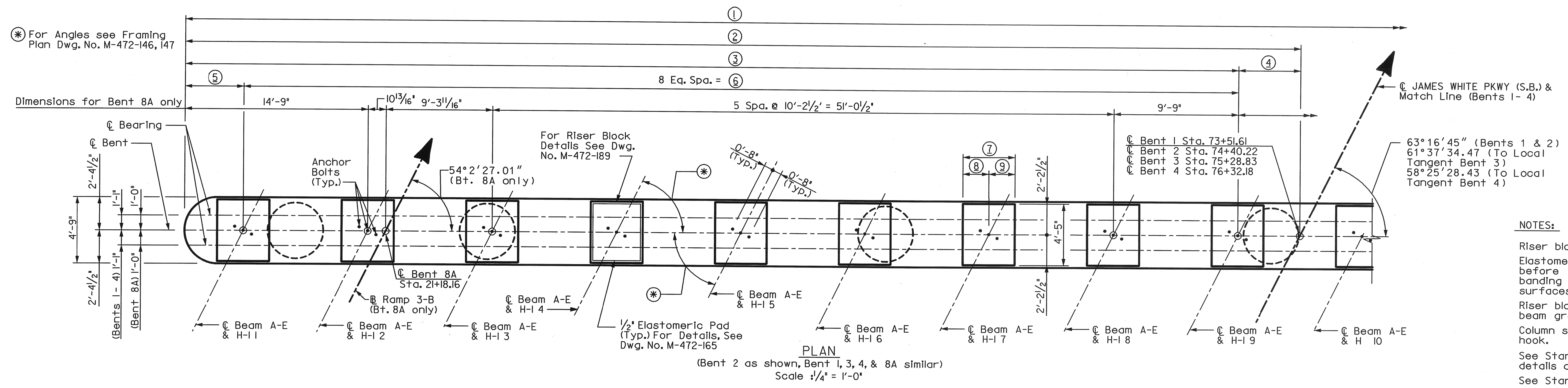
| | | |
|---------------|------|-----------|
| PROJECT NO. | YEAR | SHEET NO. |
| M/NH-40-8035) | 2005 | |

REVISIONS

| NO. | DATE | BY | BRIEF DESCRIPTION |
|---------|------|----|--|
| 2-II-05 | CEH | | FONTS |
| 1-21-06 | CMD | | REVISED BEAMS II-I25, ADDED BEAMS JI-J25 |

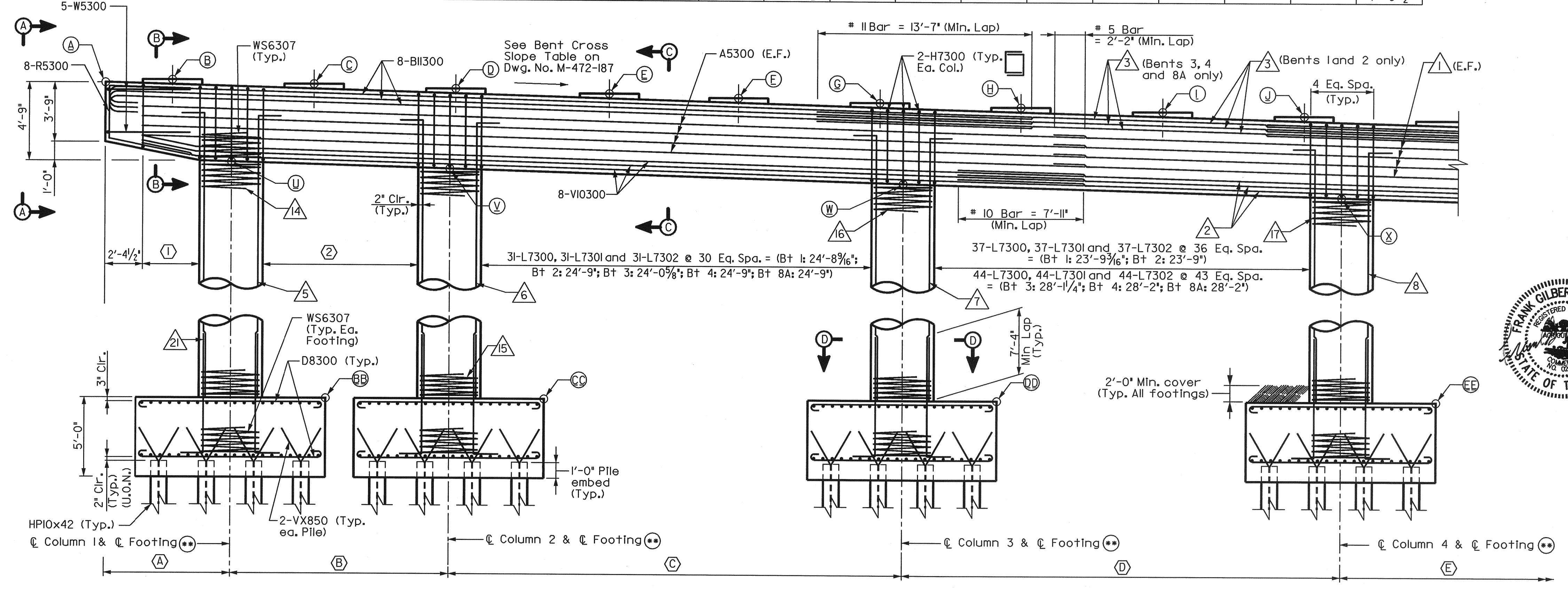
| BEAM | DIMENSIONS | | | | | | | | DEAD LOAD CORRECTION | | | LL DISTRIBUTION COMPOSITE | | A400 | | | | A500 | | | | | |
|-----------|---------------|--------------|-------------------------|-------------------------|-------|----------|--------------|--------------|----------------------|----------|-------|---------------------------|-------------------|----------|-----------|----------|--------|--------|----------|--------|--------|---|-------|
| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | A | B | C (%) | FACTOR (wheels) | DEAD LOAD (lb/ft) | Fo (psi) | Fci (psi) | BAR SIZE | NUMBER | LENGTH | BAR SIZE | NUMBER | LENGTH | | |
| G1 | 108'-5 1/2" | 54'-2 3/4" | 17 sp. @ 1'-0" = 17'-0" | 15 sp. @ 2'-0" = 30'-0" | 1'-0" | 2 3/4" | 0'-9 15/16" | 0'-0" | 1 15/16" | 1 7/16" | 28 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 78 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G2 | 109'-5 3/8" | 54'-8 11/16" | 17 sp. @ 1'-0" = 17'-0" | 15 sp. @ 2'-0" = 30'-0" | 1'-0" | 8 11/16" | 0'-10 1/4" | 0'-9 1/16" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 78 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G3 | 110'-5 7/16" | 55'-2 11/16" | 17 sp. @ 1'-0" = 17'-0" | 16 sp. @ 2'-0" = 32'-0" | N/A | 2 11/16" | 0'-10 9/16" | 0'-9 3/8" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 78 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G4 | 111'-5 11/16" | 55'-8 7/8" | 17 sp. @ 1'-0" = 17'-0" | 16 sp. @ 2'-0" = 32'-0" | N/A | 8 7/8" | 0'-10 7/8" | 0'-9 5/8" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 78 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G5 | 112'-6 1/8" | 56'-3 1/16" | 17 sp. @ 1'-0" = 17'-0" | 16 sp. @ 2'-0" = 32'-0" | 1'-0" | 3 1/16" | 0'-11 1/4" | 0'-9 15/16" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 80 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G6 | 113'-6 3/4" | 56'-9 3/8" | 17 sp. @ 1'-0" = 17'-0" | 16 sp. @ 2'-0" = 32'-0" | 1'-0" | 9 3/8" | 0'-11 9/16" | 0'-10 1/4" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 80 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G7 | 114'-7 9/16" | 57'-3 13/16" | 17 sp. @ 1'-0" = 17'-0" | 17 sp. @ 2'-0" = 34'-0" | N/A | 3 13/16" | 0'-11 15/16" | 0'-10 1/2" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 80 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G8 | 115'-5 7/8" | 57'-8 15/16" | 17 sp. @ 1'-0" = 17'-0" | 17 sp. @ 2'-0" = 34'-0" | 1'-0" | 8 15/16" | 0'-11 1/4" | 0'-11" | 2 1/8" | 1 9/16" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 80 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G9 | 116'-4 1/4" | 58'-2 1/8" | 17 sp. @ 1'-0" = 17'-0" | 17 sp. @ 2'-0" = 34'-0" | 1'-0" | 2 1/8" | 1'-0 1/2" | 0'-11 1/4" | 2 5/16" | 1 3/4" | 27 | 1.080 | 349 | 7000 | 6000 | A400 | 4 | 82 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G10 | 117'-2 3/4" | 58'-7 3/8" | 21 sp. @ 1'-0" = 21'-0" | 15 sp. @ 2'-0" = 30'-0" | 1'-0" | 7 3/8" | 1'-0 13/16" | 2'-4 13/16" | 2 5/16" | 1 3/4" | 28 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 86 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G11 | 118'-1 3/8" | 59'-0 11/16" | 21 sp. @ 1'-0" = 21'-0" | 15 sp. @ 2'-0" = 30'-0" | 1'-6" | 6 11/16" | 1'-1 1/8" | 0'-11 3/4" | 2 3/8" | 1 13/16" | 27 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 86 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G12 | 119'-0 1/16" | 59'-6 1/16" | 21 sp. @ 1'-0" = 21'-0" | 16 sp. @ 2'-0" = 32'-0" | N/A | 6 1/16" | 1'-1 7/16" | 1'-0" | 2 7/16" | 1 13/16" | 27 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 86 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G13 | 119'-10 7/8" | 59'-11 7/16" | 21 sp. @ 1'-0" = 21'-0" | 16 sp. @ 2'-0" = 32'-0" | N/A | 11 7/16" | 2'-4 15/16" | 1'-0 1/4" | 2 5/16" | 1 3/4" | 27 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 86 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G14 | 122'-10 3/16" | 61'-5 1/16" | 21 sp. @ 1'-0" = 21'-0" | 17 sp. @ 2'-0" = 34'-0" | N/A | 5 1/8" | 2'-1" | 1'-1 1/16" | 2 11/16" | 2" | 27 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 88 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G15 | 122'-0 13/16" | 61'-0 7/16" | 21 sp. @ 1'-0" = 21'-0" | 16 sp. @ 2'-0" = 32'-0" | 1'-6" | 6 7/16" | 2'-3 1/4" | 1'-0 15/16" | 2 5/8" | 1 15/16" | 27 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 88 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G16 | 125'-0 13/16" | 62'-6 3/8" | 21 sp. @ 1'-0" = 21'-0" | 17 sp. @ 2'-0" = 34'-0" | 1'-0" | 6 3/8" | 1'-4 7/16" | 1'-1 11/16" | 2 11/16" | 2" | 26 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 90 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G17 | 126'-4 7/16" | 63'-2 3/16" | 21 sp. @ 1'-0" = 21'-0" | 18 sp. @ 2'-0" = 36'-0" | N/A | 2 3/16" | 1'-5" | 1'-2 1/8" | 2 15/16" | 2 3/16" | 27 | 1.080 | 355 | 7000 | 6000 | A400 | 4 | 90 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G18 | 127'-6 7/16" | 63'-9 7/16" | 21 sp. @ 1'-0" = 21'-0" | 18 sp. @ 2'-0" = 36'-0" | N/A | 9 7/16" | 1'-5 7/16" | 1'-2 11/16" | 3 5/8" | 2 11/16" | 23 | 1.080 | 287 | 7000 | 6000 | A400 | 4 | 90 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G19 | 128'-9 1/2" | 64'-4 3/4" | 23 sp. @ 1'-0" = 23'-0" | 17 sp. @ 2'-0" = 34'-0" | 1'-0" | 4 3/4" | 1'-5 15/16" | 1'-4 7/16" | 2 15/16" | 2 1/4" | 26 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 94 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G20 | 130'-0 1/4" | 65'-0 1/8" | 23 sp. @ 1'-0" = 23'-0" | 17 sp. @ 2'-0" = 34'-0" | 1'-6" | 6 1/8" | 1'-6 7/16" | 1'-3 3/4" | 3 1/16" | 2 5/16" | 26 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 94 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G21 | 131'-3 1/8" | 65'-7 9/16" | 23 sp. @ 1'-0" = 23'-0" | 18 sp. @ 2'-0" = 36'-0" | N/A | 7 9/16" | 1'-7" | 1'-4 1/16" | 3 3/16" | 2 3/8" | 26 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 94 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G22 | 132'-6 1/8" | 66'-3 1/16" | 23 sp. @ 1'-0" = 23'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 3 1/16" | 4'-2 5/16" | 1'-4 7/16" | 3 1/16" | 2 5/16" | 25 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 96 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G23 | 135'-7 7/16" | 67'-9 3/4" | 23 sp. @ 1'-0" = 23'-0" | 19 sp. @ 2'-0" = 38'-0" | N/A | 9 3/4" | 3'-3 3/8" | 1'-5 7/16" | 3 5/8" | 2 11/16" | 26 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 96 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G24 | 134'-7 15/16" | 67'-3 15/16" | 23 sp. @ 1'-0" = 23'-0" | 19 sp. @ 2'-0" = 38'-0" | N/A | 3 15/16" | 3'-8 5/16" | 1'-5 1/4" | 3 1/2" | 2 5/8" | 26 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 96 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| G25 | 137'-9 11/16" | 68'-10 7/8" | 23 sp. @ 1'-0" = 23'-0" | 19 sp. @ 2'-0" = 38'-0" | 1'-0" | 10 7/8" | 0'-0" | 1'-6 3/16" | 3 13/16" | 2 13/16" | 25 | 1.080 | 363 | 8000 | 7000 | A400 | 4 | 98 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H1 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-8 13/16" | 0'-0" | 1 1/8" | 7/8" | 33 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H2 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-8 7/8" | 0'-8 13/16" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H3 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-8 15/16" | 0'-8 13/16" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H4 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-9" | 0'-8 13/16" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H5 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-9 1/16" | 0'-8 13/16" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H6 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-9 1/16" | 0'-8 13/16" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H7 | 98'-2 3/4" | 49'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 1 3/8" | 0'-9 5/16" | 0'-8 13/16" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H8 | 98'-5 3/16" | 49'-2 5/8" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 2 5/8" | 0'-9 3/8" | 0'-8 7/8" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H9 | 98'-4 7/16" | 49'-2 1/4" | 6 sp. @ 1'-0" = 6'-0" | 18 sp. @ 2'-0" = 36'-0" | 1'-0" | 2 1/4" | 1'-10 9/16" | 0'-9 1/8" | 1 1/16" | 13/16" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H10 | 101'-0 3/8" | 50'-6 3/16" | 6 sp. @ 1'-0" = 6'-0" | 19 sp. @ 2'-0" = 38'-0" | N/A | 6 3/16" | 1'-11 5/16" | 0'-10 1/16" | 1" | 3/4" | 28 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 62 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H11 | 103'-9 3/4" | 51'-10 7/8" | 6 sp. @ 1'-0" = 6'-0" | 19 sp. @ 2'-0" = 38'-0" | 1'-0" | 10 7/8" | 0'-10 15/16" | 0'-11 5/8" | 1 5/16" | 1" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H12 | 104'-0 1/4" | 52'-0 1/8" | 6 sp. @ 1'-0" = 6'-0" | 19 sp. @ 2'-0" = 38'-0" | 1'-6" | 6 1/8" | 0'-11" | 0'-10 13/16" | 1 9/16" | 1 1/8" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H13 | 104'-2 13/16" | 52'-1 3/8" | 6 sp. @ 1'-0" = 6'-0" | 20 sp. @ 2'-0" = 40'-0" | N/A | 1 3/8" | 0'-11 1/16" | 0'-10 15/16" | 1 9/16" | 1 1/8" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H14 | 104'-5 5/16" | 52'-2 11/16" | 6 sp. @ 1'-0" = 6'-0" | 20 sp. @ 2'-0" = 40'-0" | N/A | 2 11/16" | 0'-11 1/4" | 0'-11" | 1 9/16" | 1 1/8" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H15 | 104'-7 7/8" | 52'-3 15/16" | 6 sp. @ 1'-0" = 6'-0" | 20 sp. @ 2'-0" = 40'-0" | N/A | 3 15/16" | 0'-11 5/16" | 0'-11 1/16" | 1 9/16" | 1 1/8" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| 120'-2H16 | 104'-10 3/8" | 52'-5 3/16" | 6 sp. @ 1'-0" = 6'-0" | 20 sp. @ 2'-0" = 40'-0" | N/A | 5 3/16" | 0'-11 7/16" | 0'-11 1/8" | 1 9/16" | 1 1/8" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H17 | 105'-0 15/16" | 52'-6 7/16" | 6 sp. @ 1'-0" = 6'-0" | 20 sp. @ 2'-0" = 40'-0" | N/A | 6 7/16" | 0'-11 9/16" | 0'-11 13/16" | 1 9/16" | 1 1/8" | 29 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H18 | 105'-3 1/2" | 52'-7 3/4" | 6 sp. @ 1'-0" = 6'-0" | 20 sp. @ 2'-0" = 40'-0" | N/A | 7 3/4" | 0'-0" | 0'-11 1/4" | 1 3/8" | 1 1/16" | 32 | 1.236 | 369 | 7000 | 6000 | A400 | 4 | 64 | 3'-2" | A500 | 5 | 8 | 5'-6" |
| H19 | 121'-7 11/16" | 6 | | | | | | | | | | | | | | | | | | | | | |

| | | | |
|---------------------------|------|-----------|-------------------|
| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |



BENTS 1 THRU 4 & 8A DIMENSION TABLE

| BENT | ① | ② | ③ | ④ | ⑤ | ⑥ | ⑦ | ⑧ | ⑨ | ⑩ | ⑪ | ⑫ | ⑬ | ⑭ |
|------|---------------|--------------|--------------|-----------|-----------|--------------|--------|------------|------------|--------------|-----------|--------------|--------------|-----------|
| 1 | 160'-8 5/8" | 80'-4 5/16" | 75'-10 9/16" | 4'-5 3/4" | 4'-3" | 71'-7 9/16" | 3'-9" | 1'-10 1/2" | 1'-10 1/2" | 80'-4 5/16" | 4'-5 3/4" | 75'-10 9/16" | 71'-7 9/16" | 4'-3" |
| 2 | 160'-9 1/8" | 80'-4 5/16" | 75'-10 9/16" | 4'-5 3/4" | 4'-3" | 71'-7 9/16" | 3'-9" | 1'-10 1/2" | 1'-10 1/2" | 80'-4 5/16" | 4'-5 3/4" | 75'-11 1/16" | 71'-7 9/16" | 4'-3 1/2" |
| 3 | 163'-10 1/2" | 81'-11 3/16" | 77'-4 1/16" | 4'-6 3/4" | 4'-3 1/2" | 73'-0 15/16" | 3'-10" | 1'-11" | 1'-11" | 81'-11 3/16" | 4'-6 1/8" | 77'-4 1/16" | 73'-0 15/16" | 4'-3 1/2" |
| 4 | 169'-11 1/16" | 84'-5 9/16" | 80'-2 1/16" | 4'-2 1/8" | 4'-4 1/2" | 75'-10 3/16" | 4'-0" | 2'-0" | 2'-0" | 85'-5 1/2" | 5'-2 1/8" | 80'-2 5/8" | 75'-10 3/16" | 4'-4 1/2" |
| 8A | 180'-6 1/4" | | | | 4'-6 1/2" | | 4'-4" | 2'-2" | 2'-2" | | | | | 4'-6 1/2" |



ELEVATION
(Bent 2 as shown, Bent 1, 3, 4, & 8A similar)
Scale: 1/4" = 1'-0"

BENTS 1 THRU 4 & 8A REINFORCING STEEL CALLOUTS

| | Bent 1 | Bent 2 | Bent 3 | Bent 4 | Bent 8A |
|-------------|-----------|-----------|-----------|-----------|-----------|
| ▲ A5301 | A5302 | A5303 | A5304 | A5300 | A5300 |
| ▲ A10300 | A10300 | A10301 | A10302 | A10302 | A10303 |
| ▲ A11300 | A11300 | A11304 | A11304 | A11304 | A11304 |
| ▲ | | A11302 | A11305 | A11307 | A11307 |
| ▲ 23-C11300 | 23-C11310 | 23-C11320 | 23-C11330 | 29-C11370 | 29-C11370 |
| ▲ 23-C11301 | 23-C11311 | 23-C11321 | 23-C11331 | 29-C11371 | 29-C11371 |
| ▲ 23-C11302 | 23-C11312 | 23-C11322 | 23-C11332 | 29-C11372 | 29-C11372 |
| ▲ 23-C11303 | 23-C11313 | 23-C11323 | 23-C11333 | 29-C11373 | 29-C11373 |
| ▲ 23-C11304 | 23-C11314 | 23-C11324 | 23-C11334 | 29-C11374 | 29-C11374 |
| ▲ 23-C11305 | 23-C11315 | 23-C11325 | 23-C11335 | 29-C11375 | 29-C11375 |
| ▲ 23-C11306 | 23-C11316 | 23-C11326 | 23-C11336 | 29-C11376 | 29-C11376 |
| ▲ H5300 | H5300 | H5302 | H5304 | H5306 | H5306 |
| ▲ H5301 | H5301 | H5303 | H5305 | H5307 | H5307 |
| ▲ WS6300 | WS6310 | WS6320 | WS6330 | WS6370 | WS6370 |
| ▲ WS6301 | WS6311 | WS6321 | WS6331 | WS6371 | WS6371 |
| ▲ WS6302 | WS6312 | WS6322 | WS6332 | WS6372 | WS6372 |
| ▲ WS6303 | WS6313 | WS6323 | WS6333 | WS6373 | WS6373 |
| ▲ WS6304 | WS6314 | WS6324 | WS6334 | WS6374 | WS6374 |
| ▲ WS6305 | WS6315 | WS6325 | WS6335 | WS6375 | WS6375 |
| ▲ WS6306 | WS6316 | WS6326 | WS6336 | WS6376 | WS6376 |
| ▲ | 23-C11307 | | 23-C11307 | 29-C11307 | 29-C11307 |

COLUMN SPACING

| BENT | (A) | (B) | (C) | (D) | (E) | (F) | (G) | (H) |
|------|-------|-------------|-------------|-------------|--------------|--------------|-------------|-------|
| 1 | 8'-0" | 13'-9 1/16" | 28'-8 3/16" | 27'-9 3/16" | 33'-8 1/2" | 26'-11 3/16" | 13'-9 1/2" | 8'-0" |
| 2 | 8'-0" | 13'-9 1/16" | 28'-9" | 27'-9" | 33'-9" | 26'-11" | 13'-9 1/16" | 8'-0" |
| 3 | 8'-0" | 13'-9" | 28'-0 5/8" | 32'-1 1/4" | 33'-8 11/16" | 26'-5 1/16" | 13'-9" | 8'-0" |
| 4 | 8'-0" | 13'-8" | 28'-9" | 32'-2" | 33'-9" | 27'-0" | 18'-7 1/16" | 8'-0" |
| 8A | 8'-0" | 13'-10" | 28'-9" | 32'-2" | 33'-9" | 27'-0" | 29'-0 1/4" | 8'-0" |

- ① I Series L6300 & I Series L6301 @ 7 Eq. Spa. = 3'-7 1/2" (8 Bars per Series)
- ② 8-L7300, 8-L7301 and 8-L7302 @ 7 Eq. Spa. = (Bt 1: 9'-9 1/16"; Bt 2: 9'-9 1/16"; Bt 3: 9'-9"; Bt 4: 9'-8"; Bt 8A: 9'-10")

NOTE: A drilled shaft foundation only shall be used at Bent 1 & 3. For details see Dwg. No. M-472-189A. For drilled shaft alternate see Dwg. No. M-472-189A.

NOTES:

Riser blocks shall be poured monolithically with cap beam. Elastomeric pads shall be in place a minimum of one day before being disturbed by setting beams. Place rubber banding cement in such a way that visible concrete surfaces will not be stained. Riser block bearing surface to conform to bottom of beam grade. Column steel to extend 3'-0" into cap along with standard hook.

See Standard Drawing STD-6-2 for spiral reinforcement details and notes.

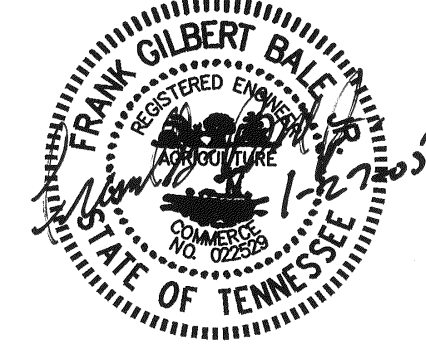
See Standard Drawing STD-6-1 for pile details and notes.

When pouring cap beam, provisions shall be made for setting anchor bolts. See Standard Drawing STD-6-1. Drilling of anchor bolts shall not be allowed. Bolt projection shall be 1".

For Section A-A, B-B, C-C & D-D and additional details, see Dwg. No. M-472-188, 189 & 189A.

Cap reinforcing may be adjusted slightly to miss projecting column bars.

The Contractor may elect to use precast columns or bent caps in the construction of the bents. All precast elements shall be bid, fabricated and installed per the requirements shown on Dwg. No. M-472-293.

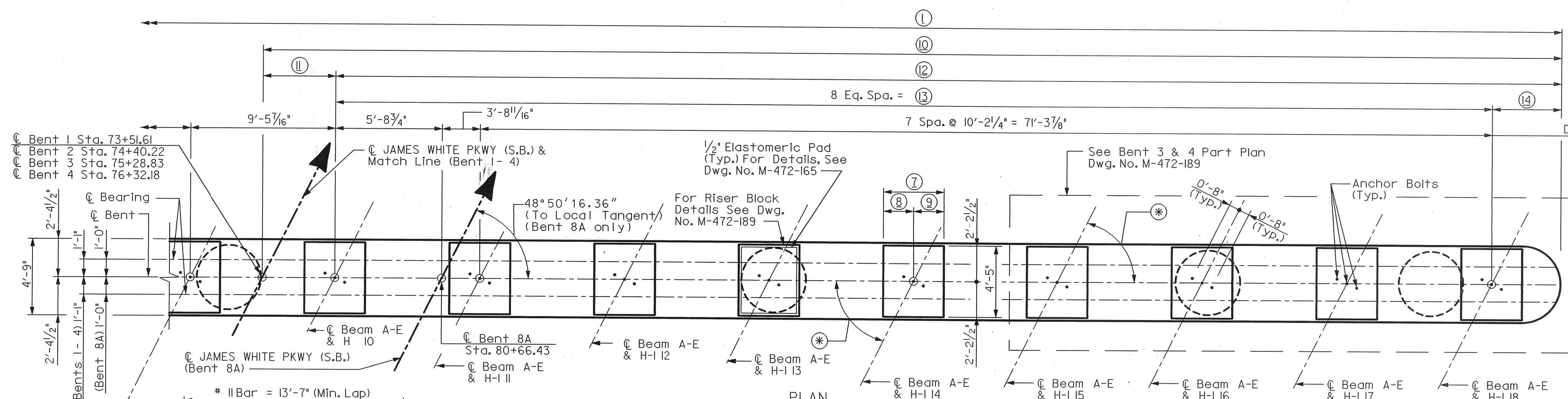


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENTS 1-4 & 8A (I)
JAMES WHITE PKWY OVER
NORFOLK SOUTHERN RWY.,
WILLOW AVE. AND JACKSON AVE.
STA. 81+37.61 @ JWP (S.B.)
KNOX COUNTY
2005

CORRECT ENGINEER OF STRUCTURES
APPROVED DIRECTOR OF HIGHWAYS

DESIGNED BY: AAM DATE: 12-04
DRAWN BY: VD DATE: 12-04
SUPERVISED BY: FCB DATE: 12-04
CHECKED BY: CCS DATE: 01-05

DESIGNER: AAM
 DRAWN BY: VD
 SUPERVISED BY: FCB
 CHECKED BY: CCS



* For Angles see Framing Plan Dwg. No. M-472-146, 147

Dimensions for Bent 8A only

Note: For Dimensions not shown see Bent 1 thru 4 & 8A Dimension Table on Dwg. No. M-472-186.

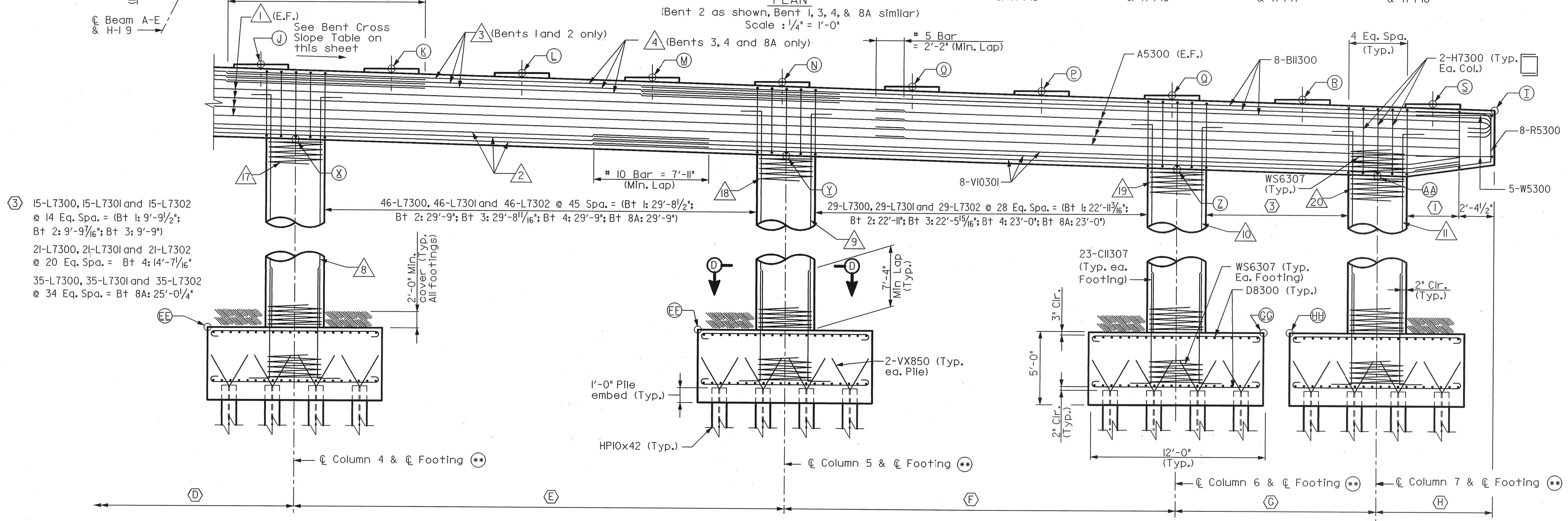
NOTES:

For additional bent notes see Dwg. No. M-472-186.

CONSTR. NO. 47003-3154-44

| PROJECT NO. | YEAR | SHEET NO. |
|-----------------|------|-----------|
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
|-----------|------|----|---|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 03-02-05 | VD | | Revised Estimated Quantities. |
| 05-25-07 | VD | | Revised Beam Seat Elevation for Bents 1 thru 4. |



ELEVATION
(Bent 2 as shown, Bent 1, 3, 4, & 8A similar)
Scale: 1/4" = 1'-0"

** NOTE: A drilled shaft foundation only shall be used at Bent 1 & 3. For details see Dwg. No. M-472-189A. For drilled shaft alternate see Dwg. No. M-472-189A.

- ③ 15-L7300, 15-L7301 and 15-L7302 @ 14 Eq. Spa. = (Bt 1: 9'-9 1/2"; Bt 2: 9'-9 1/6"; Bt 3: 9'-9")
- 21-L7300, 21-L7301 and 21-L7302 @ 20 Eq. Spa. = Bt 4: 14'-7 1/6"
- 35-L7300, 35-L7301 and 35-L7302 @ 34 Eq. Spa. = Bt 8A: 25'-0 1/4"

ESTIMATED QUANTITIES

| | CLASS "A" CONCRETE (BRIDGES) PILE ALTERNATE C.Y. | CLASS "A" CONCRETE (BRIDGES) SHAFT ALTERNATE C.Y. | STEEL BAR REINF. (BRIDGES) PILE ALTERNATE LB. | STEEL BAR REINF. (BRIDGES) SHAFT ALTERNATE LB. |
|----------|--|---|---|--|
| △ Bent 1 | 216 | 216 | 104713 | 104713 |
| △ Bent 2 | 418 | 231 | 146894 | 11037 |
| △ Bent 3 | 239 | 239 | 112328 | 112328 |
| Bent 4 | 423 | 236 | 149214 | 113357 |
| Bent 8A | 408 | 221 | 177737 | 112628 |

| BENT | SLOPE |
|------|---------|
| 1 | 2.8296% |
| 2 | 2.8296% |
| 3 | 4.1861% |
| 4 | 4.9837% |
| 8A | 4.4970% |

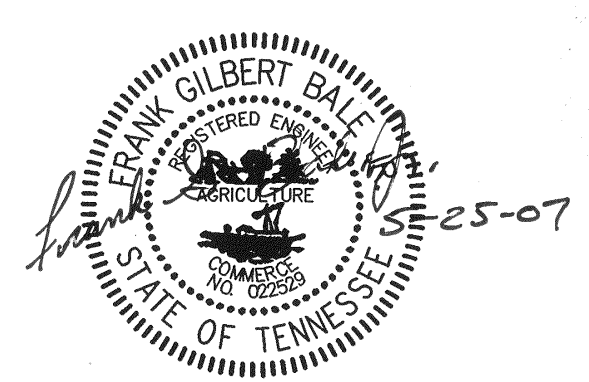
| BENTS 1 THRU 4 & 8A TABLE OF ELEVATIONS | | | | | | | | | | | | | | | | | | | | |
|---|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|-----------|----------|-----------|----------|
| BENT | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T |
| 1 | 923.543 | 924.022 | 923.768 | 923.515 | 923.262 | 923.008 | 922.755 | 922.502 | 922.248 | 921.995 | 921.742 | 921.488 | 921.235 | 920.982 | 920.728 | 920.475 | 920.222 | 919.968 | 919.715 | 918.995 |
| 2 | 921.487 | 921.949 | 921.689 | 921.426 | 921.163 | 920.898 | 920.632 | 920.365 | 920.096 | 919.826 | 919.554 | 919.281 | 919.009 | 918.738 | 918.468 | 918.199 | 917.932 | 917.666 | 917.396 | 916.938 |
| 3 | 919.989 | 919.902 | 919.659 | 919.404 | 919.139 | 918.862 | 918.573 | 918.270 | 917.956 | 917.627 | 917.286 | 916.932 | 916.565 | 916.184 | 915.749 | 915.298 | 914.959** | 914.793* | 914.523** | 914.273* |
| 4 | 918.754 | 918.829 | 918.435 | 918.027 | 917.606 | 917.171 | 916.721 | 916.257 | 915.777 | 915.281 | 914.796 | 914.323 | 913.862 | 913.413 | 912.935 | 912.469 | 912.106** | 911.939* | 911.646** | 911.396* |
| 8A | 906.109 | 906.252 | 905.857 | 905.460 | 905.060 | 904.656 | 904.251 | 903.842 | 903.431 | 903.035 | 902.649 | 902.207 | 901.646 | 901.097 | 900.561 | 900.037 | 899.526 | 899.028 | 898.542 | 897.991 |

| BENTS 1 THRU 4 & 8A TABLE OF ELEVATIONS | | | | | | | | | | |
|---|---------|---------|---------|---------|---------|---------|---------|---------|---------|---------|
| BENT | U | V | W | X | Y | Z | AA | BB | CC | DD |
| 1 | 918.566 | 918.176 | 917.363 | 916.577 | 915.624 | 914.862 | 914.471 | 890.485 | 893.274 | 892.802 |
| 2 | 916.510 | 916.120 | 915.306 | 914.521 | 913.566 | 912.804 | 912.414 | 884.516 | 884.481 | 885.887 |
| 3 | 914.904 | 914.329 | 913.154 | 911.811 | 910.399 | 909.290 | 908.714 | 881.977 | 882.144 | 882.591 |
| 4 | 913.605 | 912.924 | 911.491 | 909.888 | 908.206 | 906.861 | 905.934 | 882.328 | 882.291 | 882.490 |
| 8A | 900.999 | 900.377 | 899.084 | 897.638 | 896.120 | 894.906 | 893.601 | 878.306 | 879.220 | 877.956 |

- * - Riser Block (Span D side)
- ** - Riser Block (Span C side)
- ** - Riser Block (Span E side)

DESIGNED BY: AAM
 DRAWN BY: VD
 SUPERVISED BY: FCB
 CHECKED BY: CCS

DATE: 12-04
 DATE: 12-04
 DATE: 12-04
 DATE: 01-05

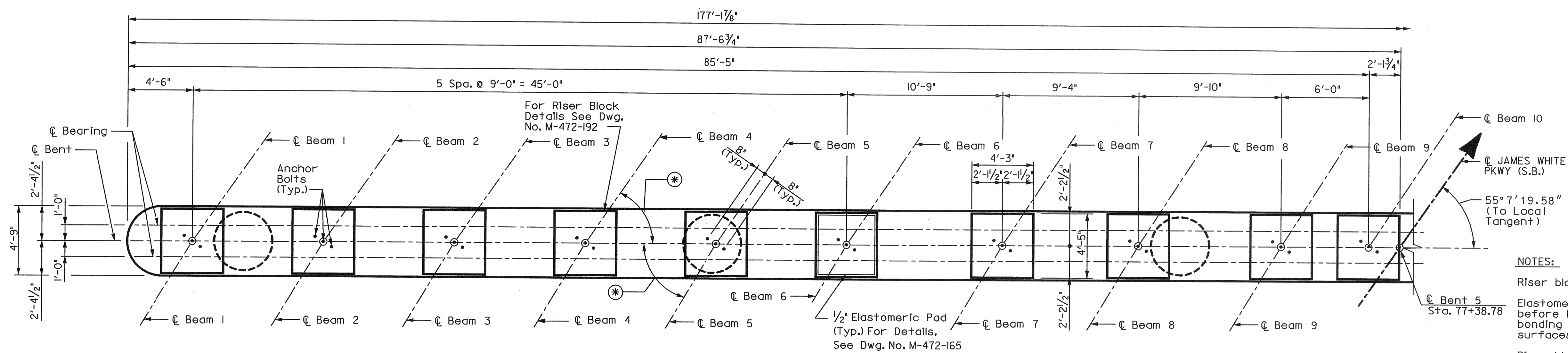


STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENTS 1-4 & 8A (2)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

CORRECT
 ENGINEER OF STRUCTURES
 APPROVED
 DIRECTOR OF HIGHWAYS

SCANNED
 cadkon M 99829 045
 J:\Sfr\N-408rdway\5th Ave.F.dgn\TN1400Plans\New

| | | | |
|---------------------------|------|-----------|-------------------|
| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
| | | | |
| | | | |
| | | | |
| | | | |
| | | | |



* For Angles see Framing Plan Dwg. No. M-472-147

PLAN
Scale: 1/4" = 1'-0"

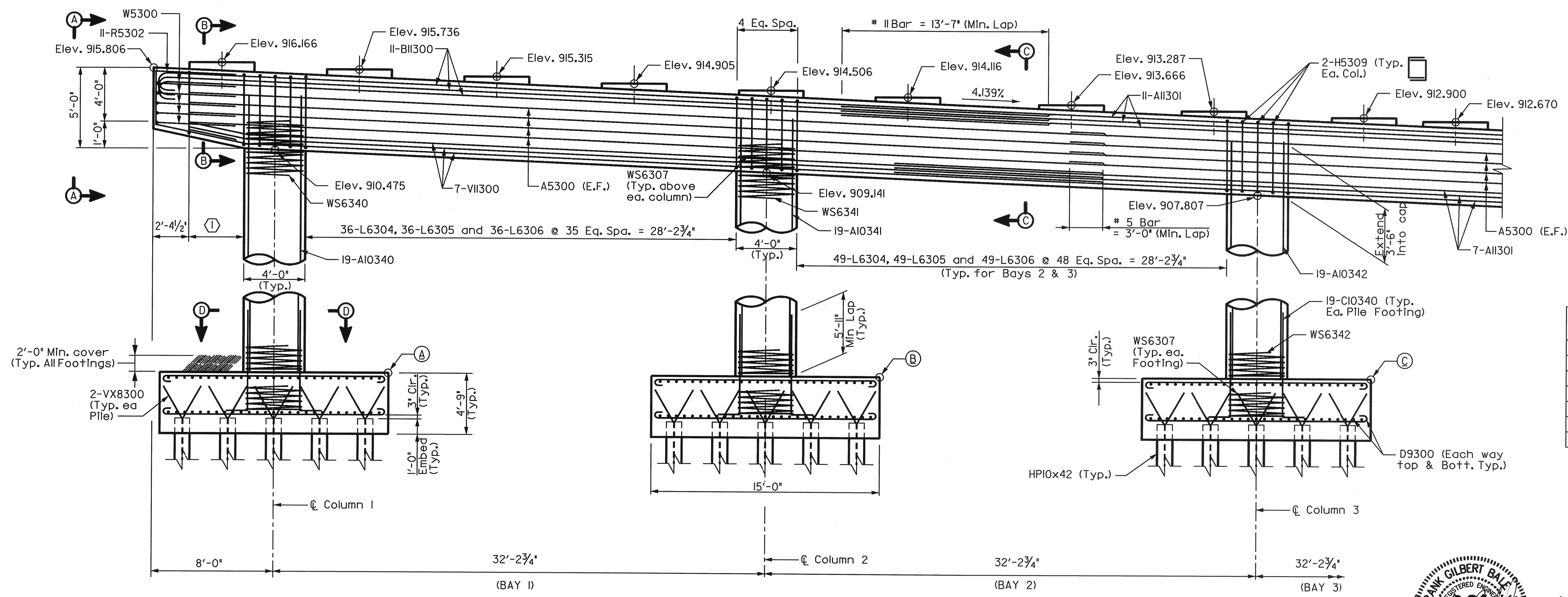
ESTIMATED QUANTITIES

| | CLASS "A" CONCRETE (BRIDGES) C.Y. | STEEL BAR REINFORCEMENT (BRIDGES) LB. |
|-----------------|-----------------------------------|---------------------------------------|
| Pile Alternate | 472 | 160583 |
| Shaft Alternate | 234 | 112777 |

NOTES:

- Riser blocks shall be poured monolithically with cap beam.
- Elastomeric pads shall be in place a minimum of one day before being disturbed by setting beams. Place rubber bonding cement in such a way that visible concrete surfaces will not be stained.
- Riser block bearing surface to conform to bottom of beam grade.
- Column steel to extend 3'-6" into cap.
- See Standard Drawing STD-6-2 for spiral reinforcement details and notes.
- See Standard Drawing STD-6-1 for pile details and notes.
- When pouring cap beam, provisions shall be made for setting anchor bolts. See Standard Drawing STD-6-1. Drilling of anchor bolts shall not be allowed. Bolt projection shall be 1/2".

- For Sections A-A, B-B, C-C, D-D and additional details, see Dwg. No. M-472-192 and M-472-192A.
- Cap reinforcing may be adjusted slightly to miss projecting column bars.
- The Contractor may elect to use precast columns or bent caps in the construction of the bents. All precast bent elements shall be bid, fabricated and installed per the requirements shown on Dwg. No. M-472-293.
- For drilled shaft alternate see Dwg. No. M-472-192A.



ELEVATION
Scale: 1/4" = 1'-0"

| COLUMN | ELEVATION |
|--------|-----------|
| 1 | 878.406 |
| 2 | 878.226 |
| 3 | 877.827 |
| 4 | 877.808 |
| 5 | 878.137 |
| 6 | 879.014 |

| SECTION | ELEVATION |
|---------|-----------|
| A | 882.156 |
| B | 881.976 |
| C | 881.577 |
| D | 881.558 |
| E | 881.887 |
| F | 882.764 |

① I Series L5307 & I Series L5308 @ 7 Eq. Spa. = 3'-7 1/2" (8 Bars per Series)

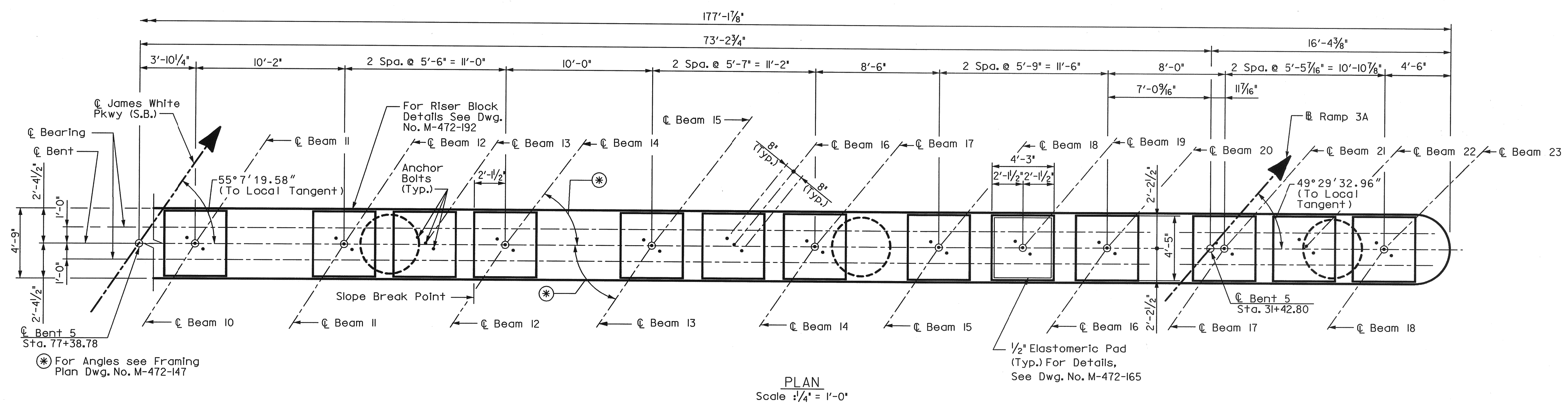
| | | | |
|---------------|-----|------|------|
| DESIGNED BY | AAM | DATE | 9-04 |
| DRAWN BY | VD | DATE | 9-04 |
| SUPERVISED BY | FGP | DATE | 9-04 |
| CHECKED BY | CCS | DATE | 1-21 |



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENT 5 (5)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

CORRECT _____ ENGINEER OF STRUCTURES
 APPROVED _____ DIRECTOR OF HIGHWAYS
 M-472-190

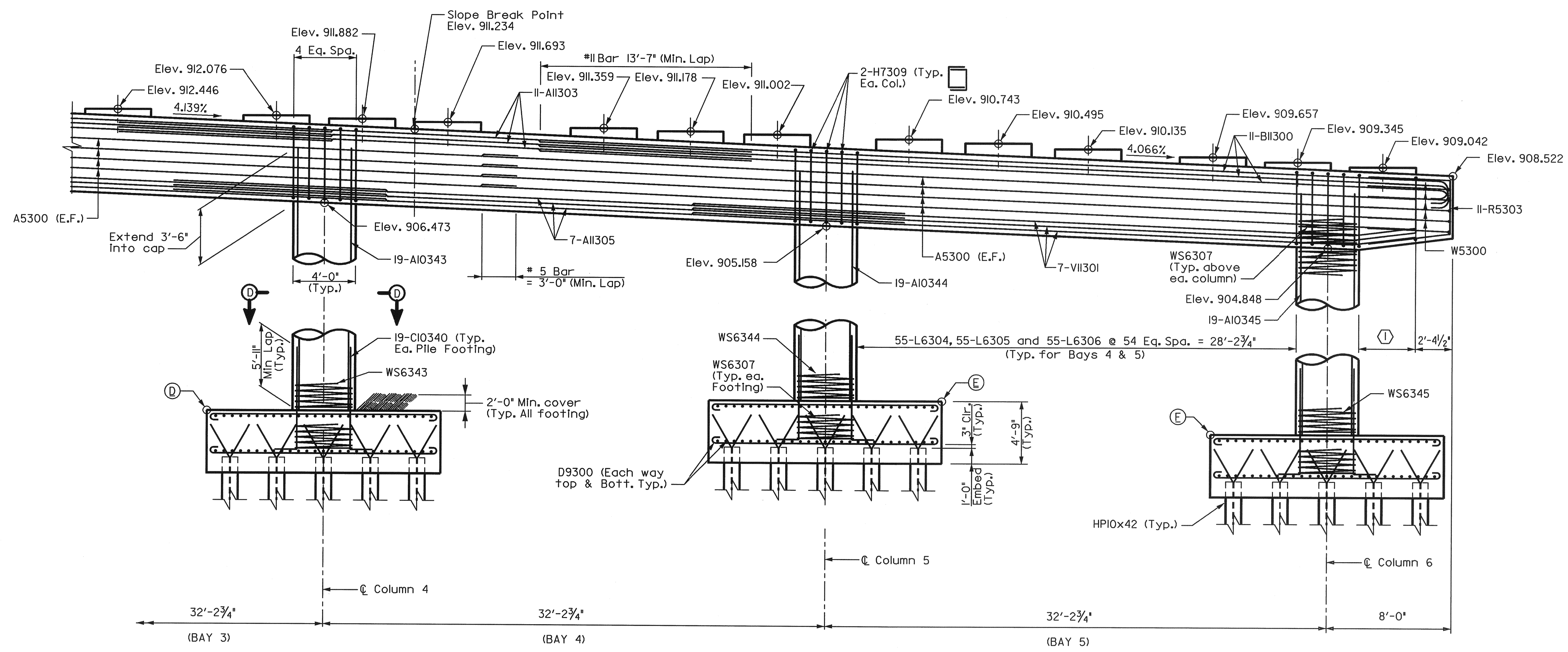
vdmtriley
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PLAN
Scale: 1/4" = 1'-0"

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|---------------------------|------|-----------|-------------------|
| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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NOTES:
For bent notes see Dwg. No. M-472-190.



① I Series L5307 & I Series L5308
@ 7 Eq. Spa. = 3'-7 1/2"
(8 Bars per Series)

DESIGNED BY: CCS DATE: 9-04
 DRAWN BY: VD DATE: 9-04
 SUPERVISED BY: FGB DATE: 9-04
 CHECKED BY: CCS DATE: 01-05



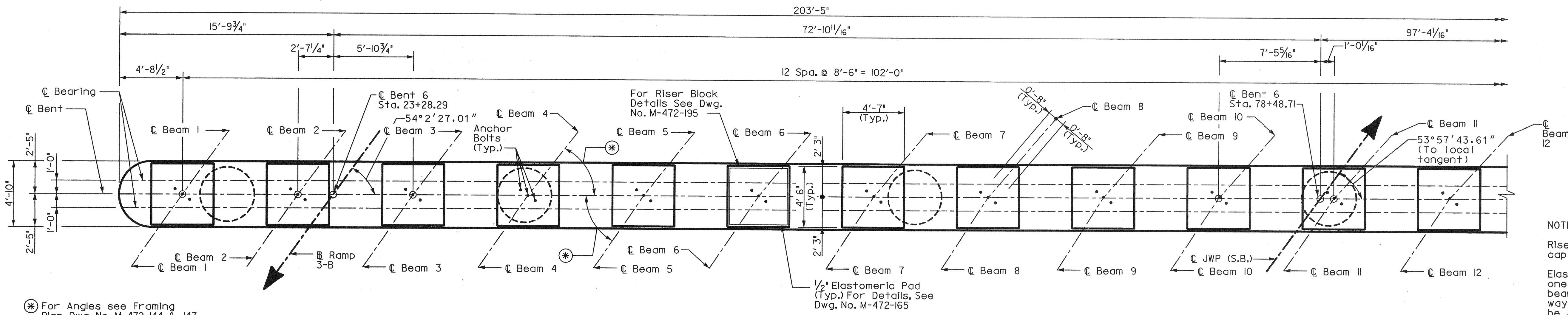
CORRECT
ENGINEER OF STRUCTURES
APPROVED
DIRECTOR OF HIGHWAYS

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENT 5 (6)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

| PROJECT NO. | YEAR | SHEET NO. |
|-----------------|------|-----------|
| IM/NH-40-8(135) | 2005 | |

REVISIONS

| NO. | DATE | BY | DESCRIPTION |
|-----|----------|-----|------------------------------|
| 1 | 02-09-05 | CED | Revised Bent 6 Reinforcement |

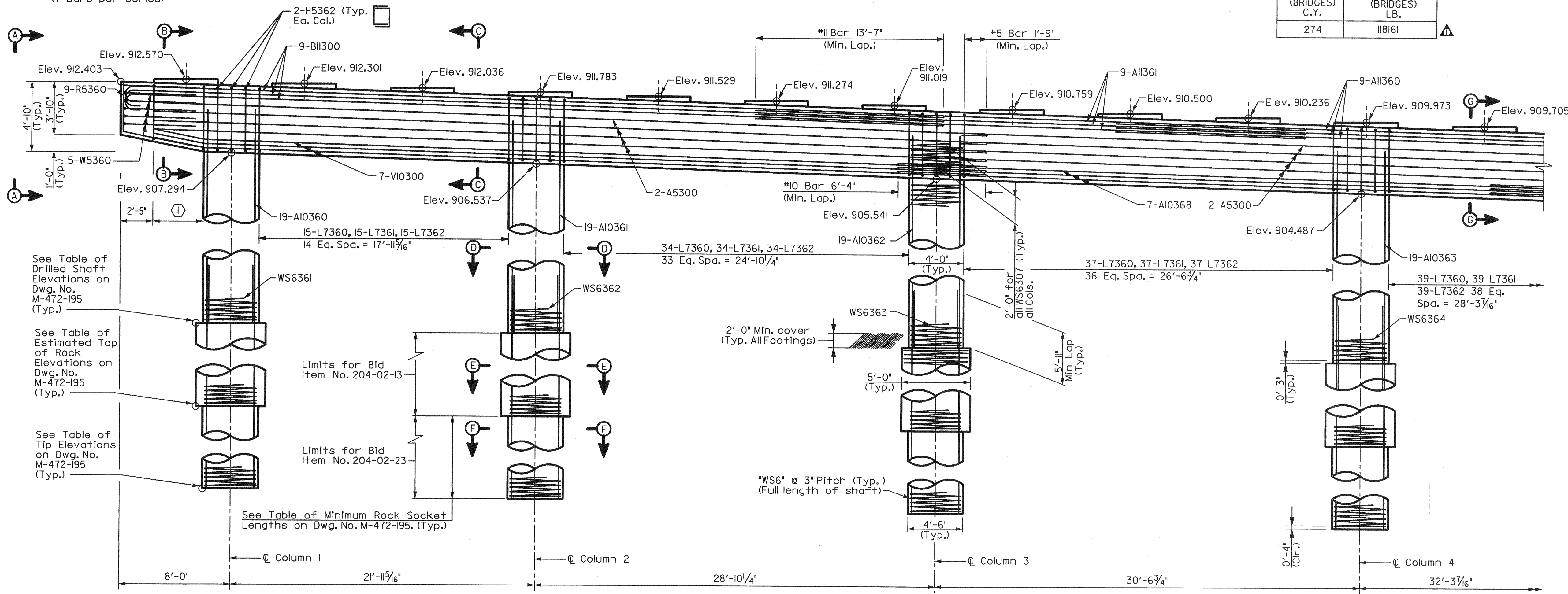


* For Angles see Framing Plan Dwg. No. M-472-144 & 147

① 6 Eq. Spa. = 3'-7" 1 Series L5363 / w 1 Series L5364 (7 Bars per Series)

PLAN Scale: 1/4" = 1'-0"

| ESTIMATED QUANTITIES | |
|-----------------------------------|---------------------------------------|
| CLASS "A" CONCRETE (BRIDGES) C.Y. | STEEL BAR REINFORCEMENT (BRIDGES) LB. |
| 274 | 118161 |



See Table of Drilled Shaft Elevations on Dwg. No. M-472-195 (Typ.)

See Table of Estimated Top of Rock Elevations on Dwg. No. M-472-195 (Typ.)

See Table of Tip Elevations on Dwg. No. M-472-195 (Typ.)

Limits for Bid Item No. 204-02-13

Limits for Bid Item No. 204-02-23

See Table of Minimum Rock Socket Lengths on Dwg. No. M-472-195. (Typ.)

NOTES:

Riser blocks shall be poured monolithically with cap beam.

Elastomeric pads shall be in place a minimum of one day before being disturbed by setting beams. Place rubber banding cement in such a way that visible concrete surfaces will not be stained.

Riser block bearing surface to conform to bottom of beam grade.

Column steel to extend 3'-6" into cap.

See Standard Drawing STD-6-2 for spiral reinforcement details and notes.

When pouring cap beam, provisions shall be made for setting anchor bolts. See Standard Drawing STD-6-1. Drilling of anchor bolts shall not be allowed. Bolt projection shall be 1".

Cap reinforcing may be adjusted slightly to miss projecting column bars.

For Sections A-A, B-B, C-C, D-D, E-E, F-F and additional details, see Dwg. No. M-472-195.

The Contractor may elect to use precast columns or bent caps in the construction of the bents. All precast bent elements shall be bid, fabricated and installed per the requirements shown on Dwg. No. M-472-293.



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENT 6 (8)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

| | | | |
|---------------|-----|------|-------|
| DESIGNED BY | AAM | DATE | 12-04 |
| DRAWN BY | CD | DATE | 1-23 |
| SUPERVISED BY | FGB | DATE | 12-04 |
| CHECKED BY | FGB | DATE | 1-24 |

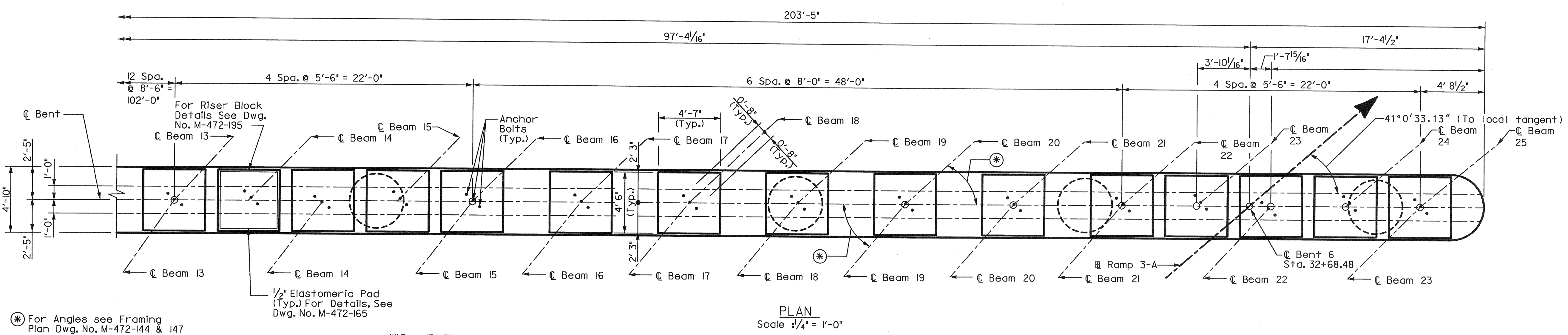
CORRECT ENGINEER OF STRUCTURES

APPROVED DIRECTOR OF HIGHWAYS

SCANNED

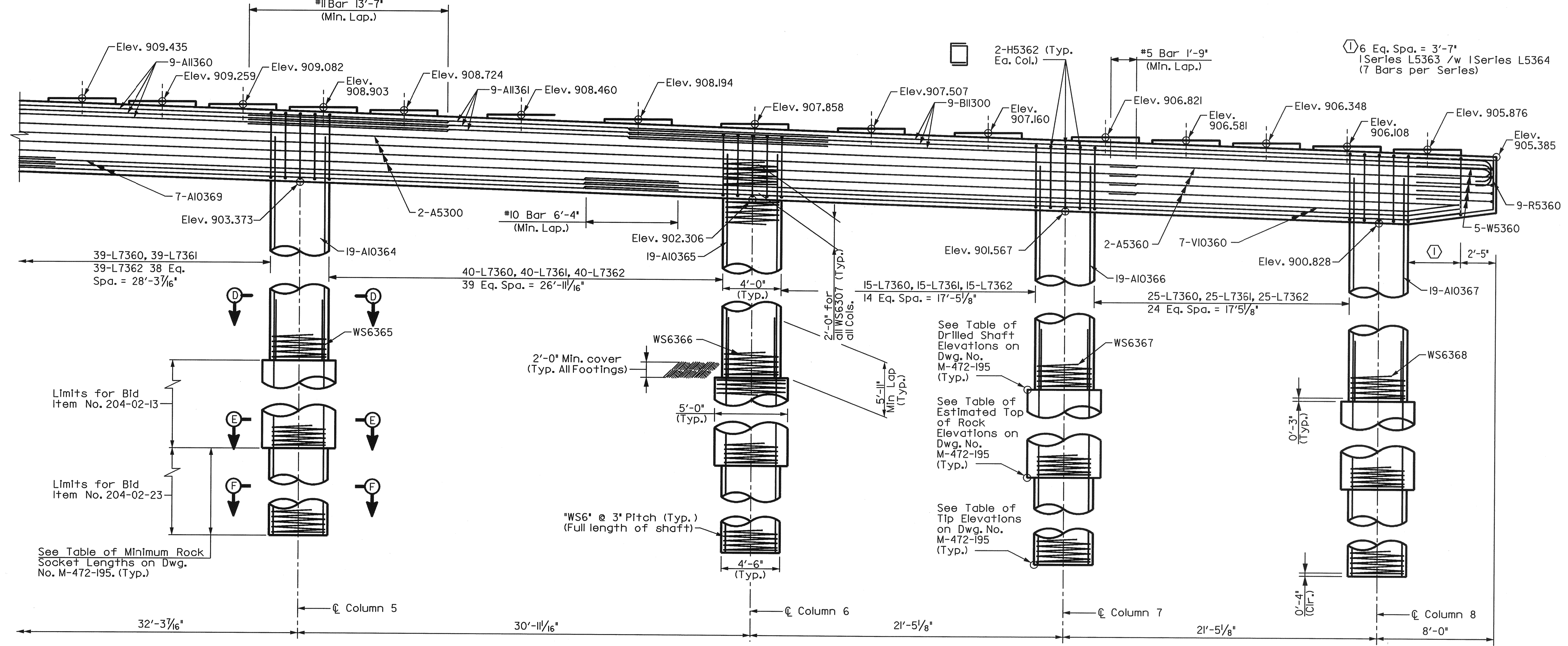
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| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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NOTES:
For bent notes see Dwg. No. M-472-193.



* For Angles see Framing Plan Dwg. No. M-472-144 & 147

PLAN
Scale: 1/4" = 1'-0"



Limits for Bid Item No. 204-02-13

Limits for Bid Item No. 204-02-23

See Table of Minimum Rock Socket Lengths on Dwg. No. M-472-195. (Typ.)

See Table of Drilled Shaft Elevations on Dwg. No. M-472-195 (Typ.)

See Table of Estimated Top of Rock Elevations on Dwg. No. M-472-195 (Typ.)

See Table of Tip Elevations on Dwg. No. M-472-195 (Typ.)



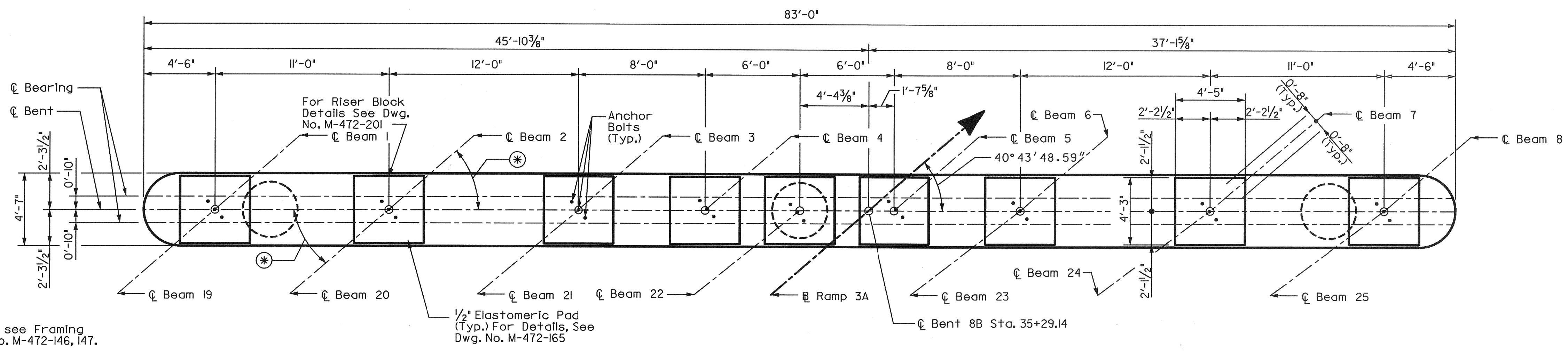
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENT 6 (9)
JAMES WHITE PKWY OVER
NORFOLK SOUTHERN Rwy.,
WILLOW AVE. AND JACKSON AVE.
STA. 81+37.61 @ JWP (S.B.)
KNOX COUNTY
2005

| | | | |
|---------------|-----|------|-------|
| DESIGNED BY | AAM | DATE | 12-04 |
| DRAWN BY | CD | DATE | 12-23 |
| SUPERVISED BY | FCB | DATE | 12-04 |
| CHECKED BY | FCB | DATE | 1-24 |

CORRECT
ENGINEER OF STRUCTURES
APPROVED
DIRECTOR OF HIGHWAYS

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 Date: 01/27/2005
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|---------------------------|------|-----------|-------------------|
| CONSTR. NO. 47003-3154-44 | | | |
| PROJECT NO. | YEAR | SHEET NO. | |
| IM/NH-40-8(135) | 2005 | | |
| REVISIONS | | | |
| NO. | DATE | BY | BRIEF DESCRIPTION |
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PLAN
Scale: 1/4" = 1'-0"

| ESTIMATED QUANTITIES | |
|---|--|
| CLASS #A CONCRETE (BRIDGES) C.Y. | STEEL BAR REINFORCEMENT (BRIDGES) LB. |
| 85 | 44231 |

* For Angles see Framing Plan Dwg. No. M-472-146, 147.

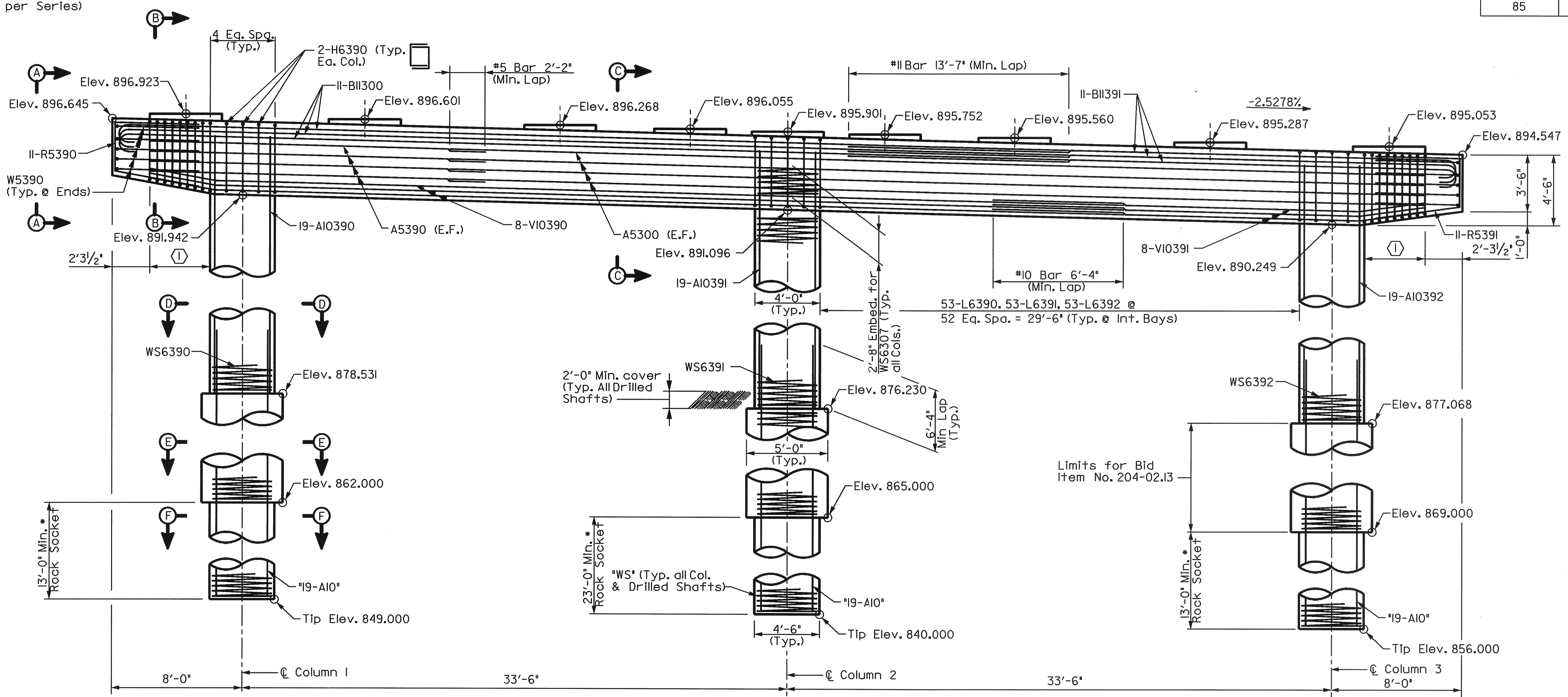
① Series L5390, 1 Series L5391, 1 Series L5392 @ 7 Eq. Spa. = 3'-8 1/2" (8 Bars per Series)

- NOTES:
- Riser blocks shall be poured monolithically with cap beam.
 - Elastomeric pads shall be in place a minimum of one day before being disturbed by setting beams. Place rubber bonding cement in such a way that visible concrete surfaces will not be stained.
 - Riser block bearing surface to conform to bottom of beam grade.
 - Column steel to extend 3'-6" into cap.
 - See Standard Drawing STD-6-2 for spiral reinforcement details and notes.
 - When pouring cap beam, provisions shall be made for setting anchor bolts. See Standard Drawing STD-6-1. Drilling of anchor bolts shall not be allowed. Bolt projection shall be 1".
 - Cap reinforcing may be adjusted slightly to miss projecting column bars.
 - For Section A-A, B-B, C-C, D-D, E-E, F-F and additional details, see Dwg. No. M-472-201.

The Contractor may elect to use precast columns or bent caps in the construction of the bents. All precast bent elements shall be bid, fabricated and installed per the requirements shown on Dwg. No. M-472-293.

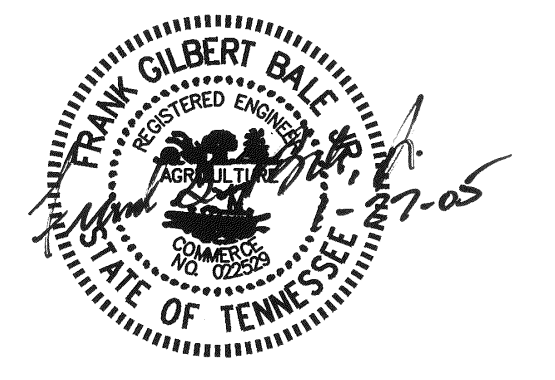
E.F. - Denotes Each Face

Unless otherwise approved by the engineer, each drilled shaft shall be advanced into rock until both that shaft's minimum length of rock socket is achieved and the tip elevation is reached.



PLAN
Scale: 1/4" = 1'-0"

* Limits for Bid Item No. 204-02.23



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENT 8B (15)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005

| | | | |
|---------------|-----|------|-------|
| DESIGNED BY | CCS | DATE | 12-04 |
| DRAWN BY | CD | DATE | 12-04 |
| SUPERVISED BY | FCB | DATE | 12-04 |
| CHECKED BY | CCS | DATE | 1-05 |

CORRECT _____
 ENGINEER OF STRUCTURES
 APPROVED _____
 DIRECTOR OF HIGHWAYS

| | | |
|---------------------------|------|-----------|
| CONSTR. NO. 47003-3154-44 | | |
| PROJECT NO. | YEAR | SHEET NO. |
| IM/NH-40-8(135) | 2005 | |

| REVISIONS | | | |
|-----------|---------|-----|-------------------|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 5-24-08 | CMD | GENERAL REVISIONS |
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NOTES:

Riser blocks shall be poured monolithically with cap beam.

Elastomeric pads shall be in place a minimum of one day before being disturbed by setting beams. Place rubber bonding cement in such a way that visible concrete surfaces will not be stained.

Riser block bearing surface to conform to bottom of beam grade.

Column steel to extend 3'-6" into cap.

See Standard Drawing STD-6-2 for spiral reinforcement details and notes.

When pouring cap beam, provisions shall be made for setting anchor bolts. See Standard Drawing STD-6-1. Drilling of anchor bolts shall not be allowed. Bolt projection shall be 11".

Cap reinforcing may be adjusted slightly to miss projecting column bars.

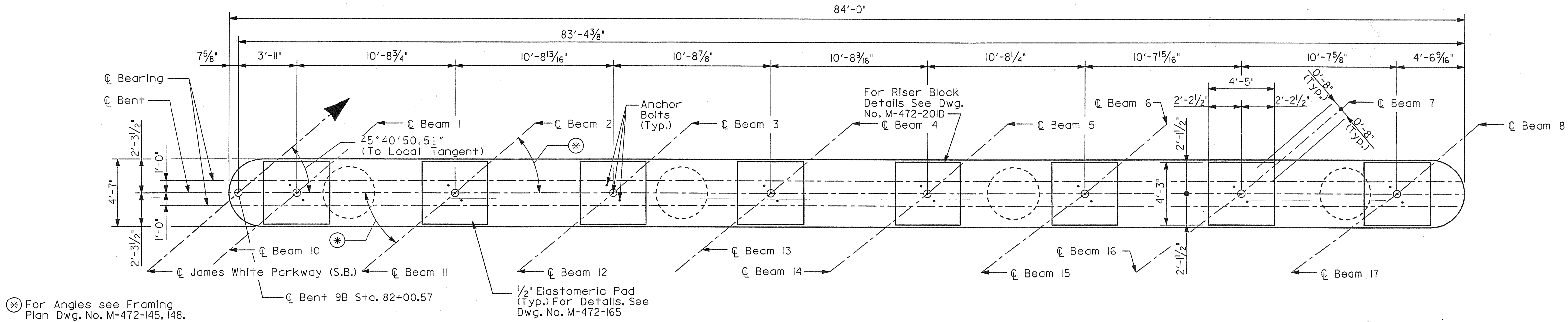
For Section A-A, B-B, C-C, D-D, E-E, F-F and additional details, see Dwg. No. M-472-201D.

The Contractor may elect to use precast columns or bent caps in the construction of the bents. All precast bent elements shall be bid, fabricated and installed per the requirements shown on Dwg. No. M-472-293.

E.F. - Denotes Each Face

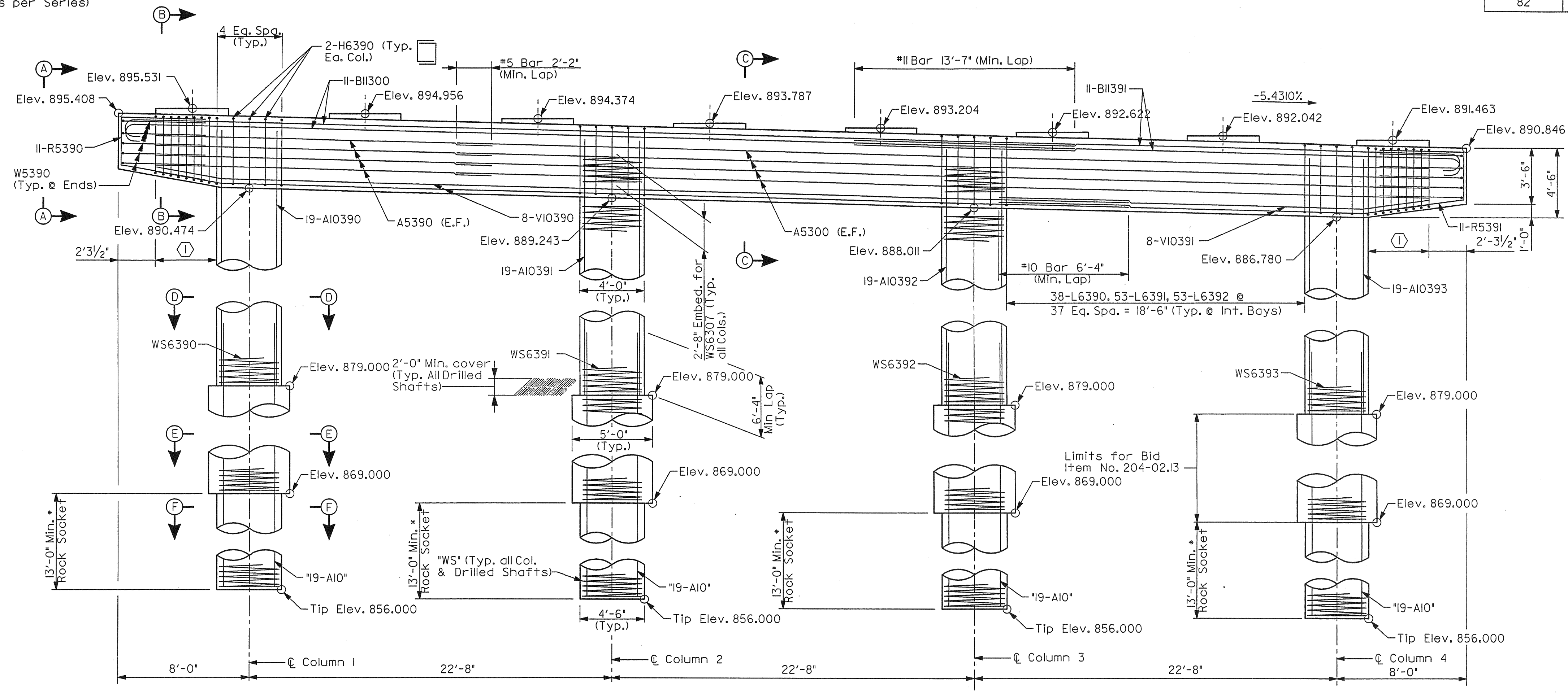
Unless otherwise approved by the engineer, each drilled shaft shall be advanced into rock until both that shaft's minimum length of rock socket is achieved and the tip elevation is reached.

| ESTIMATED QUANTITIES | |
|---------------------------------------|-------|
| CLASS "A" CONCRETE (BRIDGES) C.Y. | 82 |
| STEEL BAR REINFORCEMENT (BRIDGES) LB. | 36337 |



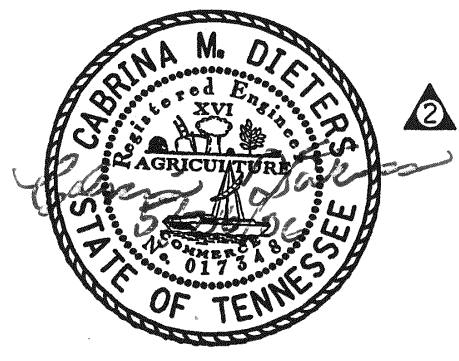
* For Angles see Framing Plan Dwg. No. M-472-145, 148.

① Series L5390, Series L5391, Series L5392 @ 7 Eq. Spa. = 3'-8 1/2" (8 Bars per Series)

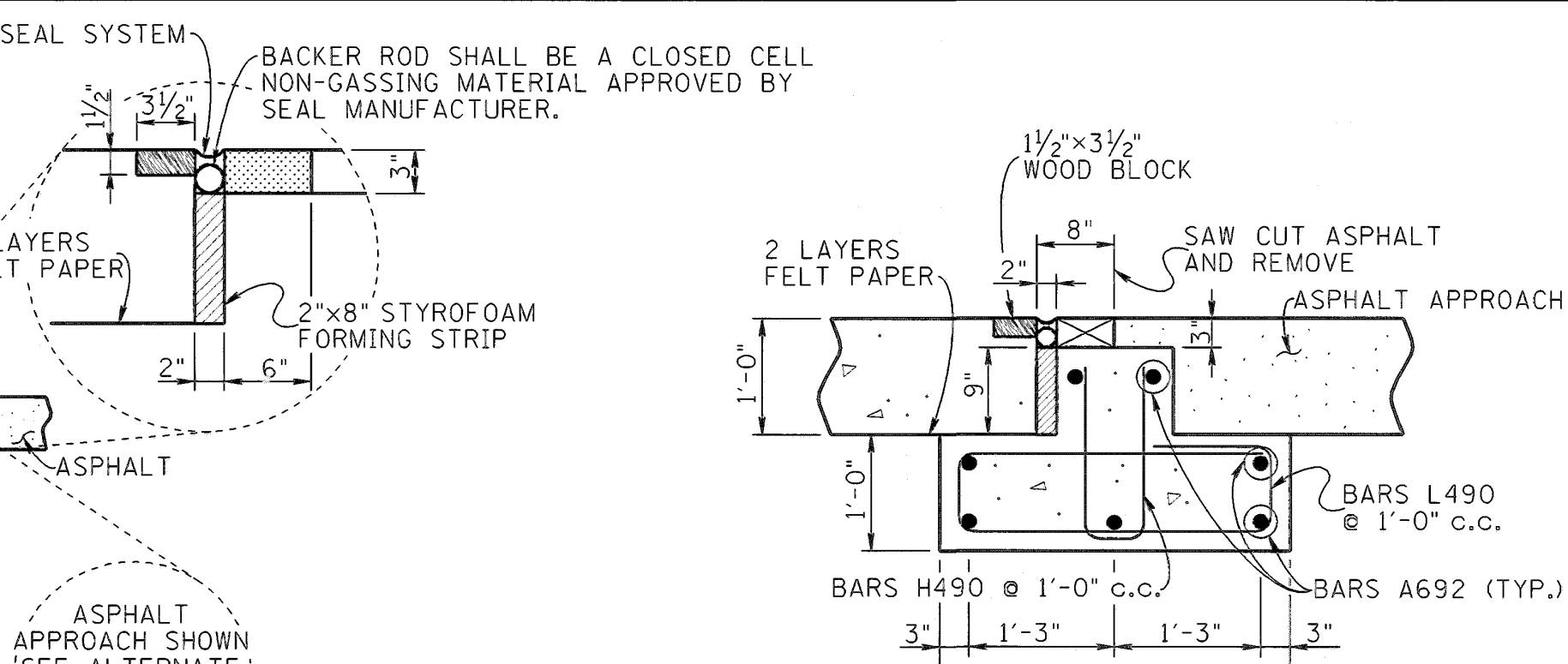
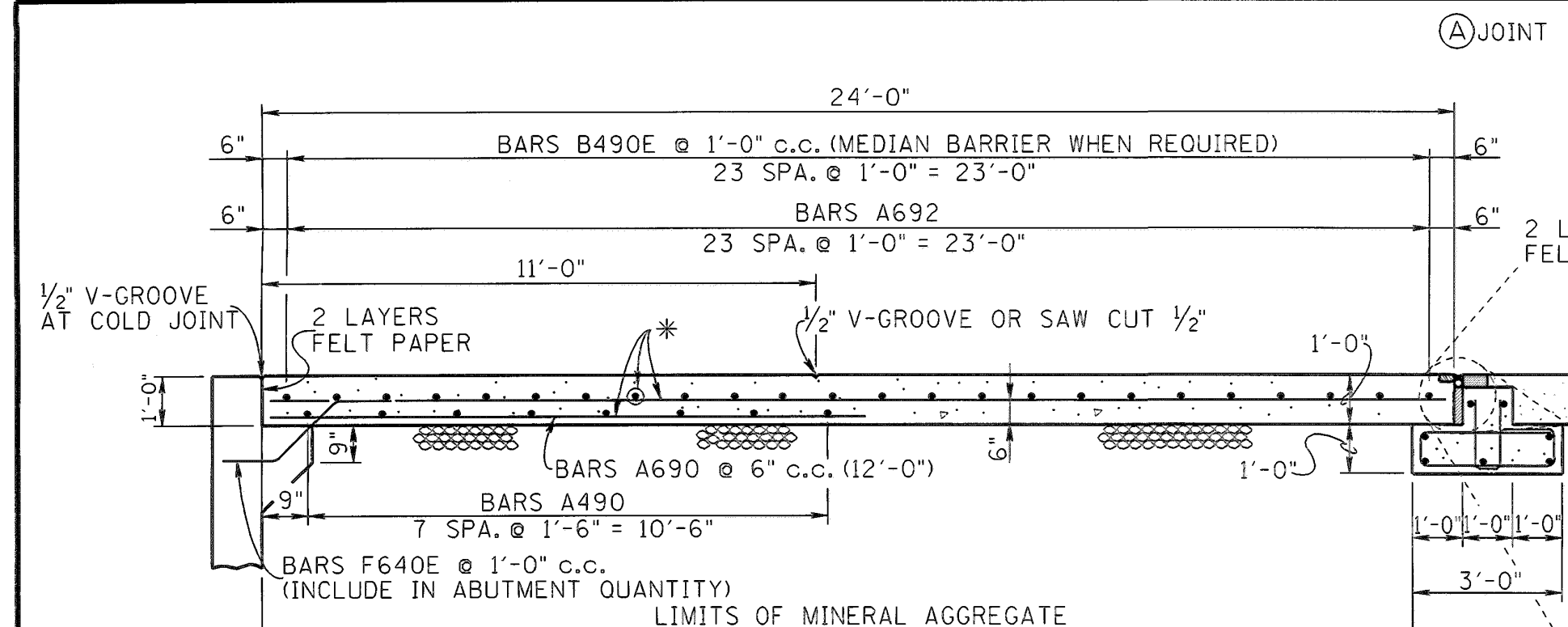


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|---------------|-----|------|-------|
| DESIGNED BY | CCS | DATE | 12-04 |
| DRAWN BY | CD | DATE | 12-04 |
| SUPERVISED BY | FCB | DATE | 12-04 |
| CHECKED BY | CCS | DATE | 1-05 |

CORRECT _____ ENGINEER OF STRUCTURES
 APPROVED _____ DIRECTOR OF HIGHWAYS



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF HIGHWAYS
BRIDGE NO. 21
BENT 9B (18)
 JAMES WHITE PKWY OVER
 NORFOLK SOUTHERN RWY.,
 WILLOW AVE. AND JACKSON AVE.
 STA. 81+37.61 @ JWP (S.B.)
 KNOX COUNTY
 2005



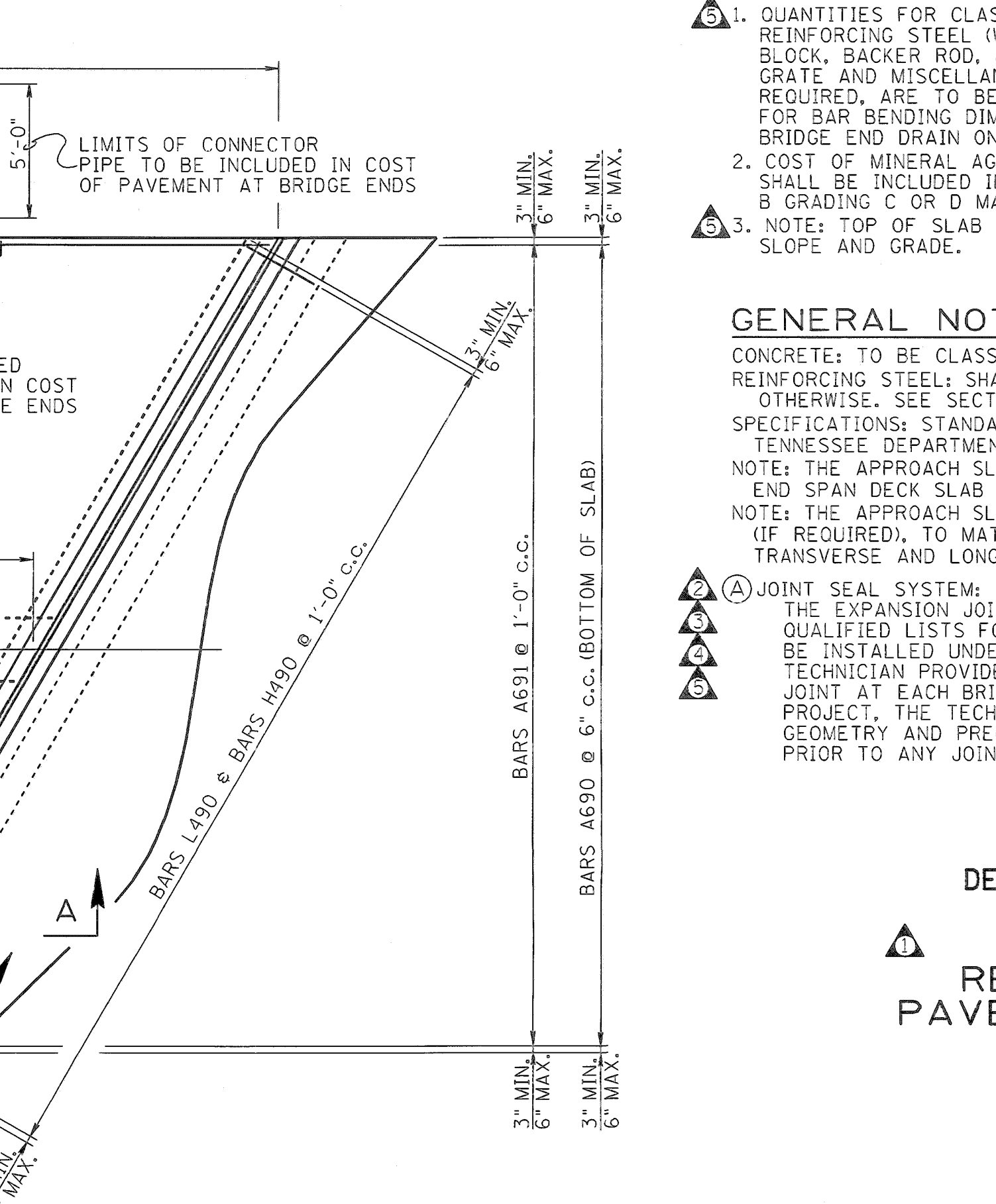
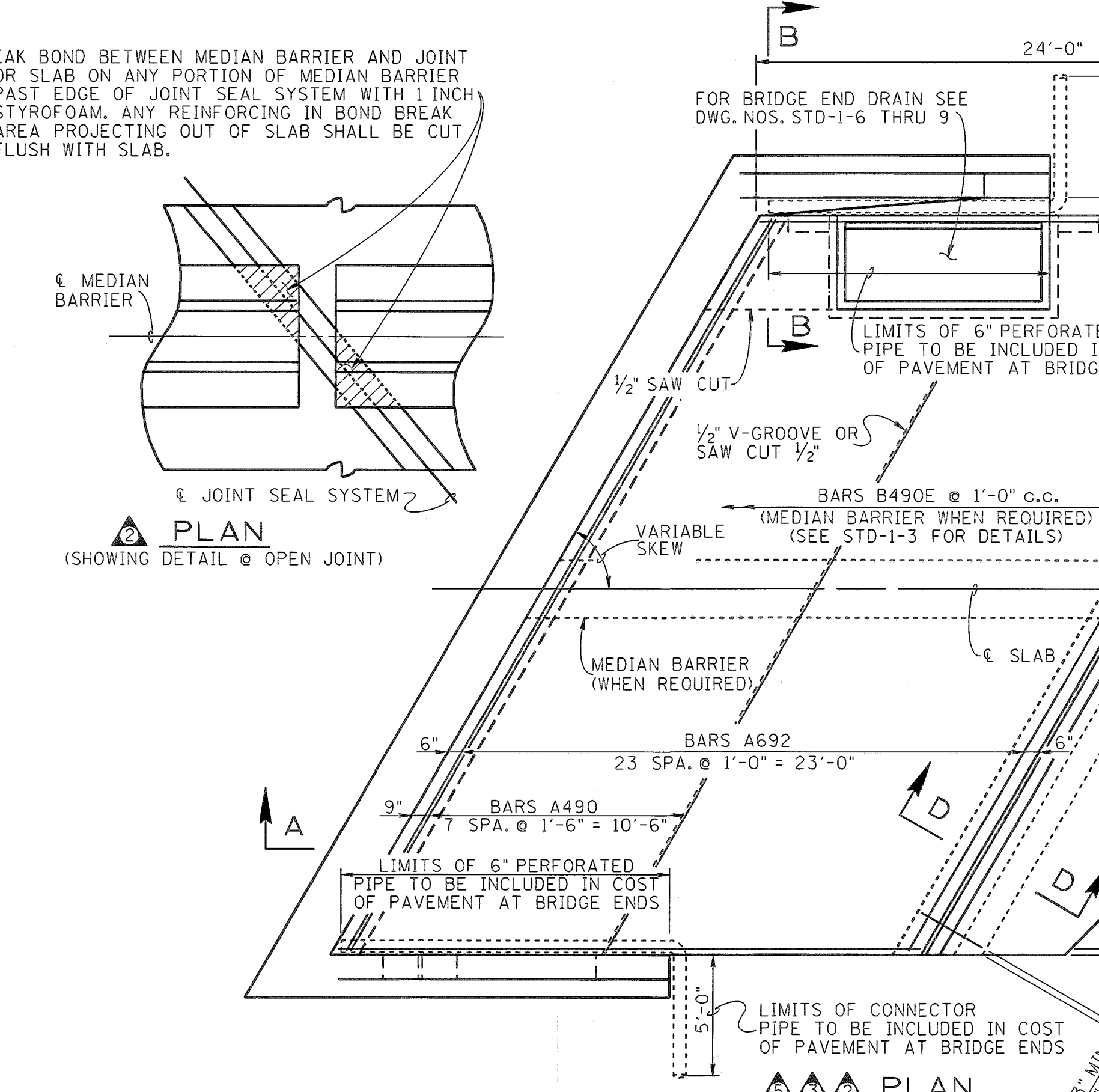
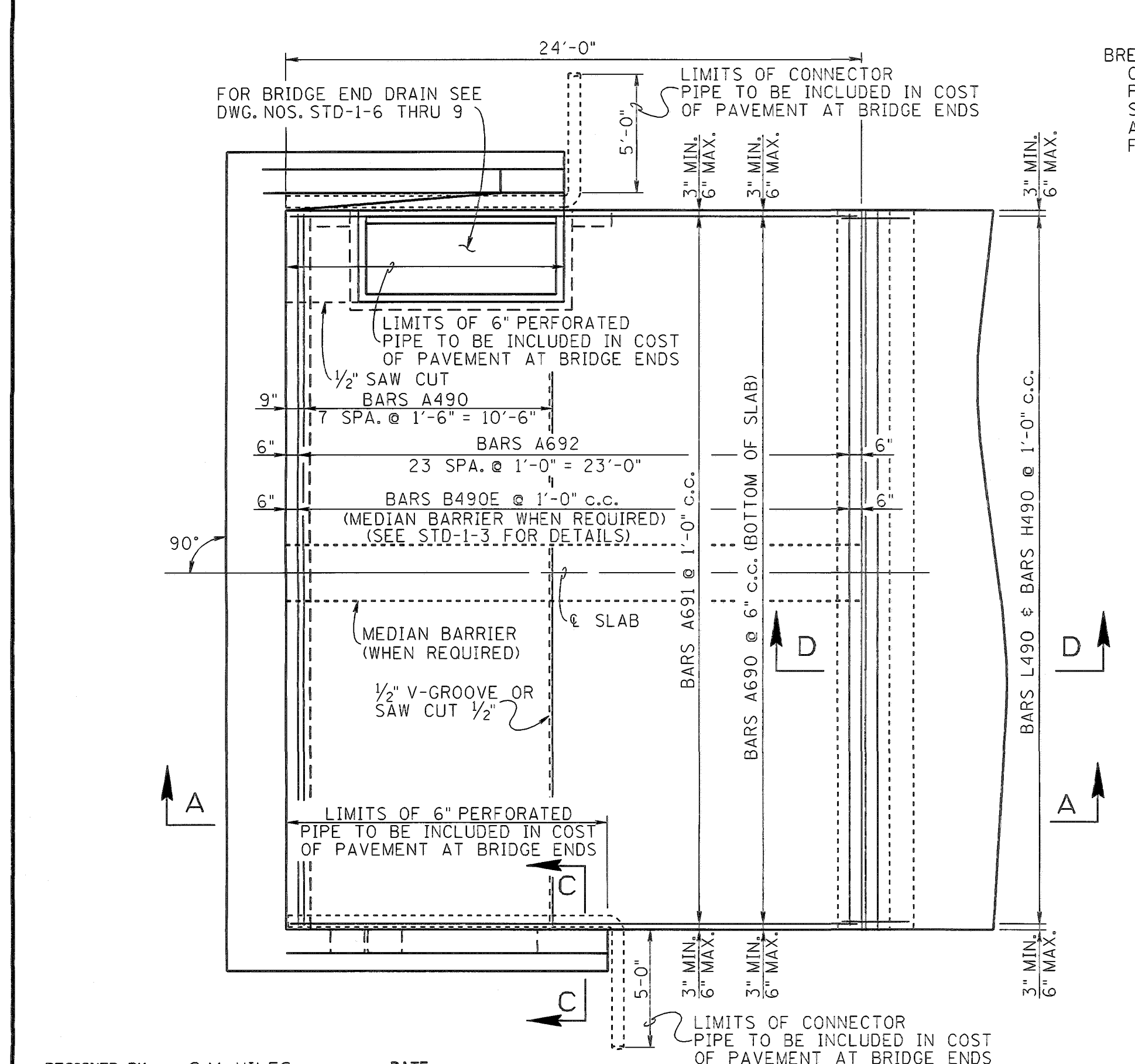
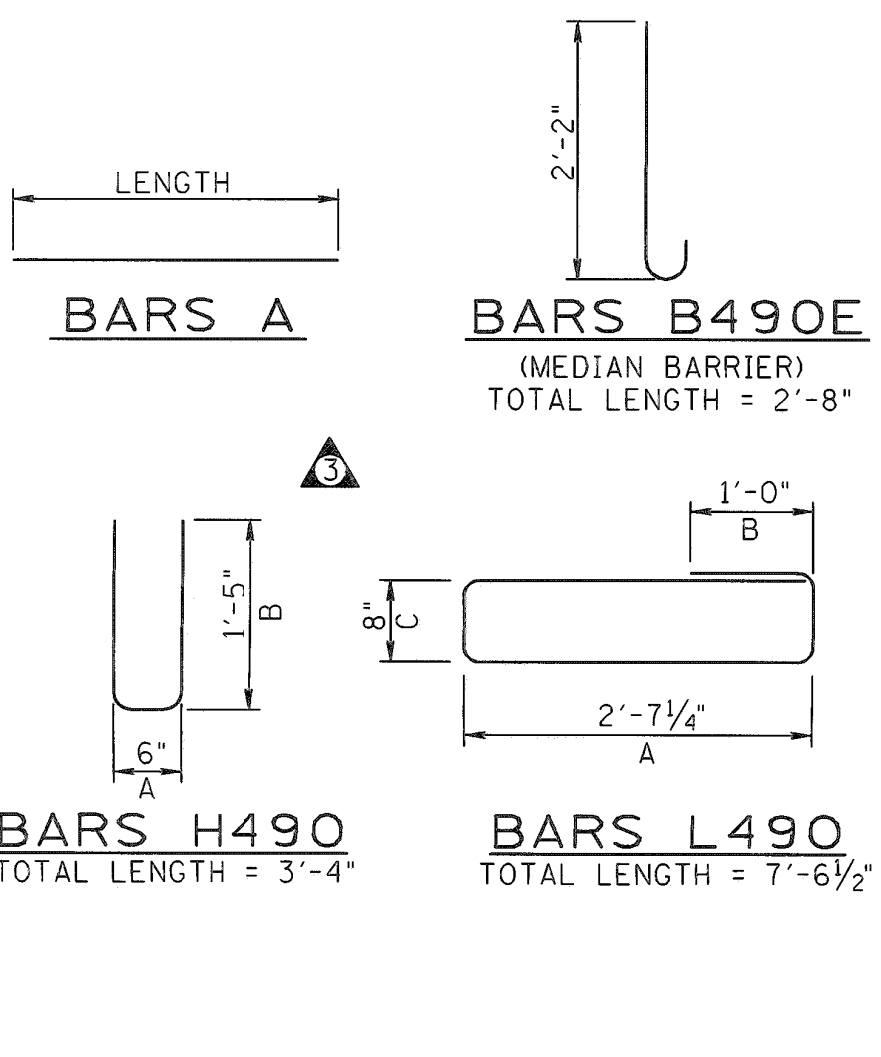
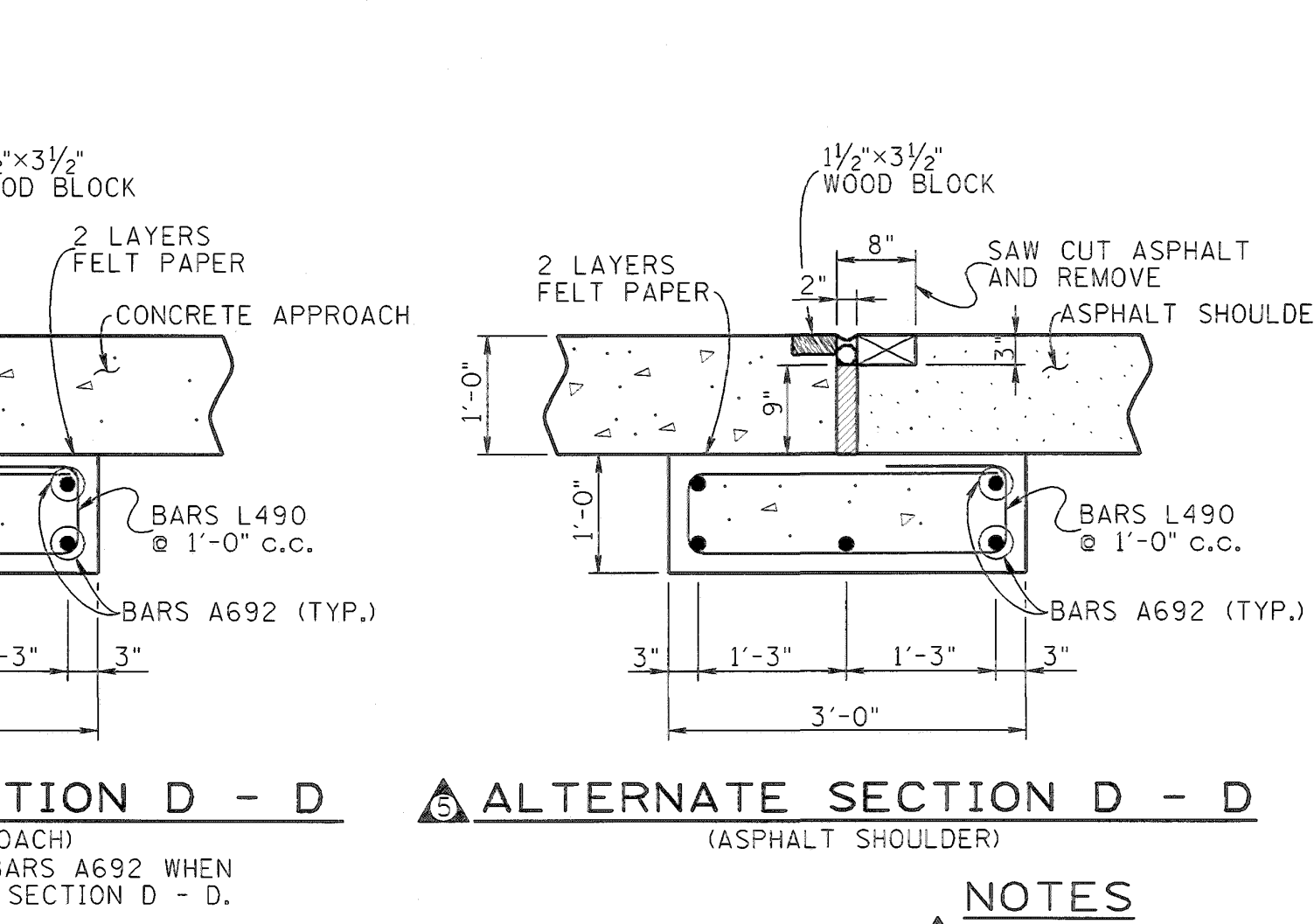
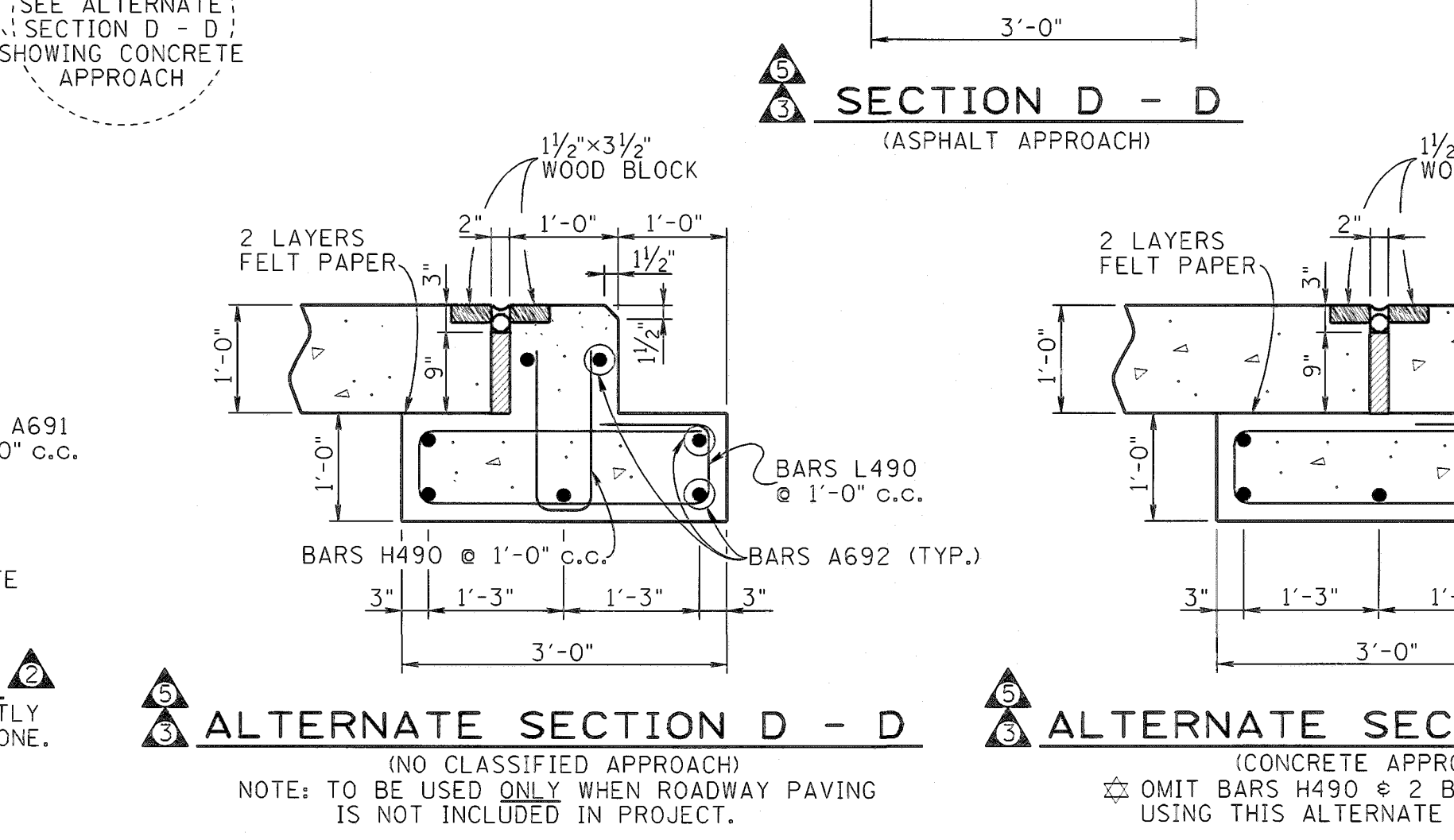
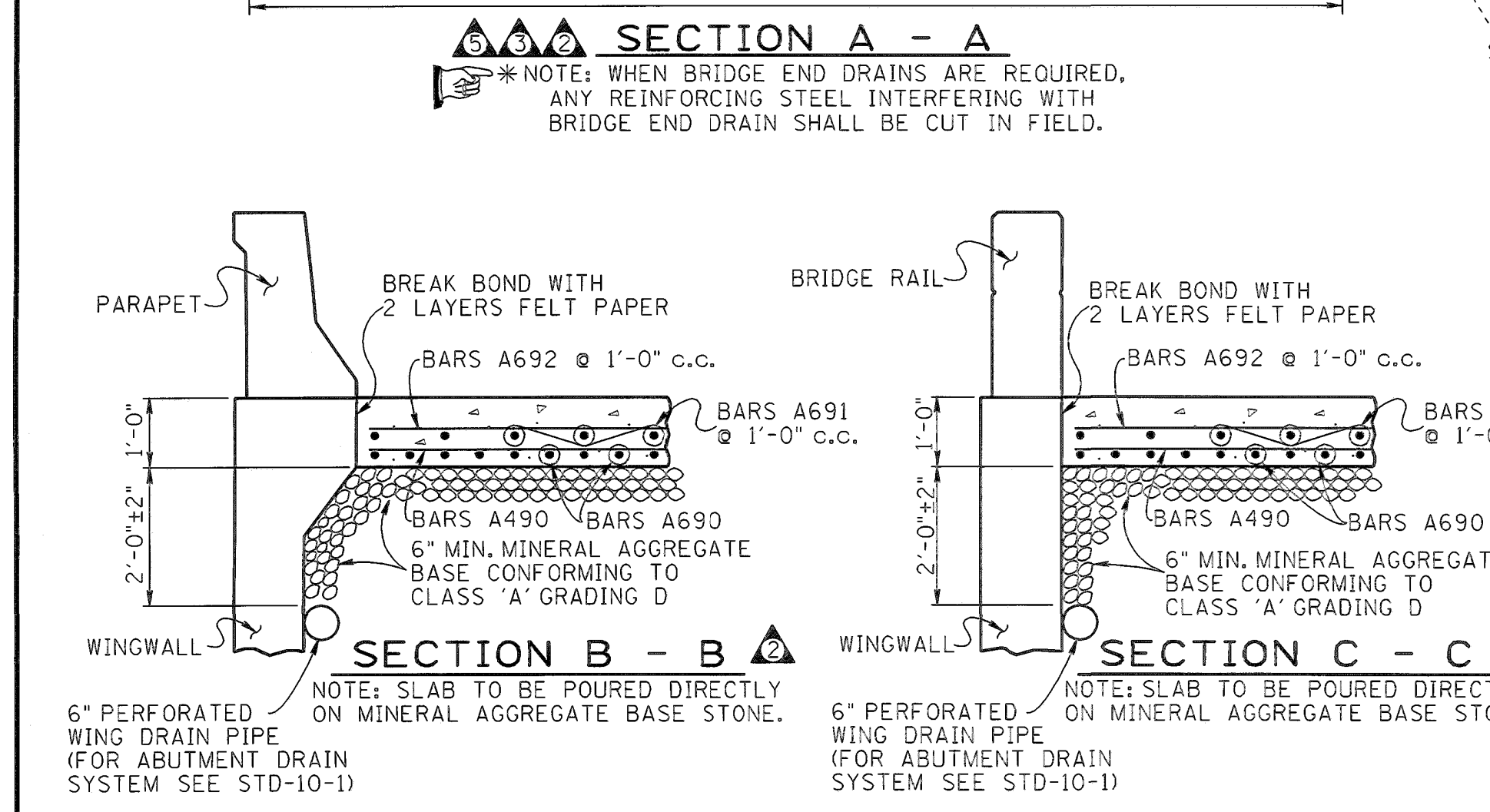
BILL OF STEEL

| BARS | LOCATION | SIZE | NO. REQ'D | BENDING DIMENSIONS | | | | LENGTH |
|-------|----------------------|------|-----------|--------------------|-------|----|---|-----------|
| | | | | A | B | C | D | |
| B490E | MEDIAN (WHEN REQ'D.) | 4 | 48 | | | | | 2'-8" |
| A490 | SLAB | 4 | 8 | | | | | ▲ |
| A690 | SLAB | 6 | ▲ | | | | | 12'-0" |
| A691 | SLAB | 6 | ▲ | | | | | 23'-8" |
| A692 | SLAB | 6 | 31 | | | | | ▲ |
| H490 | FOOTING | 4 | ▲ | 6" | 1'-4" | | | 3'-4" |
| L490 | FOOTING | 4 | ▲ | 2'-7 1/4" | 1'-0" | 8" | | 7'-6 1/2" |

▲ THESE NUMBERS VARY DEPENDING UPON ROADWAY WIDTH.

| PROJECT NO. | YEAR | SHEET NO. |
|-------------|------|-----------|
| | 1995 | |

| REVISIONS | | | |
|-----------|----------|-----|---|
| NO. | DATE | BY | BRIEF DESCRIPTION |
| 1 | 5-1-95 | CMH | GENERAL REVISION REDESIGN |
| 2 | 12-18-95 | CMH | ADDED BARS A490 AND JOINT DETAIL |
| 3 | 4-28-97 | CMH | REVISED JOINT DETAILS AND NOTE |
| 4 | 9-6-99 | CMH | REVISED JOINT NOTE |
| 5 | 7-31-00 | CMH | REVISED JOINT DETAILS, NOTE, LIMITS OF PIPE AND ADDED NOTE #3 |



MINOR REVISION - FHWA APPROVAL NOT REQUIRED

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS 1995

CORRECT *Edward P. Wasserman*
 ENGINEER OF STRUCTURES

VOID

DESIGNED BY: C.M. HILES
 DRAWN BY: KIM FRANKENFIELD
 SUPERVISED BY: C.M. HILES
 CHECKED BY:

DATE: 4-95
 DATE: 4-95
 DATE:

PLAN (90° SKEW)